

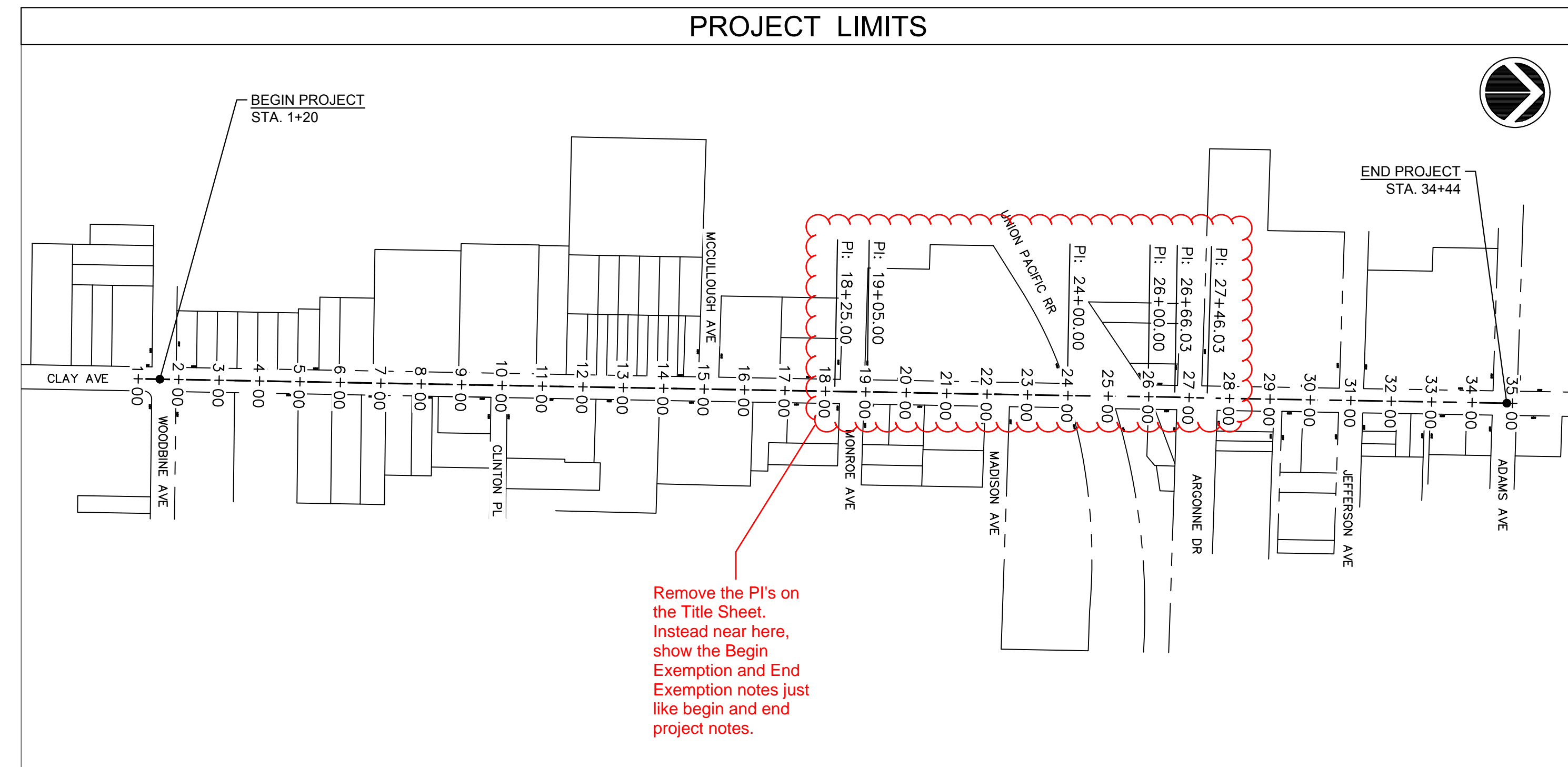
CITY OF KIRKWOOD

SOUTH CLAY AVENUE RESURFACING PROJECT

Add Federal Aid
Project Number
STP-5502(614)

FROM W. WOODBINE AVE TO W. MADISON AVE &
W. ARGONNE DR TO W. ADAMS AVE

GENERAL LEGEND	
	STORM MANHOLE
	STORM INLET
	STORM INLET
	STORM DOUBLE INLET
	FLARED END SECTION
	DOWNSPOUT
	SANITARY MANHOLE
	SANITARY/STORM CLEANOUT
	UNKNOWN MANHOLE
	WATER VALVE
	HYDRANT
	WELL
	SPRINKLER BOX
	WATER METER
	WATER SERVICE
	POWER POLE
	POWER POLE W/ LIGHT
	POWER POLE W/ METER
	GUY WIRE
	GUY POLE
	ELECTRIC MANHOLE
	ELECTRIC PEDESTAL/TRANSFORMER
	ELECTRIC METER
	TELEPHONE POLE
	TELEPHONE MANHOLE
	TELEPHONE PEDESTAL
	UTILITY MANHOLE
	HANDHOLE
	GAS VALVE
	LIGHT POLE
	VAPOR LIGHT
	LIGHT JUNCTION BOX
	SIGN
	FLAGPOLE
	POST/BOLLARD
	CONIFER TREE
	DECIDUOUS TREE
	BUSH/SHRUB
	TREE STUMP
	CONTROL POINT
	BENCHMARK
	SOIL BORING HOLE
	R.O.W. MARKER, FOUND
	RAILROAD SPIKE, FOUND
	PIPE, FOUND
	CONCRETE MONUMENT, FOUND
	MEASURED DIMENSION
	RECORDED DIMENSION
	SPOT ELEVATION
	GRADE LABEL
	DRAINAGE SLOPE
	LINE CONTINUATION



INDEX OF SHEETS	
COVER	1
NOTES-LEGEND-SITE CONTROL	2
SUMMARY OF QUANTITIES	3
SCHEDULE OF QUANTITIES	4
EXISTING CONDITIONS AND DEMO PLAN	1
EXISTING CONDITIONS AND DEMO PLAN	2
EXISTING CONDITIONS AND DEMO PLAN	3
TYPICAL SECTIONS	1
SOUTH CLAY AVENUE PLAN	1
SOUTH CLAY AVENUE PLAN	2
SOUTH CLAY AVENUE PLAN	3
SOUTH CLAY AVENUE PLAN	4
SOUTH CLAY AVENUE PLAN	5
SOUTH CLAY AVENUE PLAN	6
S. CLAY AVE & W. ARGONNE AVE PLAN & PROFILE	7
S. CLAY AVE & W. JEFFERSON AVE PLAN & PROFILE	8
S. CLAY AVE & W. ARGONNE AVE INTERSECTION DETAIL	1
S. CLAY AVE & W. JEFFERSON AVE INTERSECTION DETAIL	2
INTERSECTION WARPING DETAIL	1
S. CLAY AVE & W. ARGONNE AVE PLAN & PROFILE	1
EROSION CONTROL PLAN	1
EROSION CONTROL PLAN	2
EROSION CONTROL PLAN	3
PAVEMENT MARKINGS & TRAFFIC SIGNING PLAN	1
PAVEMENT MARKINGS & TRAFFIC SIGNING PLAN	2
PAVEMENT MARKINGS & TRAFFIC SIGNING PLAN	3
DETAILS	1
WOODBINE - MADISON CROSS SECTIONS	1
WOODBINE - MADISON CROSS SECTIONS	2
WOODBINE - MADISON CROSS SECTIONS	3
WOODBINE - MADISON CROSS SECTIONS	4
WOODBINE - MADISON CROSS SECTIONS	5
WOODBINE - ADAMS CROSS SECTIONS	6
ARGONNE - ADAMS CROSS SECTIONS	7

If we show the index sheet numbers, they should be accurate or blank for the preliminary plan submittal.

STANDARD SPECIFICATIONS

UNLESS OTHERWISE NOTED, ALL WORK ON THIS PROJECT SHALL BE COVERED BY THE ST. LOUIS COUNTY STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, AND BY APPLICATION ST. LOUIS COUNTY STANDARD DRAWINGS.

LENGTH OF PROJECT		
CLAY AVENUE		
END OF PROJECT	STA. 34+44.00	
BEGINNING OF PROJECT	STA. 01+20.00	
APPARENT LENGTH	3324.00	FEET
EQUATIONS AND EXCEPTIONS	397.03	FEET
TOTAL CORRECTIONS	0.00	FEET
NET LENGTH OF PROJECT	2926.97	FEET
COUNTY LENGTH	0.55	MILES

UTILITY NOTE

THE LOCATIONS OF THOSE BURIED AND ABOVE GROUND UTILITIES SHOWN ARE APPROXIMATE, ARE SHOWN FOR CONTRACTOR INFORMATIONAL USE ONLY, AND ARE NOT TO BE REFERENCED FOR CONSTRUCTION PURPOSES. THE IMPLIED PRESENCE OR ABSENCE OF UTILITIES IS NOT TO BE CONSTRUED BY THE OWNER, ENGINEER, CONTRACTOR, OR SUBCONTRACTORS TO BE AN ACCURATE AND COMPLETE REPRESENTATION OF UTILITIES THAT MAY OR MAY NOT EXIST ON THE CONSTRUCTION SITE. BURIED AND ABOVE GROUND UTILITY LOCATION, IDENTIFICATION, AND MARKING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. REROUTING, DISCONNECTION, PROTECTION, ETC. OF ANY UTILITY MUST BE COORDINATED BETWEEN THE CONTRACTOR, UTILITY COMPANY AND OWNER. SITE SAFETY, INCLUDING THE AVOIDANCE OF HAZARDS ASSOCIATED WITH BURIED AND ABOVEGROUND UTILITIES, REMAINS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

MSD CERTIFICATION

THE EXISTING SEWER LENGTHS, SIZES, FLOWLINES, DEPTHS OF STRUCTURES AND SEWERS AND LOCATIONS WITH RESPECT TO EXISTING OR PROPOSED EASEMENTS HAVE BEEN MEASURED. THE RESULTS OF THOSE MEASUREMENTS ARE SHOWN ON THIS SET OF FINAL MEASUREMENT PLANS. SINCE THE WYE LOCATIONS HAVE BEEN PLOTTED FROM INFORMATION PROVIDED BY THE SEWER CONTRACTOR OR OTHER SOURCES, I DISCLAIM ANY RESPONSIBILITY FOR THAT SPECIFIC INFORMATION.

ALL PUBLIC SEWERS ARE LOCATED WITHIN DESIGNATED EXISTING OR PROPOSED EASEMENTS EXCEPT AS FOLLOWS:

SIGNED P.E. _____

DATE _____

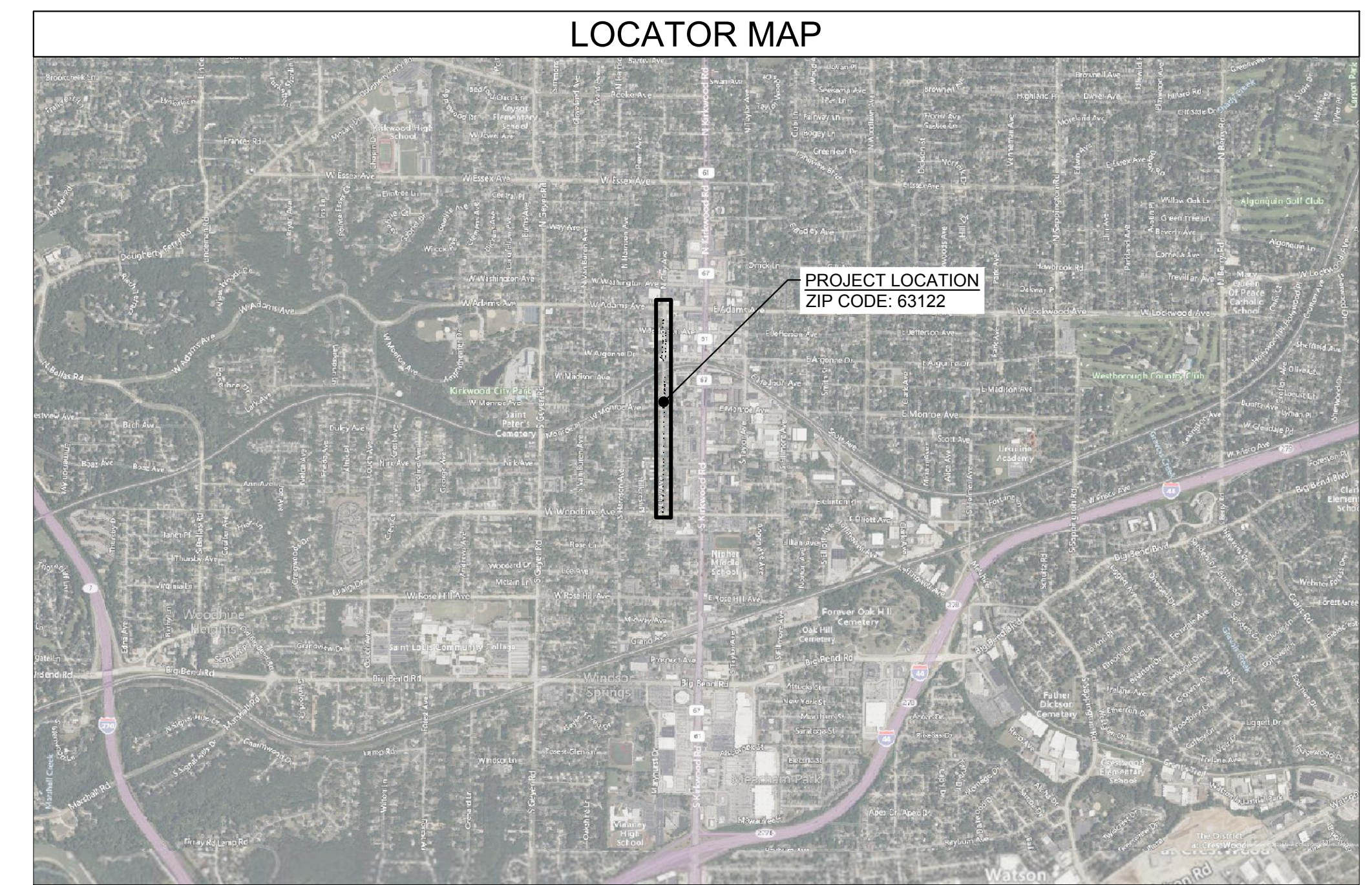
IT SHOULD BE UNDERSTOOD THAT THIS SIGNATURE AND SEAL APPLY ONLY TO THE AFOREMENTIONED FINAL MEASUREMENTS

M.S.D. P-NUMBER: _____

M.S.D. BASE MAP: _____

REVIEW ENGINEER: _____

APPROVAL DATE: _____



In Missouri, we as PE's are required to seal each sheet that we're responsible for. You will need to add this to every sheet. This can be located inside the title block for ease

Use St. Louis Office Info

1717 STATE STREET, SUITE 201
BETTENDORF, IA 52722
663.344.6266
www.imegcorp.com
PROJECT #2300720150

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REFERENCE SCALE IN INCHES
0 1 2 3

I hereby certify that this plan was prepared by me or under my direct personal supervision and that I am a duly licensed Professional Engineer under the laws of the State of Missouri.

Signature _____ Date _____

JAMES W. DIETZEL, Civil Engineer P.E.
License No. 2014023907
My license renewal date is December 31, 2023

GENERAL NOTES

- ALL IMPROVEMENTS SHOWN ON THESE ENGINEERING PLANS SHALL COMPLY WITH THE CITY OF KIRKWOOD DESIGN AND SPECIFICATIONS, LATEST EDITION, AND THE STANDARDS OF THE MISSOURI DEPARTMENT OF NATURAL RESOURCES, LATEST EDITION.
- UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS, RECORDS, AND FIELD INVESTIGATION. THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. IT IS POSSIBLE THERE MAY BE OTHERS, THE EXISTENCE OF WHICH PRESENTLY NOT KNOWN OR SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THEIR EXISTENCE AND EXACT LOCATION AND TO AVOID DAMAGE THERETO.
- ALL DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS SHALL BE PROPERLY DISPOSED OF OFF-SITE UNLESS NOTED.
- THE CONTRACTOR SHALL EXERCISE PROPER CAUTION TO PROTECT THE EXISTING IMPROVEMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING ANY DAMAGE.
- PORTLAND CEMENT CONCRETE. CONCRETE SHALL HAVE THE FOLLOWING PROPERTIES. COMPRESSING STRENGTH SHALL BE 4000PSI IN 28 DAYS. AIR ENTRAINMENT SHALL BE BETWEEN 5% AND 8% AND SLUMP SHALL BE 4 INCHES OR LESS ALL AS MEASURED BY THE APPROPRIATE ASTM METHODS. REINFORCING STEEL SHALL BE ASTM CERTIFIED 60KSI TENSILE STRENGTH. REINFORCING STEEL SHALL BE #4 SIZE UNLESS OTHERWISE SPECIFIED. HANDRAILS, BOLLARDS, AND OTHER APPURTENANCES SHALL BE INSTALLED PER PLAN AND MEET JURISDICTIONAL REQUIREMENTS.
- CLEANUP AND FINAL INSPECTION. WORK BROKEN OR DAMAGED BY CONTRACTOR ACTIVITY SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE. ALL WASTE MATERIAL, CONCRETE WASHOUT, LANDSCAPE WASTE AND BUILDING MATERIAL SHALL BE REMOVED. SOIL SHALL BE REMOVED FROM PAVED AREAS AND THE PROJECT SHALL BE LEFT IN A CLEAN AND WORKMANLIKE MANNER.

GRADING NOTES

- ALL EARTHWORK OPERATIONS SHALL BE IN ACCORDANCE WITH THE GEOTECHNICAL REPORT.
- ALL ELEVATIONS SHOWN ARE TO FLOWLINE FINISHED GRADE OR TOP OF PAVEMENT UNLESS OTHERWISE STATED.
- PROVIDE POSITIVE DRAINAGE AT ALL TIMES WITHIN THE CONSTRUCTION AREAS. DO NOT ALLOW WATER TO POND ON PROPERTY.
- PRIOR TO PLACEMENT OF ANY FILL, THE STRIPPED SITE SHALL BE SCARIFIED TO A DEPTH OF 9 INCHES AND RE-COMPACTED TO 95% DENSITY. ANY UNSUITABLE SOILS FOUND AT THIS TIME SHALL BE DRIED AND RE-COMPACTED OR REMOVED IF REQUIRED COMPACTION CANNOT BE OBTAINED. CUT AREAS SHALL ALSO BE SCARIFIED TO A DEPTH OF 9 INCHES AND RE-COMPACTED TO 95% DENSITY.
- ALL FILL MATERIAL SHALL CONSIST OF APPROVED, SUITABLE SOILS PLACED IN LOOSE LIFTS OF 9 INCHES OR LESS AND COMPACTED TO AT LEAST 95% OF THE MATERIAL'S MAXIMUM STANDARD PROCTOR DRY DENSITY (ASTM D-698) IN ALL PAVEMENT, BUILDING ADDITION AND ATHLETIC FIELD AREAS. THE COMPACTION WILL BE FIELD TESTED BY A SOILS ENGINEERING CONSULTANT REPRESENTING THE OWNER.
- PROJECT WILL BE COVERED BY A GENERAL PERMIT REGULATING RUNOFF FROM CONSTRUCTION SITES. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PERFORM THE REQUIRED MONITORING, INSPECTION AND MAINTENANCE AS REQUIRED BY THE PERMIT.
- ALL DISTURBED EMBANKMENTS GREATER THAN 3:1 SLOPES SHALL BE SEEDED ACCORDING TO A RECOMMENDED SEEDING MIX BY THE LANDSCAPER AND COVERED WITH EROSION CONTROL BLANKETS OR AS DIRECTED BY PLAN DOCUMENTS.
- CONTRACTOR SHALL ADHERE TO THE CITY OF KIRKWOOD WATER CONTROL AND LAND DISTURBANCE CONTROL REGULATIONS AND THE STATE OF MISSOURI CONSTRUCTION SITE EROSION CONTROL MANUAL.
- ALL AREAS TO BE GREENSPACE AT PROJECT COMPLETION SHALL BE LEFT WITH 6 INCHES OF TOPSOIL WHEN MASS GRADING ACTIVITIES ARE COMPLETE.

CONTACTS

CITY OF KIRKWOOD WATER
212 S. TAYLOR AVE
KIRKWOOD, MO 63122
314.984.5925

METROPOLITAN ST. LOUIS SEWER DISTRICT
2350 MARKET STREET
ST. LOUIS, MO 63103
314.768.6262

CITY OF KIRKWOOD ELECTRIC
212 S. TAYLOR AVE
KIRKWOOD, MO 63122
314.984.5912

SPIRE GAS
4118 SHREWBERRY AVE
ST. LOUIS, MO 63119
314.706.2399

LUMEN FIBEROPTIC
877.366.8344

ATT DISTRIBUTIONS
314.275.0020

CHARTER COMMUNICATIONS
636.232.1398

MCI TELECOMMUNICATIONS
800.289-3427



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EROSION CONTROL NOTES

- EROSION CONTROL SHALL BE INSTALLED PRIOR TO ANY GRADING OPERATIONS WHERE POSSIBLE.
- CONSTRUCTION ENTRANCE SHALL BE MAINTAINED TO PREVENT OFF-SITE TRACKING OF SEDIMENT ONTO PUBLIC ROADWAYS. ANY SEDIMENT DEPOSITED ON PUBLIC ROADS SHALL BE REMOVED BY SHOVELING OR STREET CLEANING BEFORE THE END OF EACH WORKING DAY.
- SHOWN LOCATION OF SILTATION CONTROL IS APPROXIMATE. ACTUAL LOCATIONS TO BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION.
- WATER PUMPED DURING CONSTRUCTION OPERATIONS SHALL BE FILTERED.
- ONCE CONSTRUCTION HAS BEEN COMPLETED, OR TEMPORARILY SUSPENDED FOR LONGER THAN 14 DAYS (SUCH AS WINTER SHUTDOWN), THE CONTRACTOR SHALL INITIATE SEEDING ON ALL AREAS DISTURBED IMMEDIATELY OF THE LAST DISTURBANCE. EROSION CONTROL DEVICES SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE CONTRACTOR ESTABLISHES A GOOD STAND OF GRASS OF UNIFORM COLOR AND DENSITY TO THE SATISFACTION OF THE ENGINEER.
- CONTRACTOR SHALL ADHERE TO THE MISSOURI CONSTRUCTION SITE EROSION CONTROL MANUAL.
- ALL EROSION CONTROL MEASURES MUST BE INSTALLED (WHERE POSSIBLE) PRIOR TO THE COMMENCEMENT OF ANY EARTH DISTURBING OPERATIONS. THE REMAINING EROSION CONTROL MEASURES SHALL BE INSTALLED AS SOON AS REASONABLY POSSIBLE AFTER GRADING OPERATIONS BEGIN. WHERE THE PRESENCE OF SILT FENCE WILL INTERFERE WITH ACTIVITIES, DIVERSION DITCHES AND SMALL TEMPORARY SEDIMENT TRAPS SHALL BE UTILIZED UNTIL SILT FENCE OR OTHER MEASURES MAY BE INSTALLED AND VEGETATION ESTABLISHED.
- EROSION CONTROL MEASURES SHALL BE INSPECTED WEEKLY AND AFTER EACH PRECIPITATION EVENT AND REPLACED OR REPAIRED AS NECESSARY.
- SILT FENCE AND SEDIMENT BASIN SHALL BE CLEANED OR REPLACED WHEN SILT BUILDS UP TO WITHIN ONE FOOT OF THE TOP OF THE SILT FENCE.
- PROJECT WILL BE COVERED BY A GENERAL PERMIT REGULATING RUNOFF FROM CONSTRUCTION SITES. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PERFORM THE REQUIRED MONITORING, INSPECTION AND MAINTENANCE AS REQUIRED BY THE PERMIT.
- CONCRETE WASHOUT DEBRIS SHOULD BE HAULED OFF-SITE. WASHOUT SHOULD BE FILLED IN AND SEEDED.
- ALL AREAS DISTURBED BEYOND LIMITS SHOWN SHOULD BE SEEDED WITH ADJACENT SEED MIXTURE OR IN-KIND.
- THERE ARE NO EXPECTED DOWNSTREAM IMPACTS OTHER THAN THOSE ALLOWED PER ORDINANCE (2 YEAR, PRE-DEVELOPED RATE OF RELEASE)

UTILITY NOTES

- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING UTILITIES AND PAVED STREETS, INCLUDING ANY NOT SHOWN ON THESE DRAWINGS. THE CONTRACTOR SHALL VERIFY ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER IF ANY CONFLICTS WITH THE DRAWINGS OCCUR. ANY DAMAGE TO EXISTING UTILITIES AND/OR PAVED STREETS CAUSED BY TRENCHING AND GRADING OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. EXISTING UTILITY LOCATIONS SHOWN ON THE DRAWINGS ARE APPROXIMATE.
- ALL EXISTING UNDERGROUND UTILITIES SHOWN WERE LOCATED PARTIALLY IN THE FIELD AND PARTIALLY FROM REVIEW OF EXISTING PUBLIC RECORDS. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT EACH UTILITY COMPANY FOR THE FIELD LOCATION OF THEIR EXISTING LINES IN OR NEARBY THE CONSTRUCTION AREA PRIOR TO BEGINNING ANY CONSTRUCTION.
- THE CONTRACTOR SHALL EXERCISE PROPER CAUTION TO PROTECT THE EXISTING IMPROVEMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING ANY DAMAGE.
- THE LOCATIONS OF THOSE BURIED AND ABOVE GROUND UTILITIES SHOWN ARE APPROXIMATE, ARE SHOWN FOR CONTRACTOR INFORMATIONAL USE ONLY, AND ARE NOT TO BE REFERENCED FOR CONSTRUCTION PURPOSES. THE IMPLIED PRESENCE OR ABSENCE OF UTILITIES IS NOT TO BE CONSTRUED BY THE OWNER, ENGINEER, CONTRACTOR, OR SUBCONTRACTORS TO BE AN ACCURATE AND COMPLETE REPRESENTATION OF UTILITIES THAT MAY OR MAY NOT EXIST ON THE CONSTRUCTION SITE. BURIED AND ABOVE GROUND UTILITY LOCATION, IDENTIFICATION, AND MARKING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. REROUTING, DISCONNECTION, PROTECTION, ETC. OF ANY UTILITY MUST BE COORDINATED BETWEEN THE CONTRACTOR, UTILITY COMPANY AND OWNER. SITE SAFETY, INCLUDING THE AVOIDANCE OF HAZARDS ASSOCIATED WITH BURIED AND ABOVEGROUND UTILITIES, REMAINS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- WATER MAIN SHALL BE CONSTRUCTED IN ACCORDANCE WITH LOCAL WATER COMPANY STANDARD SPECIFICATIONS FOR WATER MAIN CONSTRUCTION.
- ALL WATER MAIN SHALL HAVE A MINIMUM COVER OF 5 FEET.
- MAINTAIN 18 INCHES VERTICAL SEPARATION (OUTER EDGE TO OUTER EDGE) BETWEEN WATER MAIN AND SEWER.
- NITRILE GASKETS SHALL BE USED WHERE WATER MAIN CROSSES BELOW STORM SEWER.
- UTILITY PIPING, ALL WORK SHALL BE PERFORMED IN CONFORMANCE WITH THE PIPE MANUFACTURER'S RECOMMENDATIONS FOR INSTALLATION METHODS INCLUDING BACKFILL MATERIAL AND MATERIAL DEPTHS. PIPE MATERIAL SHALL BE AS SPECIFIED ON THE PLANS. ALL MATERIAL SHALL MEET THE REQUIREMENTS OF THE LOCAL JURISDICTION FOR STRENGTH, MATERIAL TYPE AND CONFORMITY WITH THE EXISTING SYSTEM. SEWER LINES SHALL BE CONSTRUCTED STRAIGHT TO THE SPECIFIED LINE AND GRADES. MANHOLES, STORM INLETS, VALVE BOXES AND APPURTENANCES SHALL BE ADJUSTED TO GRADE OR PER PLAN PRIOR TO SEEDING/LANDSCAPING ACTIVITIES.



SITE CONTROL
SCALE: 1"=900.0011

HORIZONTAL CONTROL		
XXX	XXX	XXX
XX	XX	XX

VERTICAL CONTROL		
XXX	XXX	XXX
XX	XX	XX

LEGEND

PROPOSED	EXISTING	
		STORM MANHOLE
		STORM INLET
		STORM DOUBLE INLET
		FLARED END SECTION
		DOWNSPOUT
		SANITARY MANHOLE
		SANITARY/STORM CLEANOUT
		UNKNOWN MANHOLE
		WATER VALVE
		HYDRANT
		WELL
		SPRINKLER BOX
		WATER METER
		WATER SERVICE
		POWER POLE
		POWER POLE W/ LIGHT
		POWER POLE W/ METER
		GUY WIRE
		GUY POLE
		ELECTRIC MANHOLE
		ELECTRIC PEDESTAL/TRANSFORMER
		ELECTRIC METER
		TELEPHONE POLE
		TELEPHONE MANHOLE
		TELEPHONE PEDESTAL
		UTILITY MANHOLE
		HANDHOLE
		GAS VALVE
		LIGHT POLE
		VAPOR LIGHT
		LIGHT JUNCTION BOX
		SIGN
		FLAGPOLE
		POST/BOLLARD
		CONIFER TREE
		DECIDUOUS TREE
		BUSH/SHRUB
		TREE STUMP
		CONTROL POINT
		BENCHMARK
		SOIL BORING HOLE
		R.O.W. MARKER, FOUND
		RAILROAD SPIKE, FOUND
		PIPE, FOUND
		CONCRETE MONUMENT, FOUND
		MEASURED DIMENSION
		RECORDED DIMENSION
		SPOT ELEVATION
		GRADE LABEL
		DRAINAGE SLOPE
		LINE CONTINUATION
		SURVEY BOUNDARY
		PROPERTY LINE
		CENTERLINE
		HISTORICAL LINE - AS NOTED
		EASEMENT LINE
		SECTION LINE
		R.O.W. LINE
		SETBACK LINE
		FORCE MAIN
		SANITARY SEWER
		STORM SEWER
		PIPE UNDERDRAIN
		WATER LINE
		OVERHEAD ELECTRIC
		UNDERGROUND ELECTRIC
		GAS LINE
		TELEPHONE LINE
		UTILITY LINE
		EDGE OF WATER LINE/DITCH FLOWLINE
		CHAIN LINK FENCE
		SILT FENCE
		CONTOUR
		GUARD RAIL
		TREE LINE
		FLOOD PLAIN
		FLOODWAY
		CONSTRUCTION LIMITS

REVISIONS	
DESCRIPTION	DATE
No.	

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177 STATE STREET
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BETTENDORF, IA 52722

SOUTH CLAY AVENUE RESURFACING PROJECT
KIRKWOOD, MISSOURI

NOTES-LEGEND-SITE CONTROL

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File Name:
23001201.00-Notes-Details.dwg

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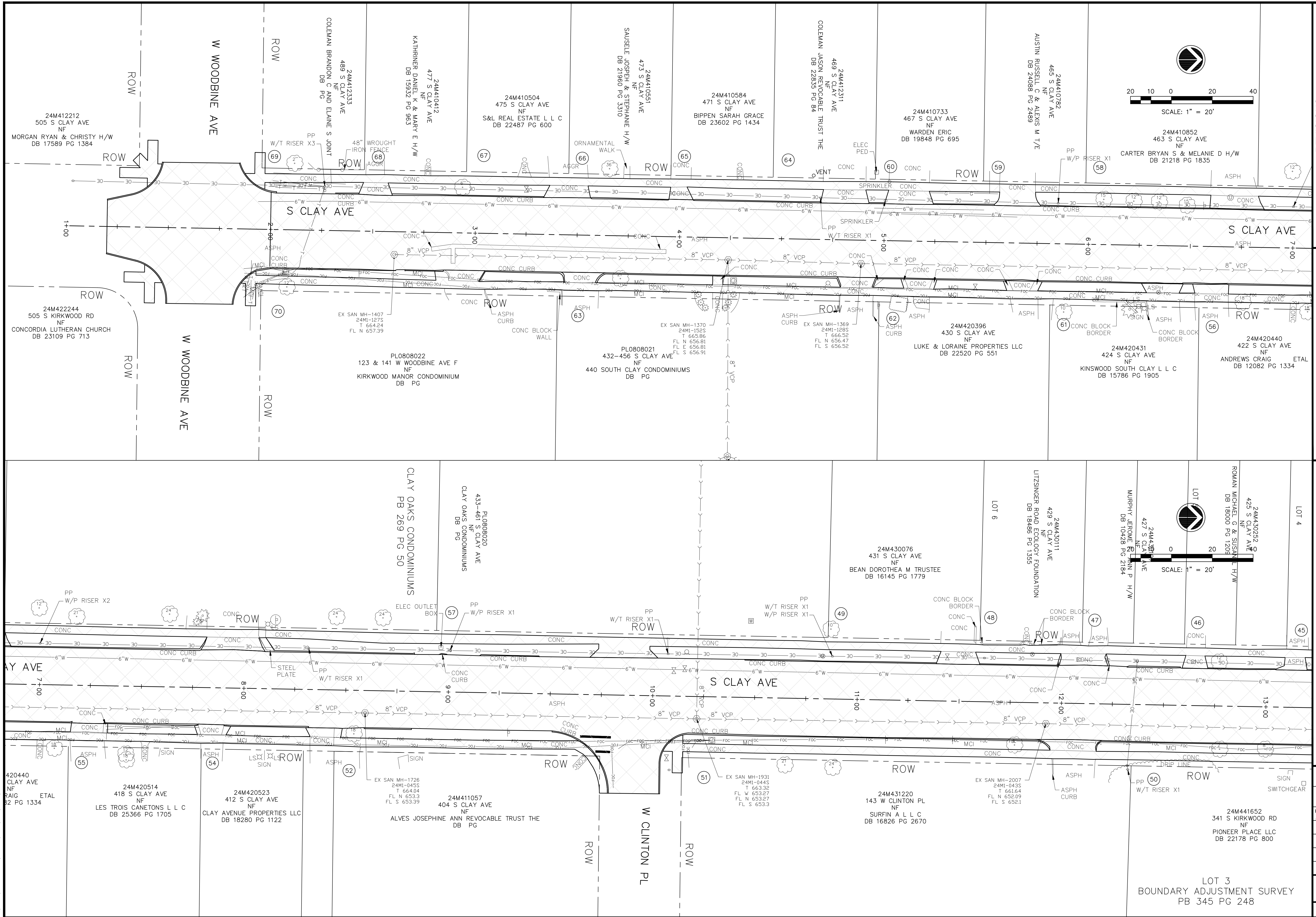
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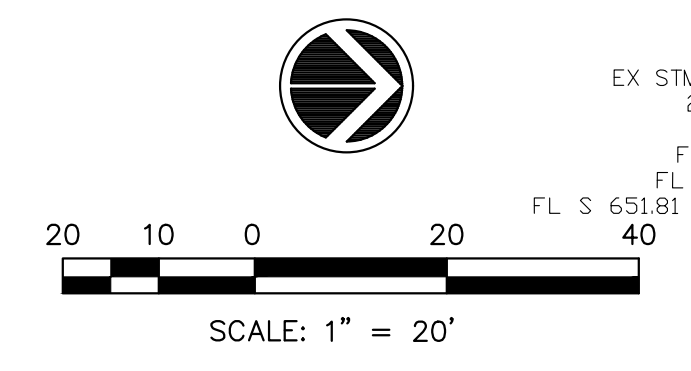
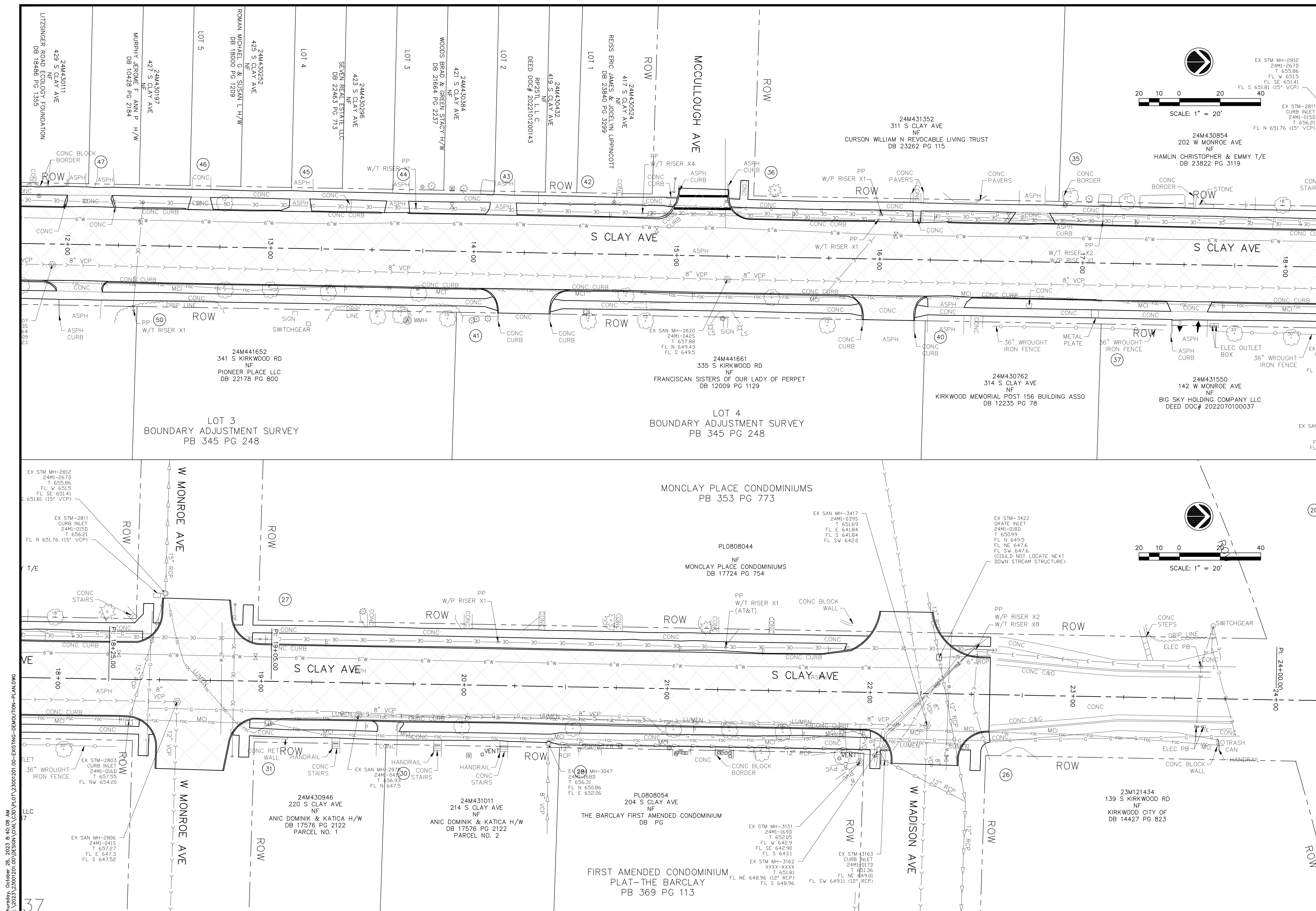
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SOUTH CLAY AVENUE RESURFACING PROJECT
 KIRKWOOD, MISSOURI
EXISTING CONDITIONS AND DEMO PLAN

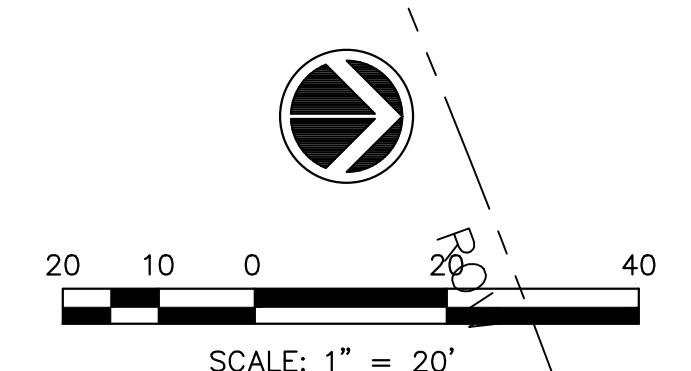
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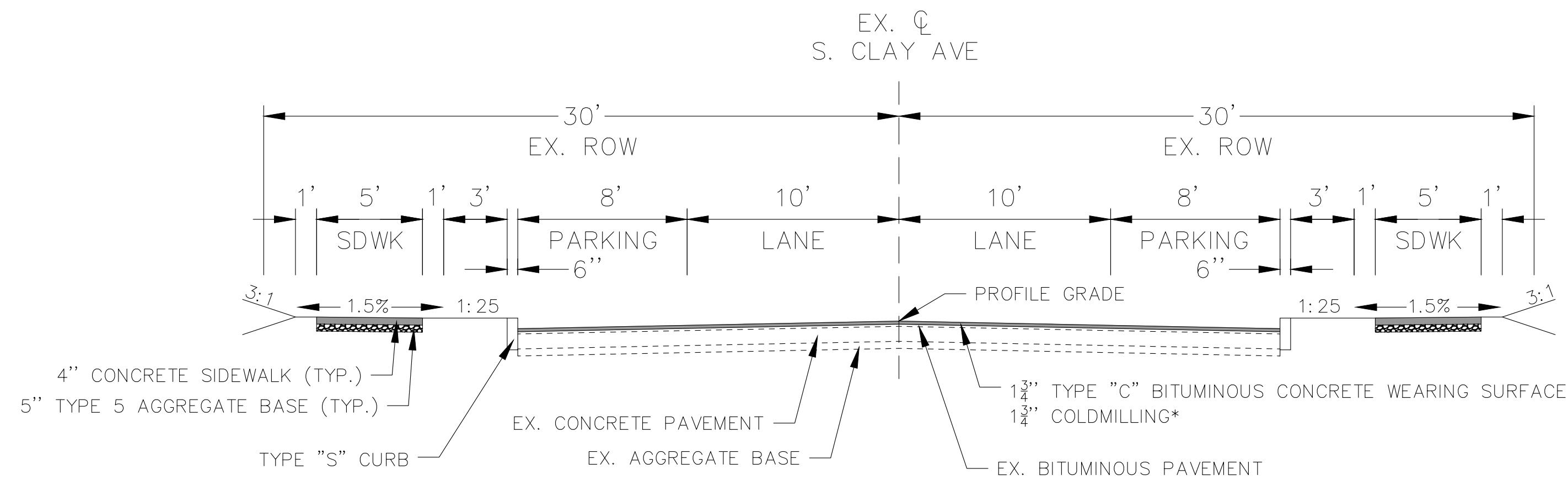


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 KIRKWOOD, MISSOURI

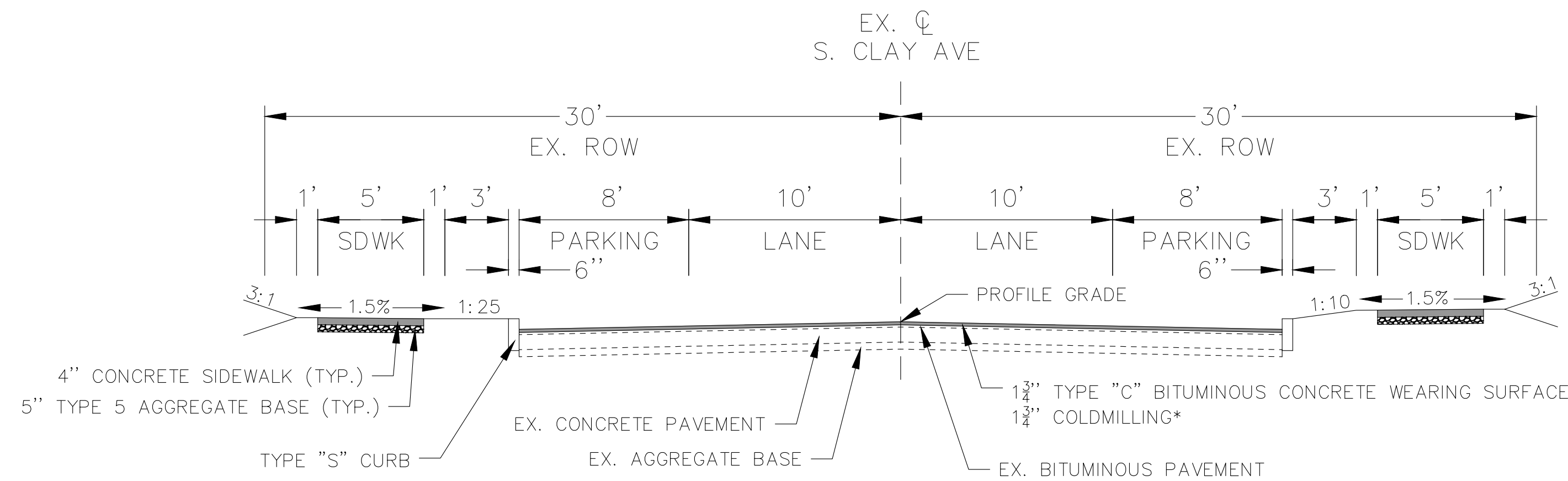
EXISTING CONDITIONS AND DEMO PLAN

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Sheet ---- of ##

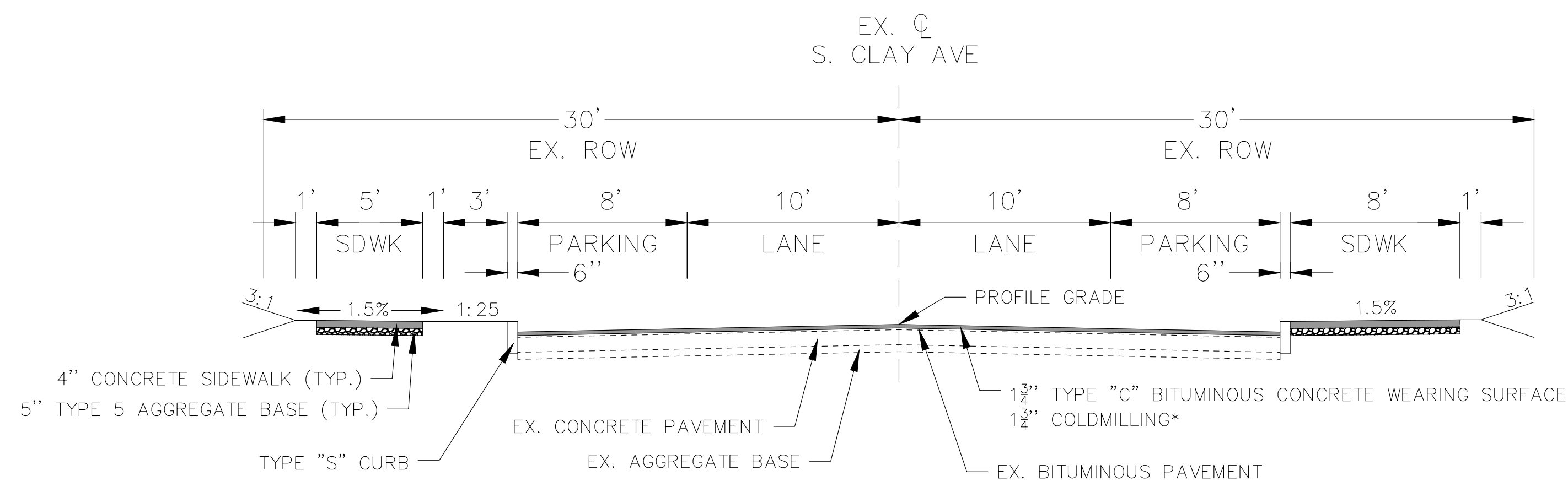
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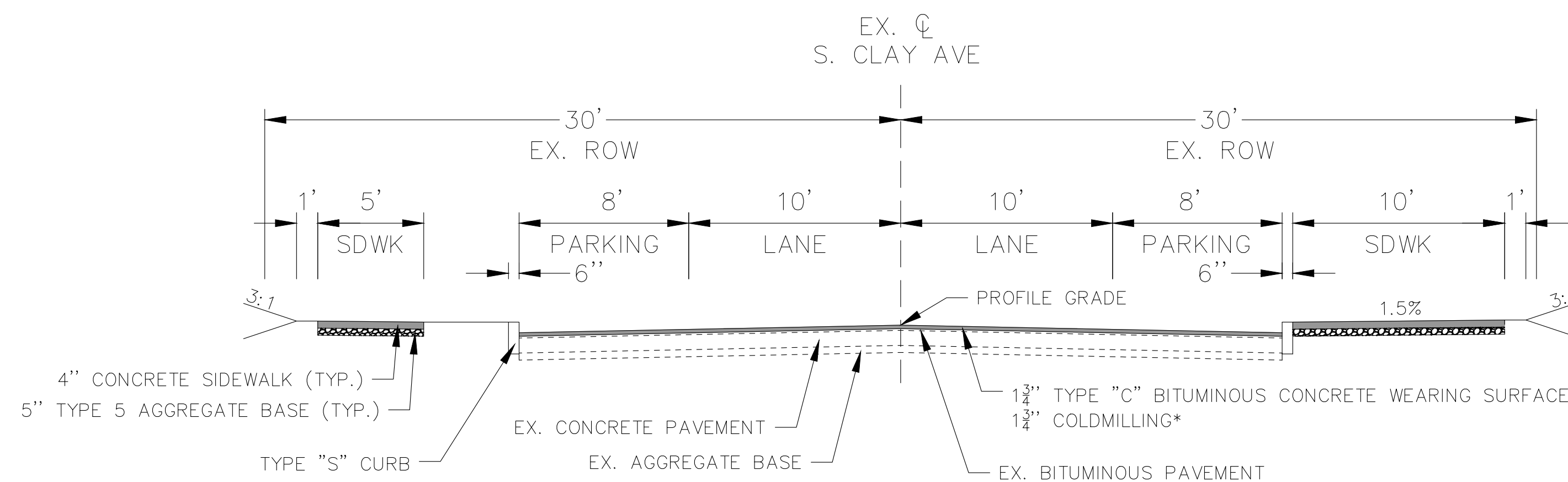
PROPOSED TYPICAL SECTION - S. CLAY AVE
STA 1+98.27 TO STA 9+59.21
SCALE: N.T.S.



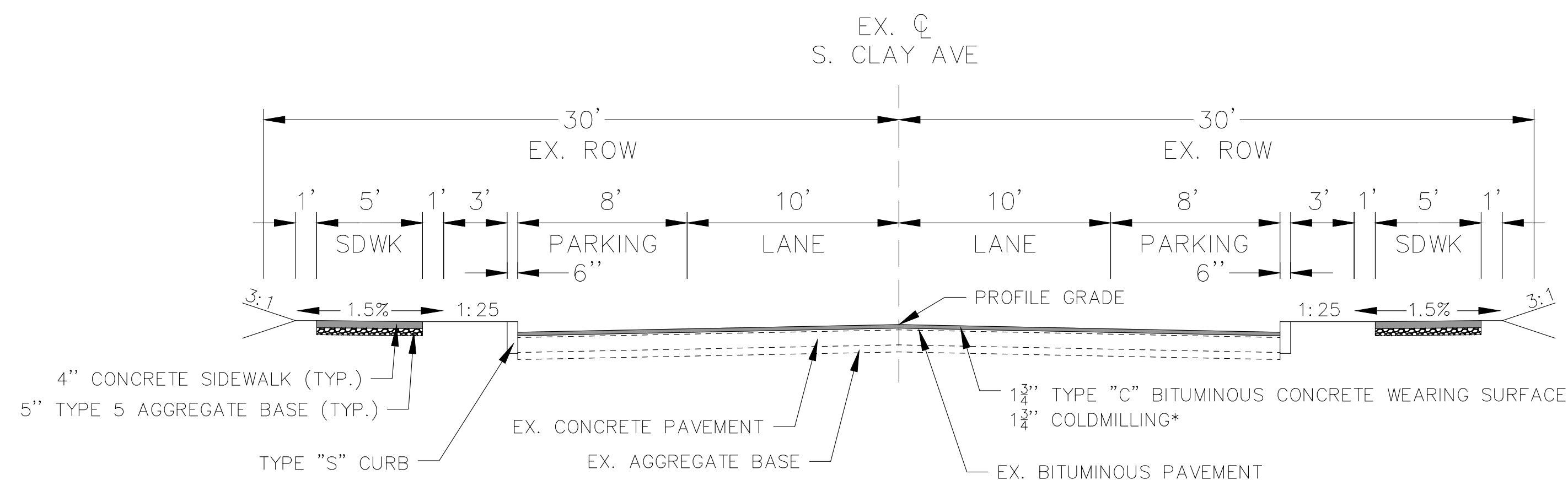
PROPOSED TYPICAL SECTION - S. CLAY AVE
STA 10+20.25 TO STA 21+99.52
SCALE: N.T.S.



PROPOSED TYPICAL SECTION - S. CLAY AVE
STA 27+71.03 TO STA 29+11.55
SCALE: N.T.S.



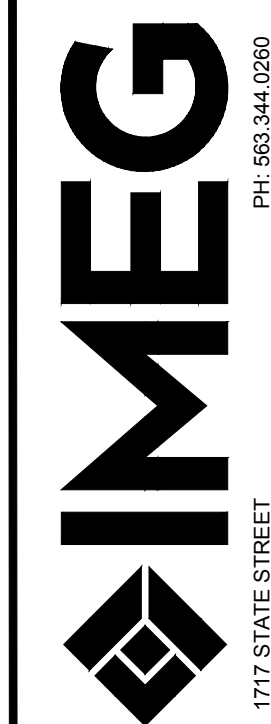
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STA 29+28.61 TO STA 30+78.25
SCALE: N.T.S.



PROPOSED TYPICAL SECTION - S. CLAY AVE
STA 31+36.17 TO STA 34+44.00
SCALE: N.T.S.

*NOTE: COLDMILLING TO NOT EXCEED 2" TO MAINTAIN SMOOTH SURFACE

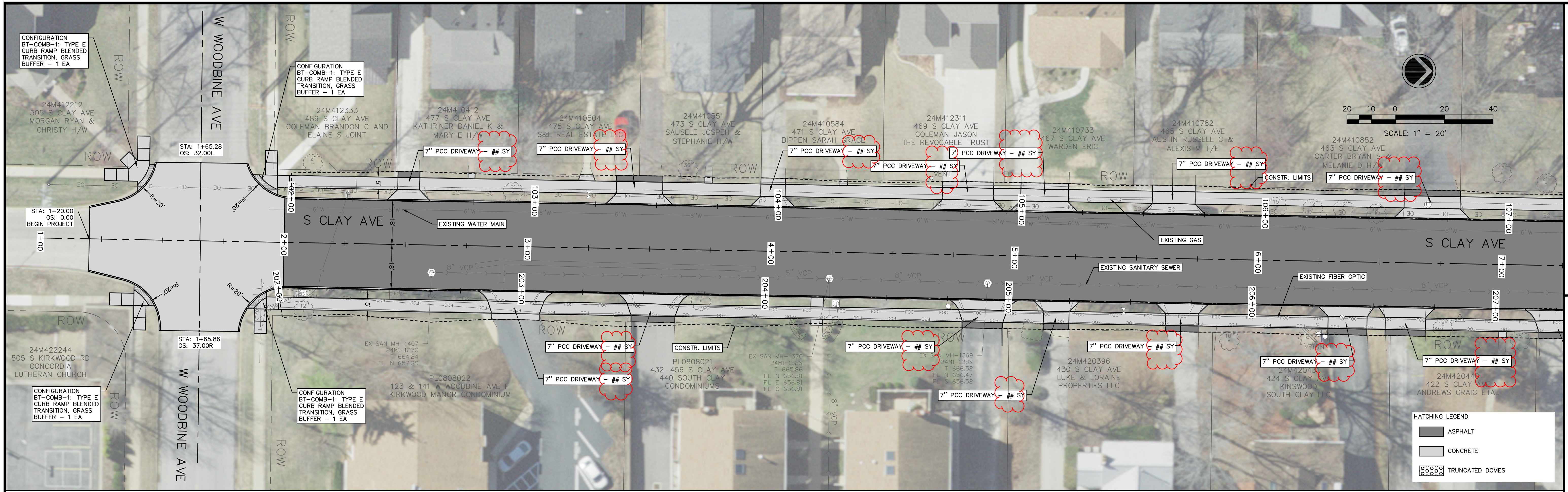
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No.		



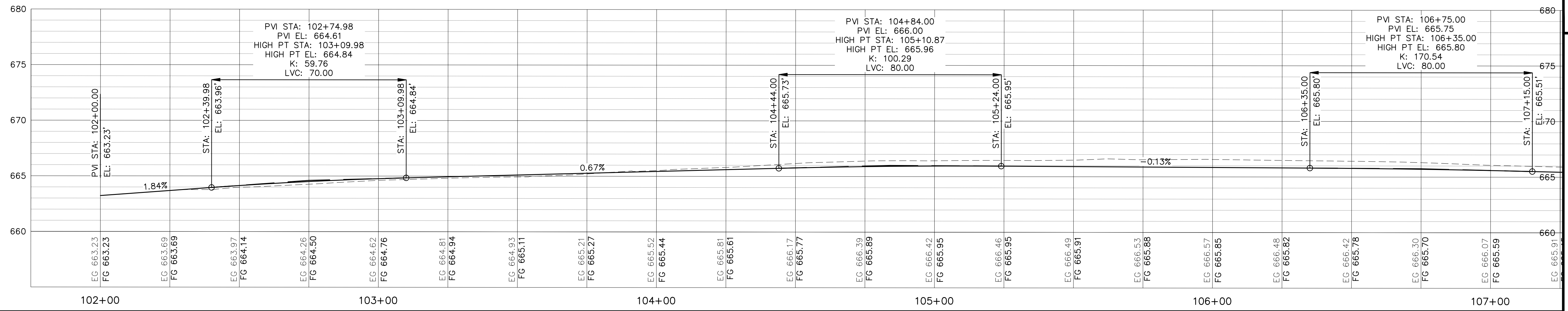
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KIRKWOOD, MISSOURI
TYPICAL SECTIONS

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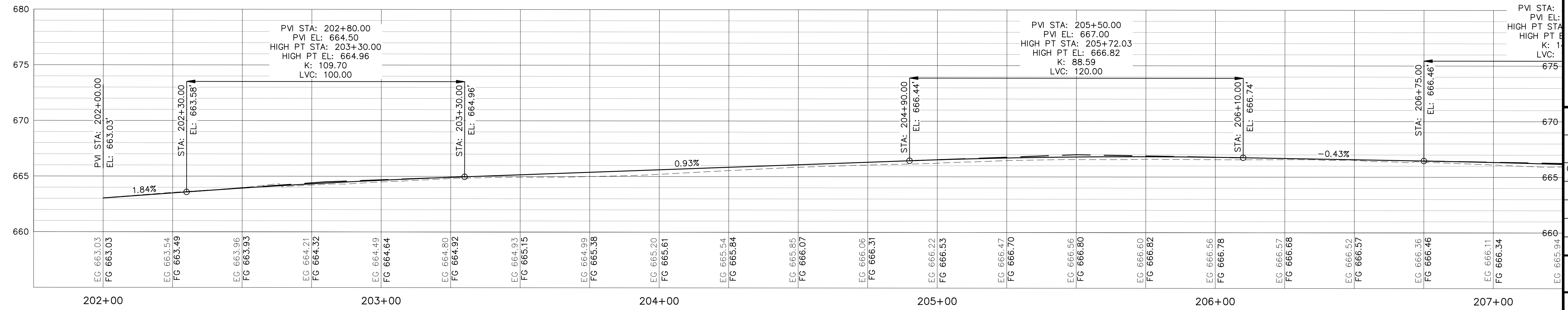
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PROFILE VIEW OF CLAY AVE_WOODBINE AVE TO CLINTON PL LEFT C&G | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'



PROFILE VIEW OF CLAY AVE_WOODBINE AVE TO CLINTON PL RIGHT C&G | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'

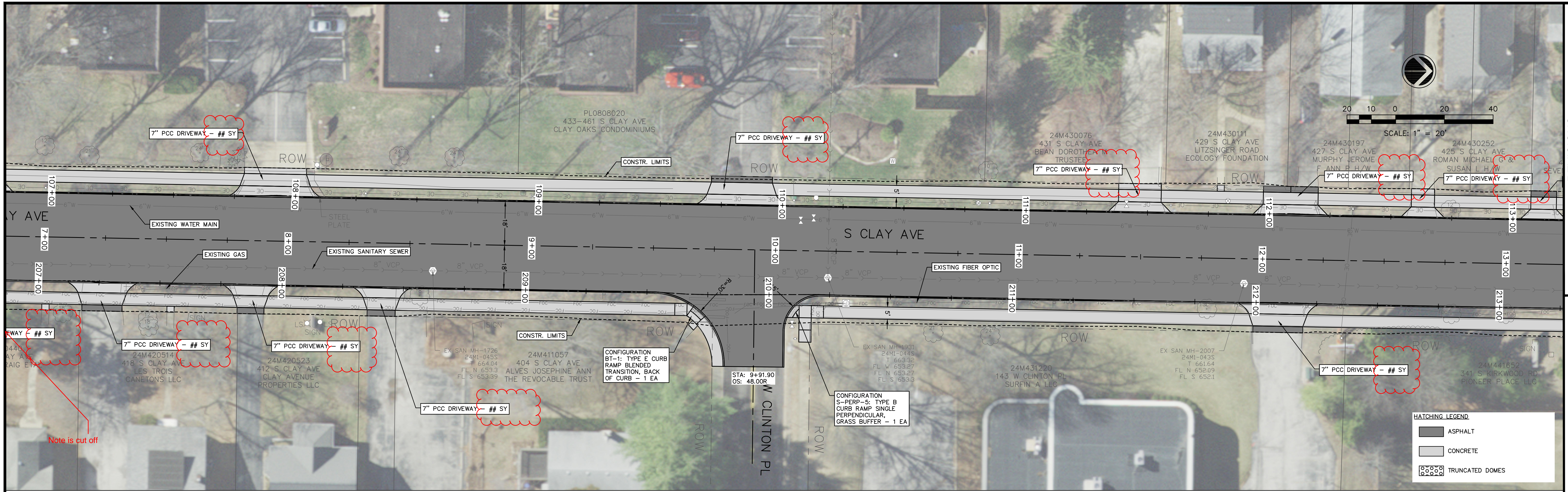


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SOUTH CLAY AVENUE RESURFACING PROJECT
KIRKWOOD, MISSOURI
SOUTH CLAY AVENUE PLAN

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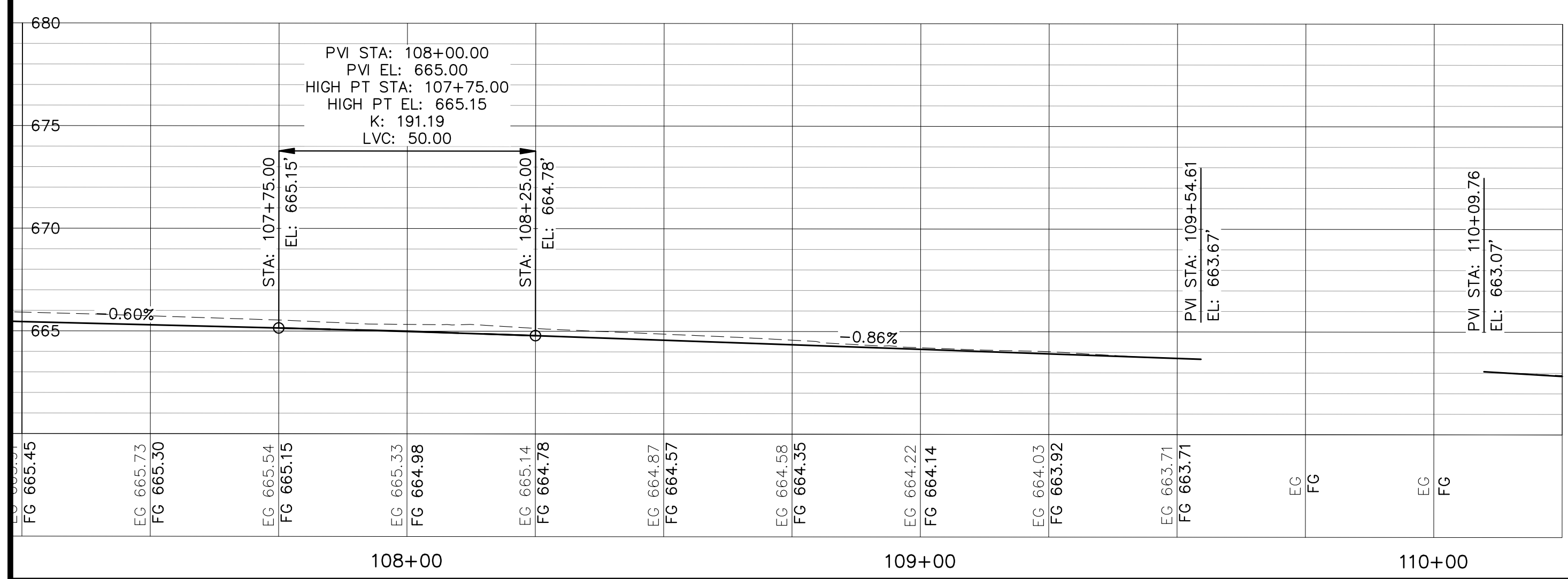
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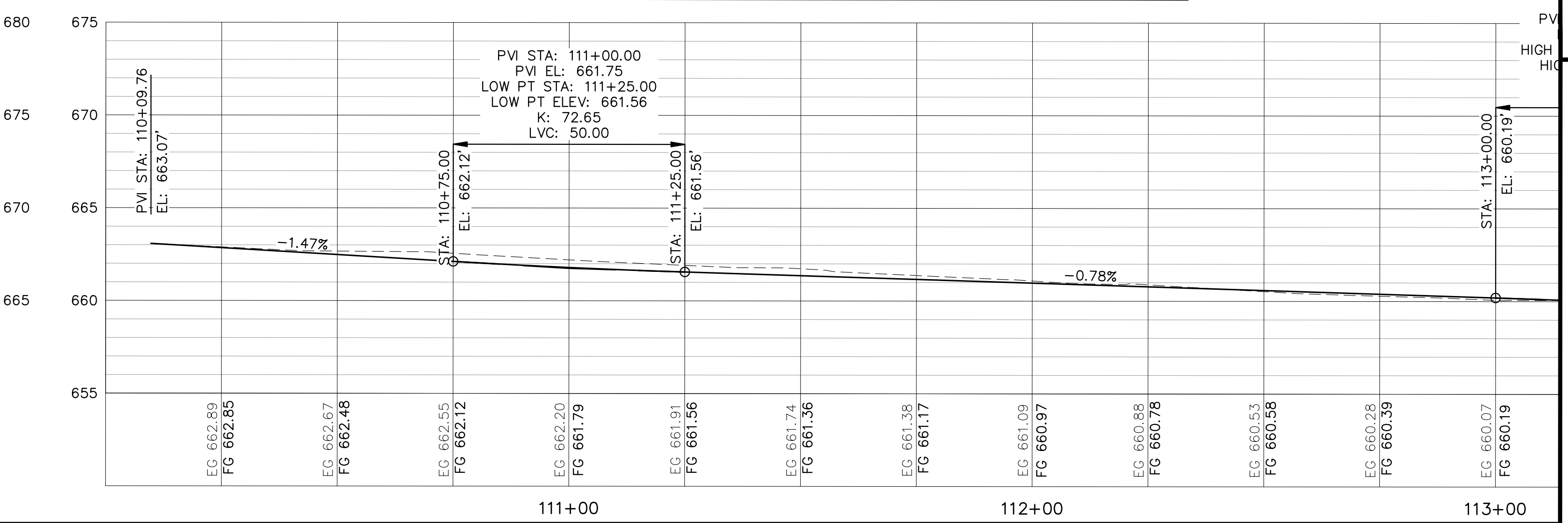
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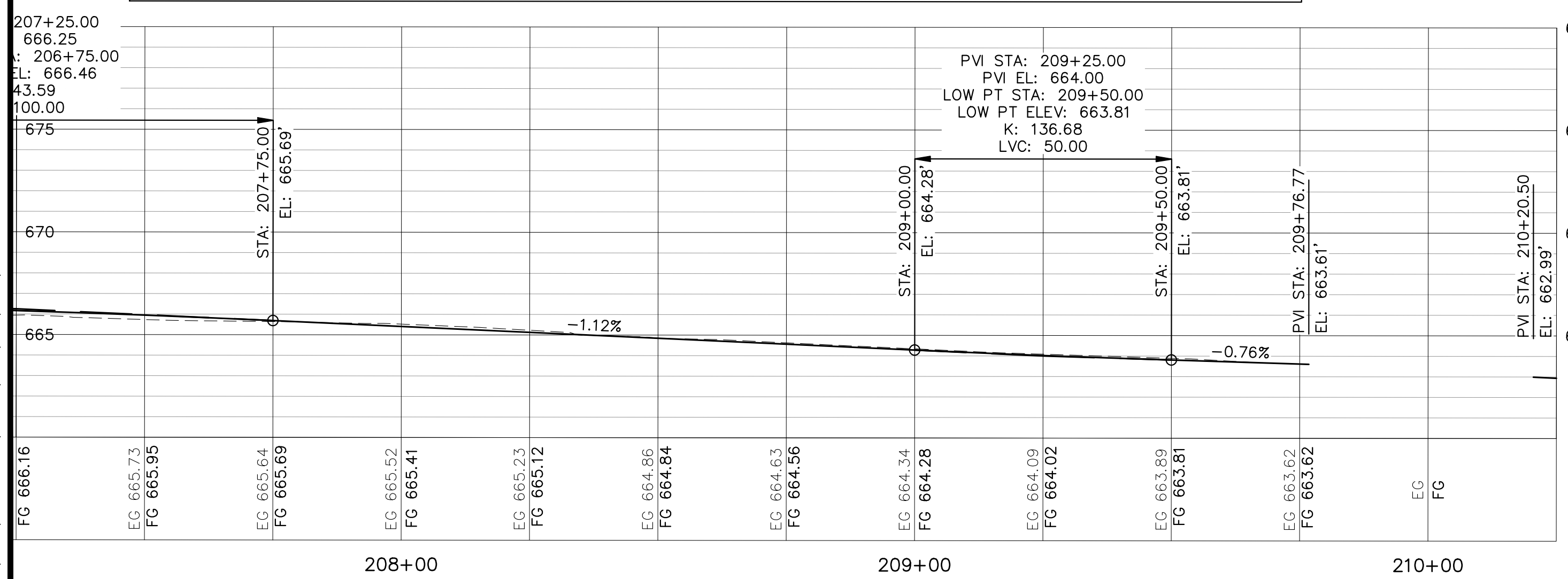
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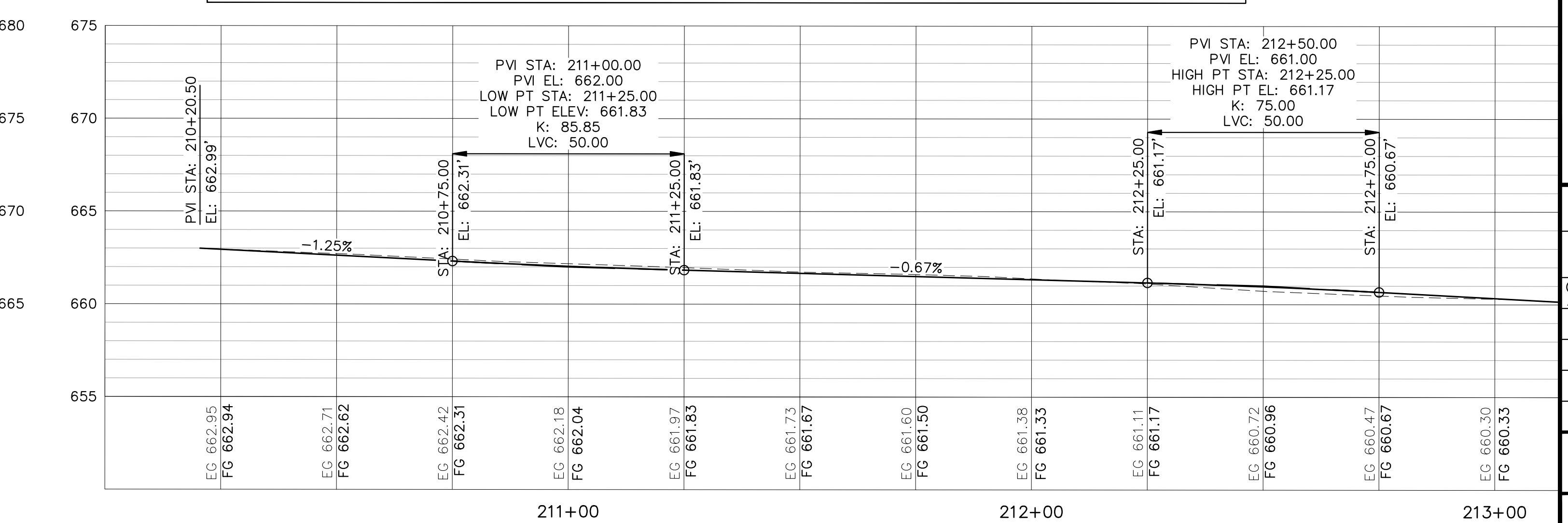
PROFILE VIEW OF CLAY AVE_CLINTON PL TO MCCULLOUGH AVE | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'



PROFILE VIEW OF CLAY AVE_WOODBINE AVE TO CLINTON PL RIGHT C&G | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'



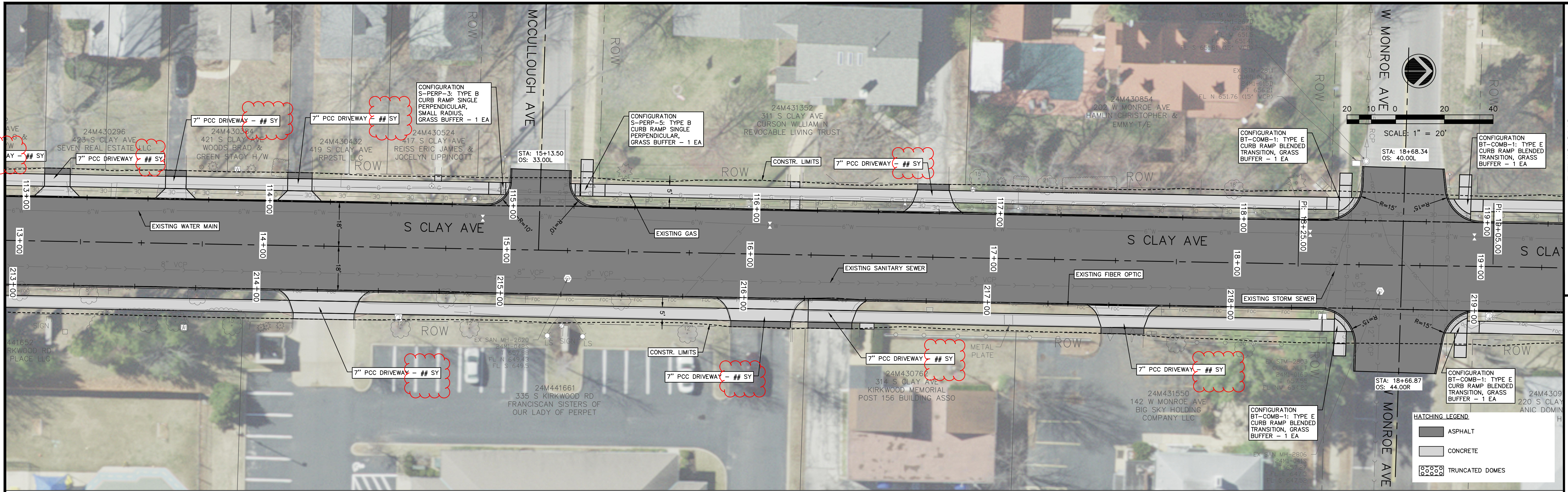
PROFILE VIEW OF CLAY AVE_CLINTON PL TO MONROE AVE RIGHT C&G | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'



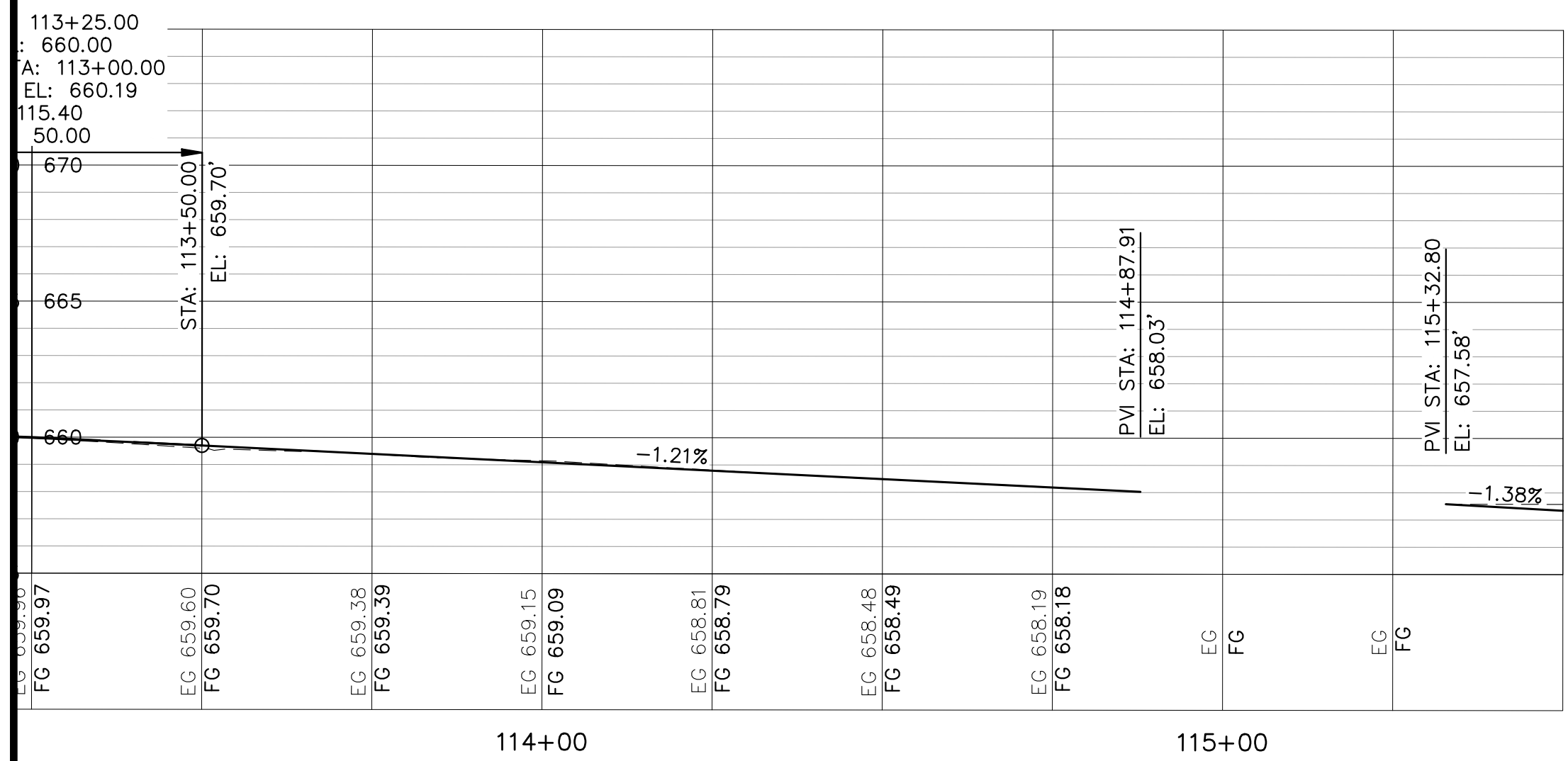
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SOUTH CLAY AVENUE PLAN

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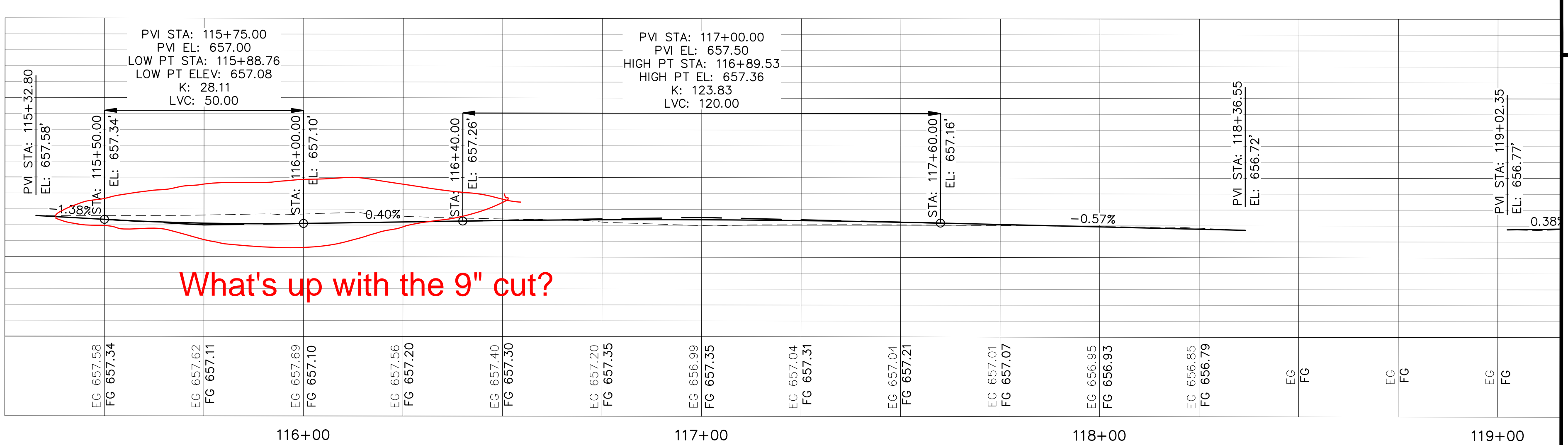
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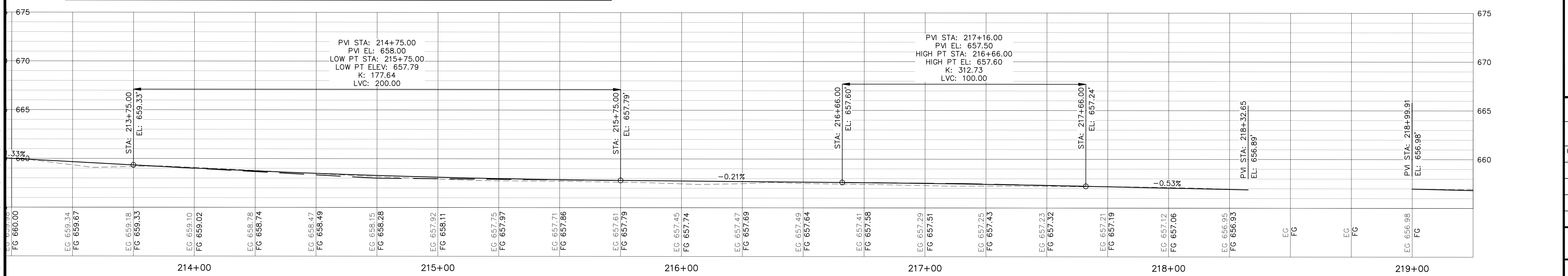
PROFILE VIEW OF CLAY AVE_CLINTON PL TO MCCULLOUGH AVE | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'



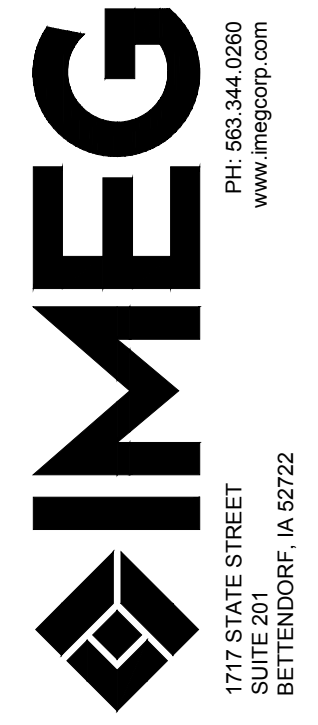
PROFILE VIEW OF CLAY AVE_MCCULLOUGH AVE TO MONROE AVE | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'



PROFILE VIEW OF CLAY AVE_CLINTON PL TO MONROE AVE RIGHT C&G | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'



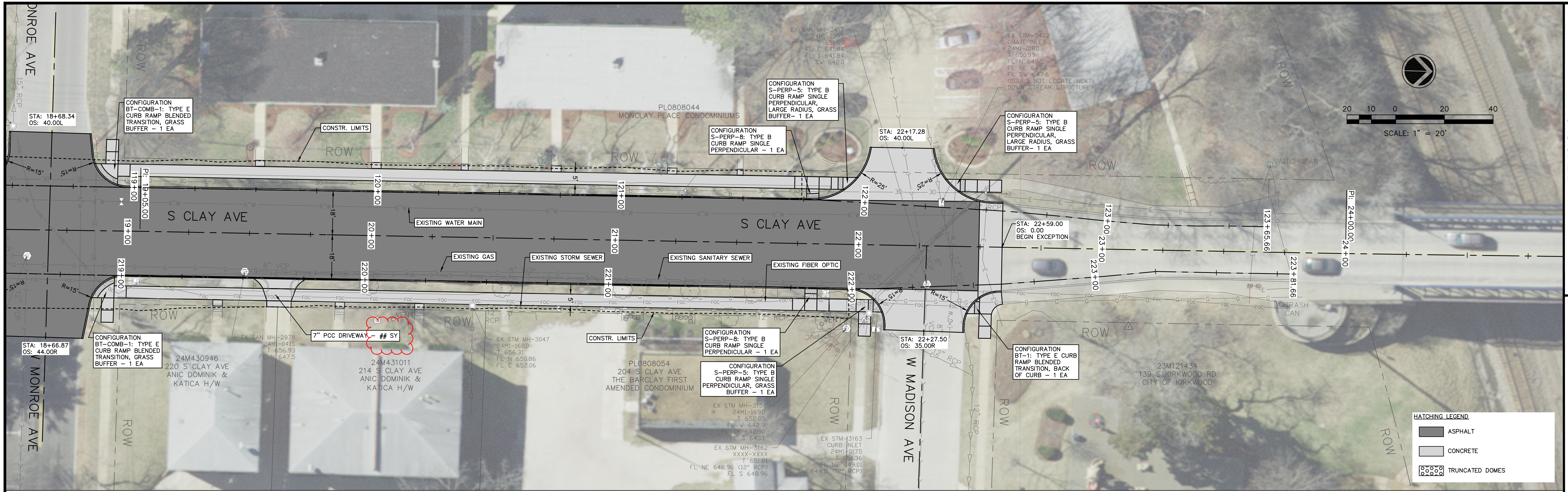
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KIRKWOOD, MISSOURI
SOUTH CLAY AVENUE PLAN

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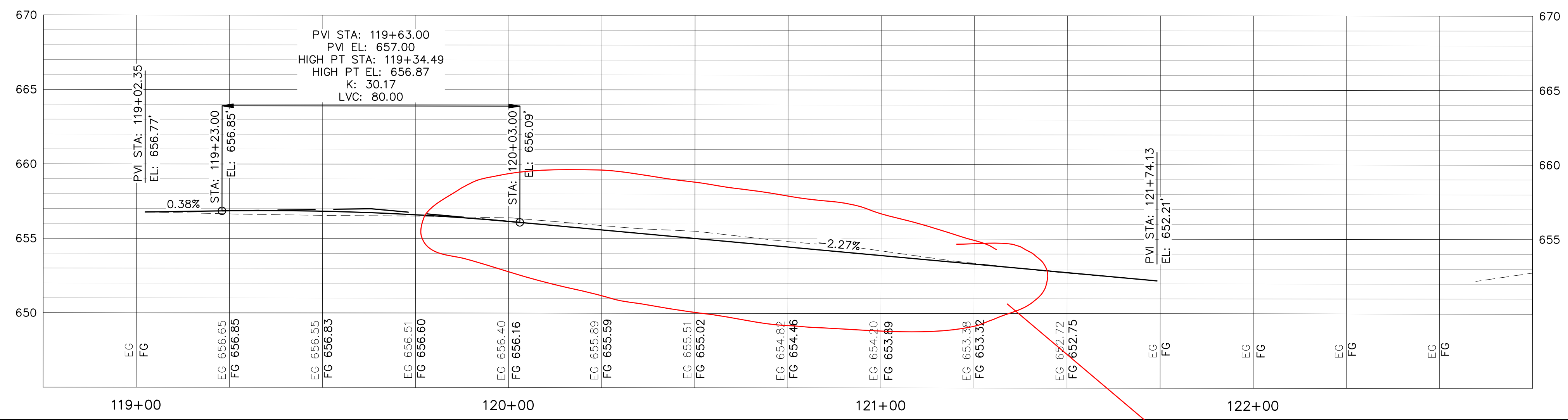
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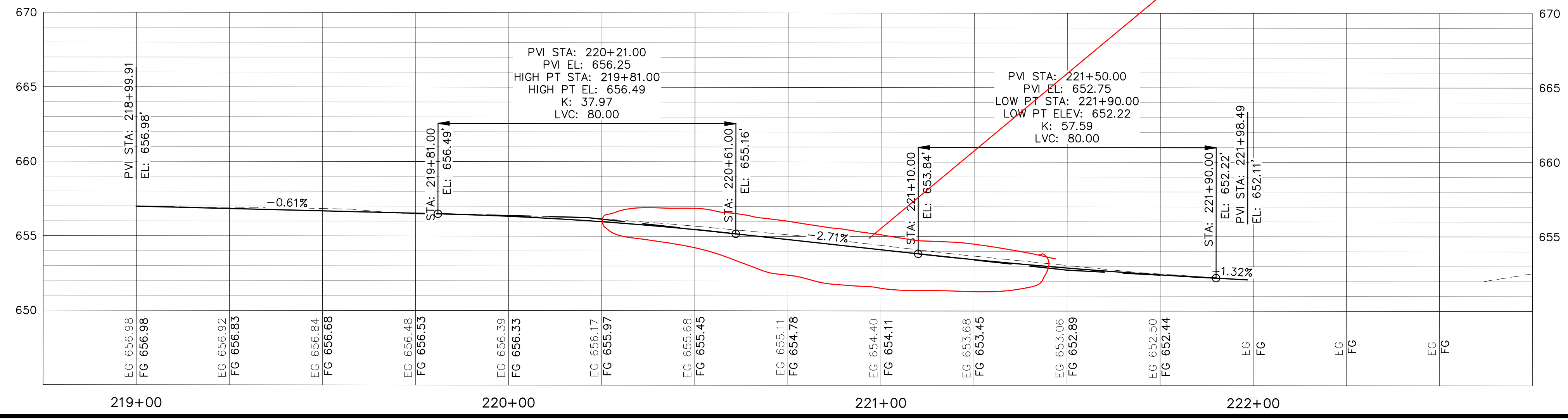
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PROFILE VIEW OF CLAY AVE_MONROE AVE TO MADISON AVE LEFT C&G | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'



PROFILE VIEW OF CLAY AVE_MONROE AVE TO MADISON AVE RIGHT C&G | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'

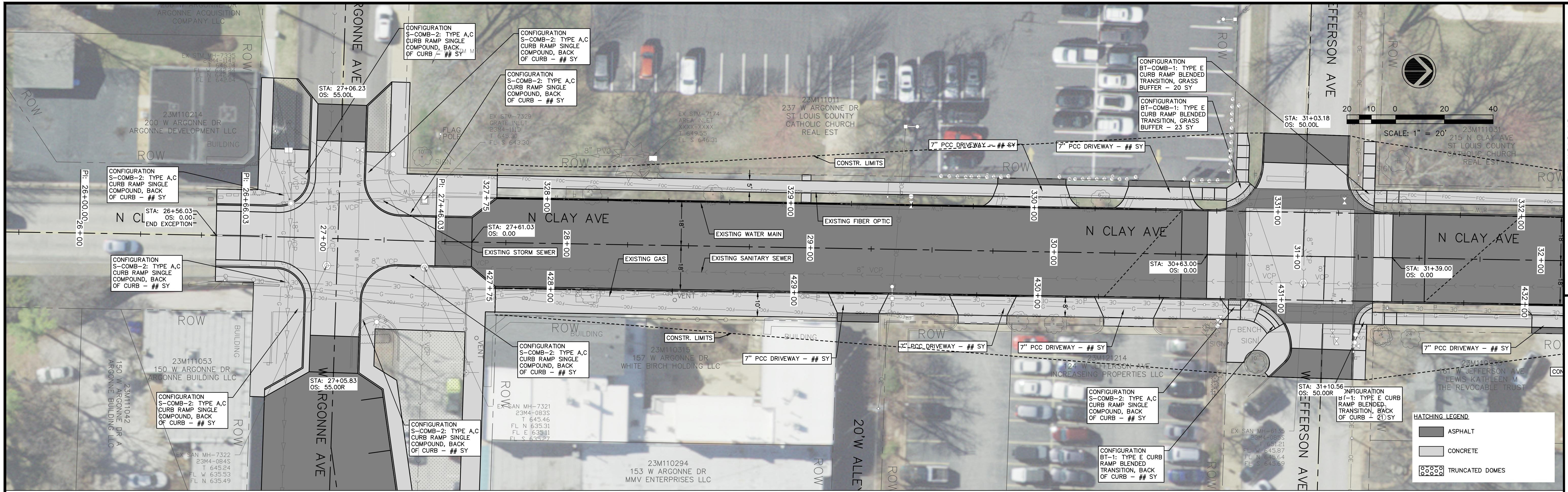


What's going on here with the 6" cut?

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 KIRKWOOD, MISSOURI
SOUTH CLAY AVENUE PLAN

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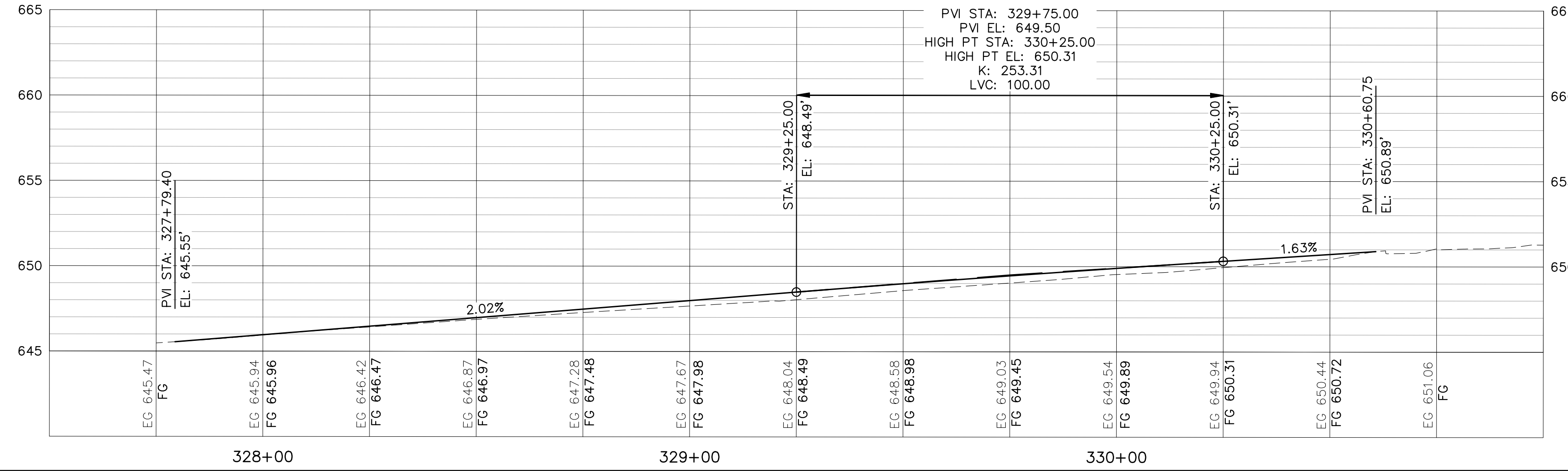
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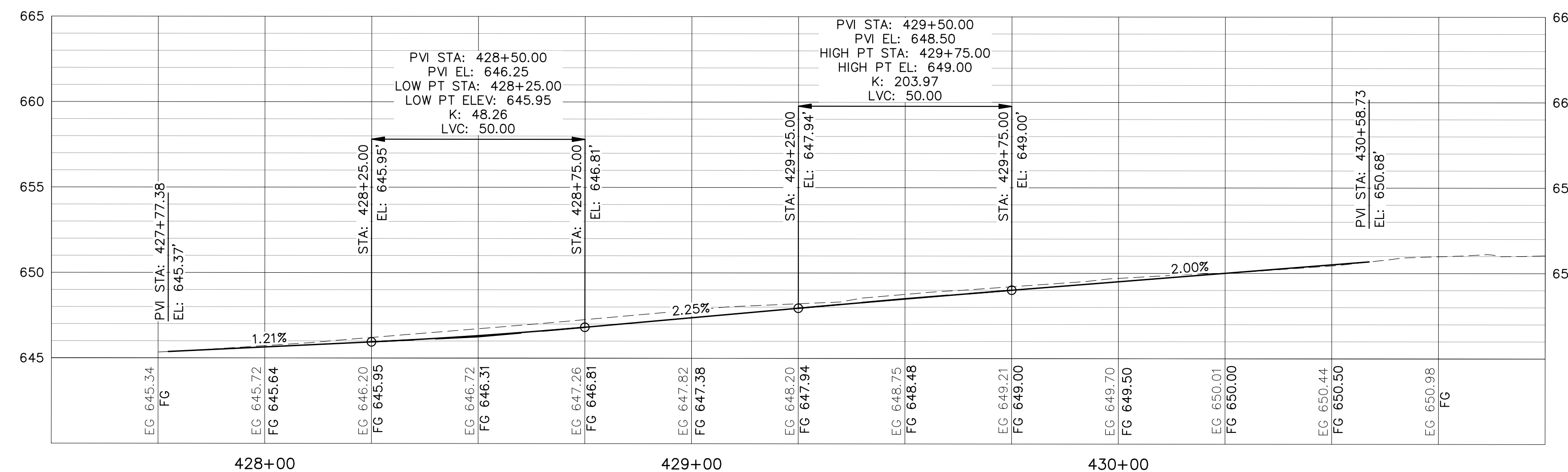
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PROFILE VIEW OF CLAY AVE_ARGONNE AVE TO JEFFERSON AVE LEFT C&G | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'



PROFILE VIEW OF CLAY AVE_ARGONNE AVE TO JEFFERSON AVE RIGHT C&G | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'



Thursday, October 26, 2023 9:26:53 AM G:\2023\23001201.00\DESIGN\CIVIL\C3D\PLOT\23001201.00--PNP.DWG

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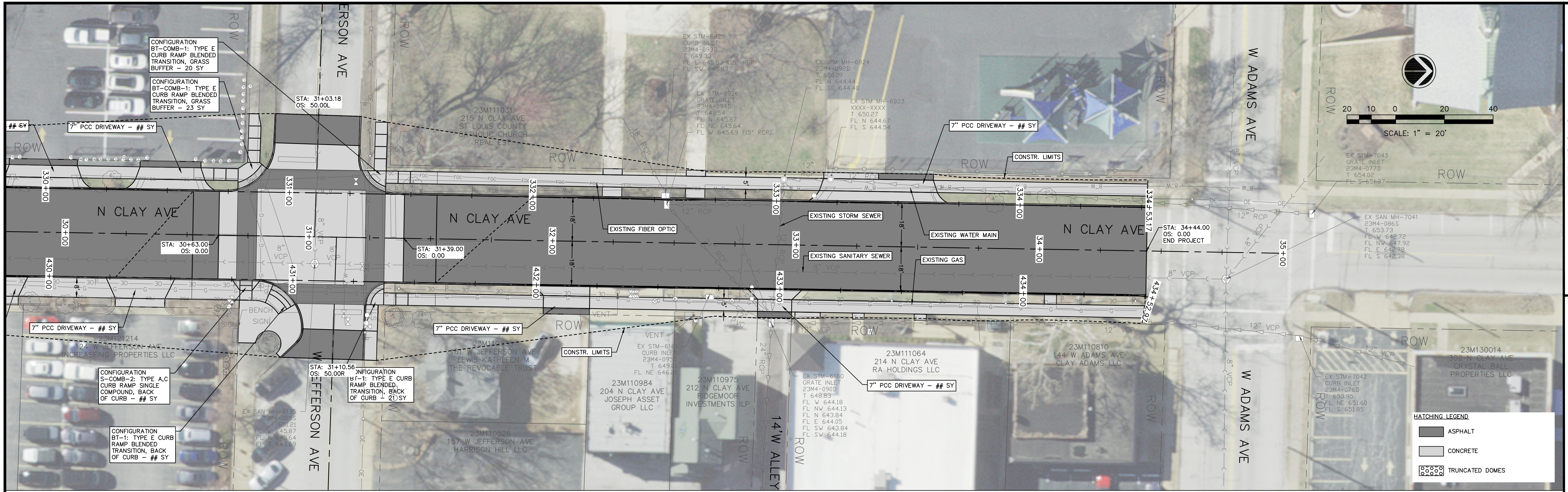
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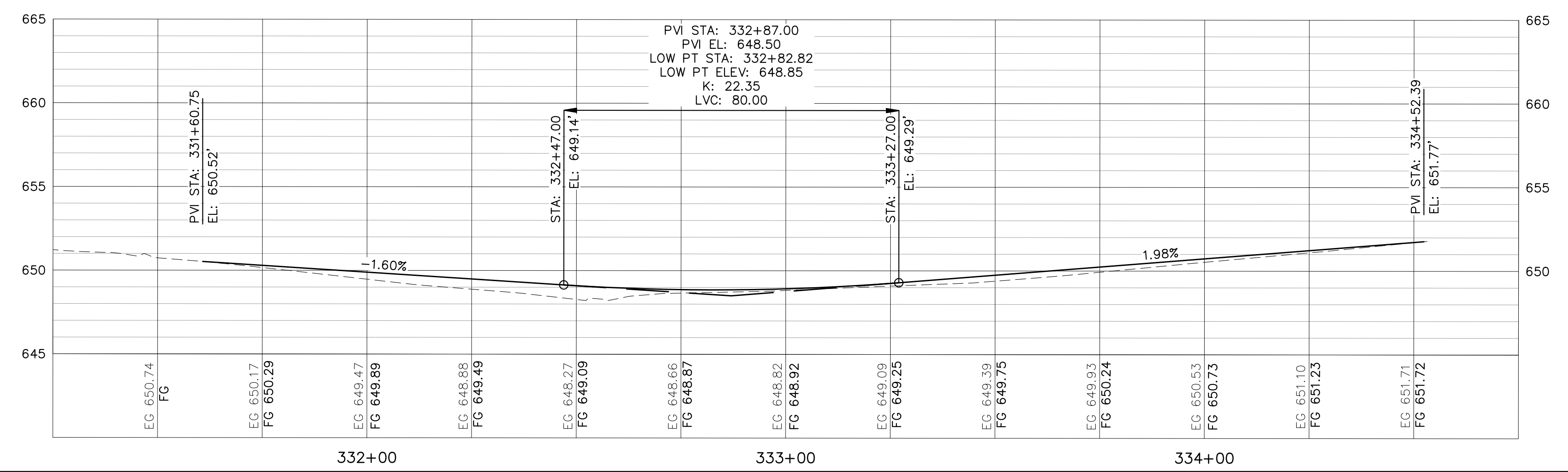
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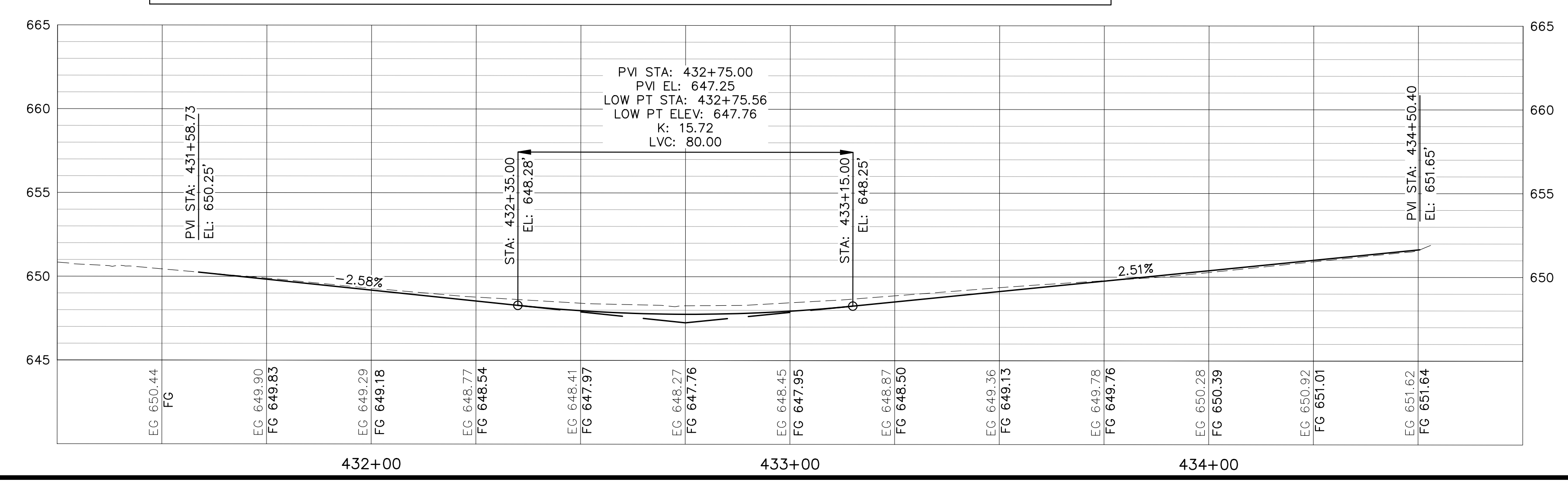
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PROFILE VIEW OF CLAY AVE_JEFFERSON AVE TO ADAMS AVE LEFT C&G | HOR. SCALE: 1" = 10' | VERT. SCALE: 1" = 2'



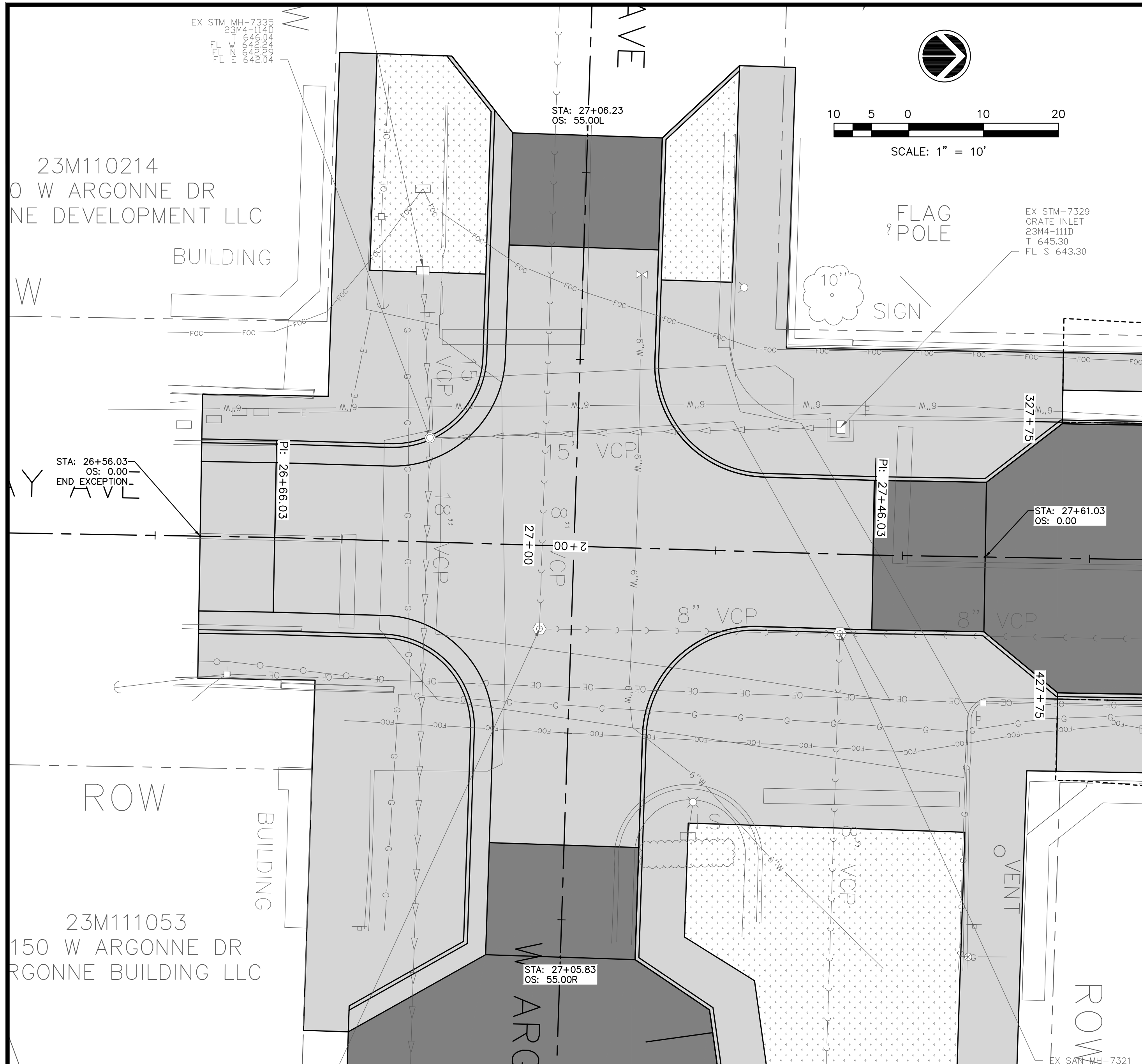
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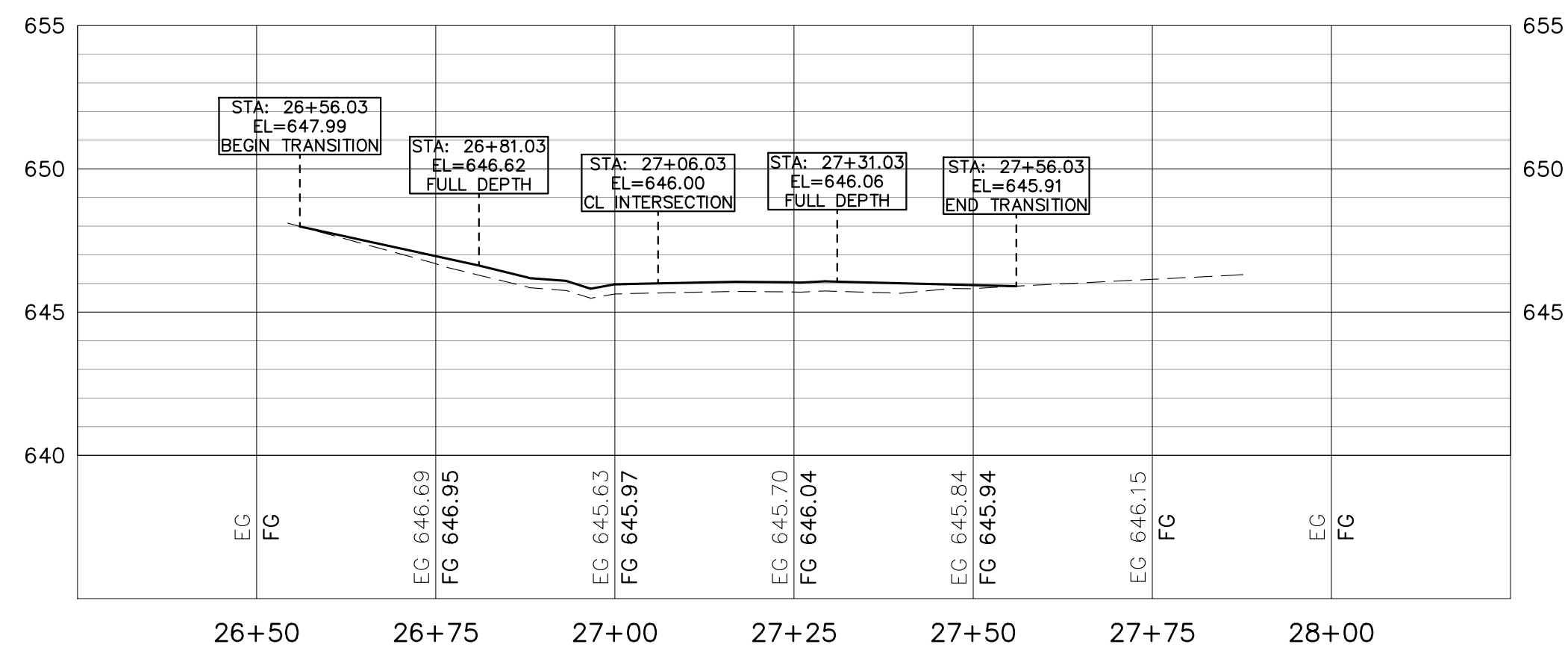
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 KIRKWOOD, MISSOURI
SOUTH CLAY AVENUE PLAN

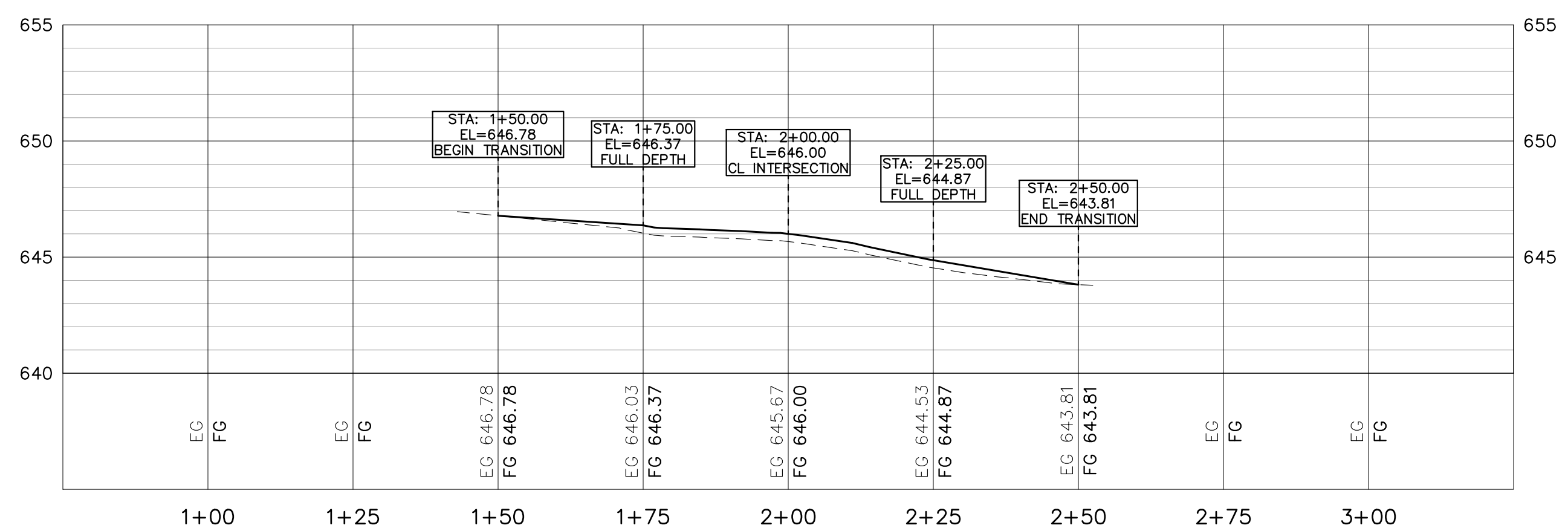
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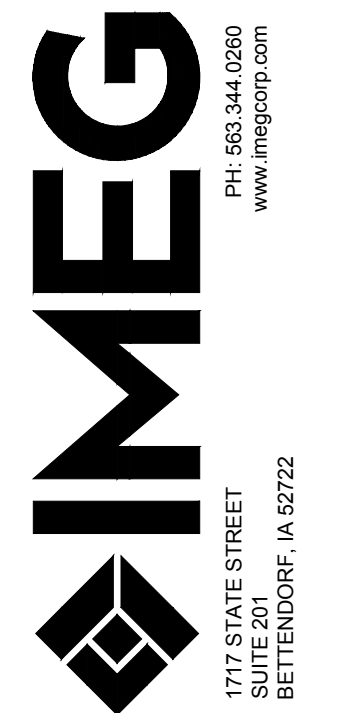
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 VERT. SCALE: 1" = 2'



PROFILE VIEW OF 23001201.00-ARGONNE AVE
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 VERT. SCALE: 1" = 2'



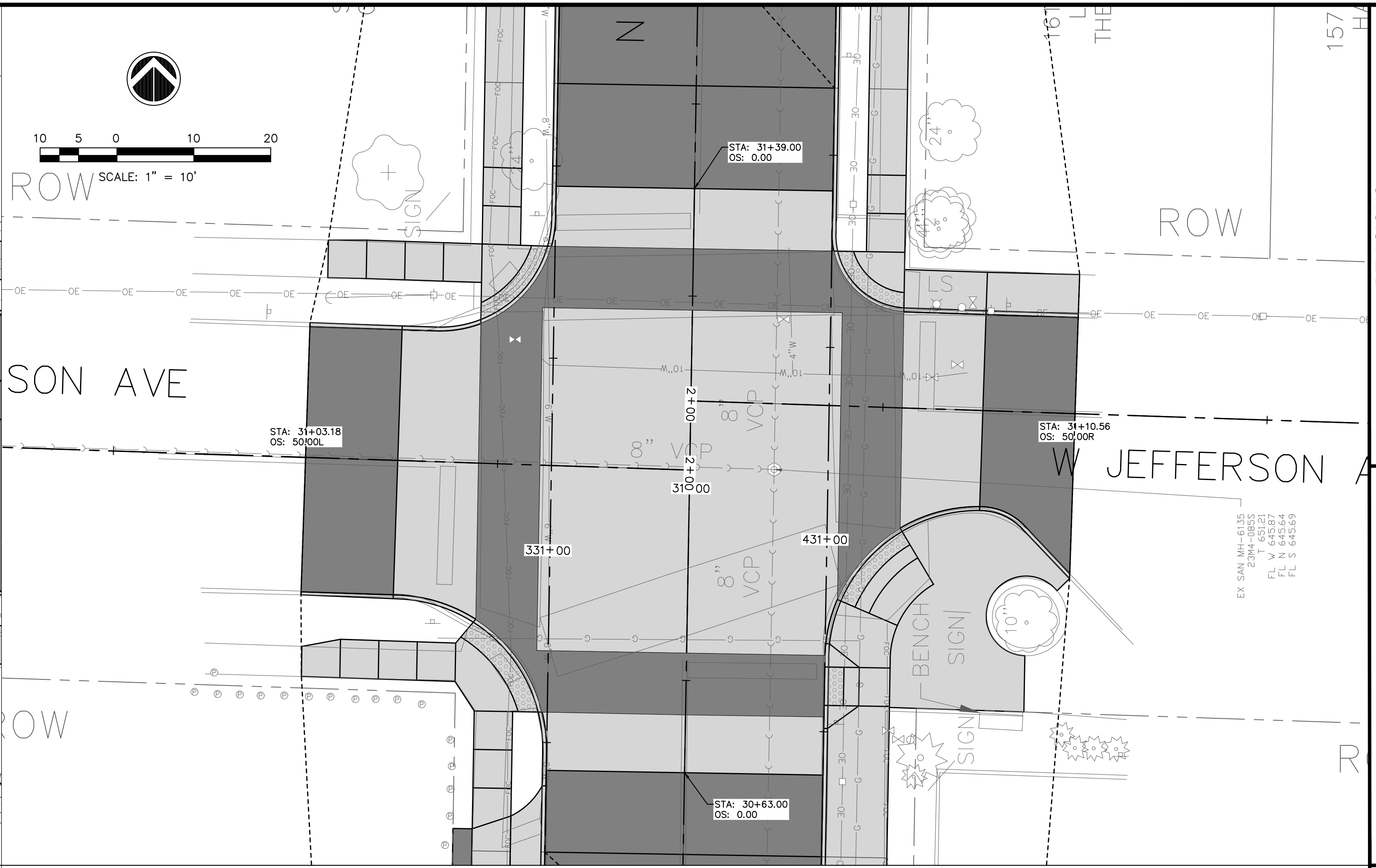
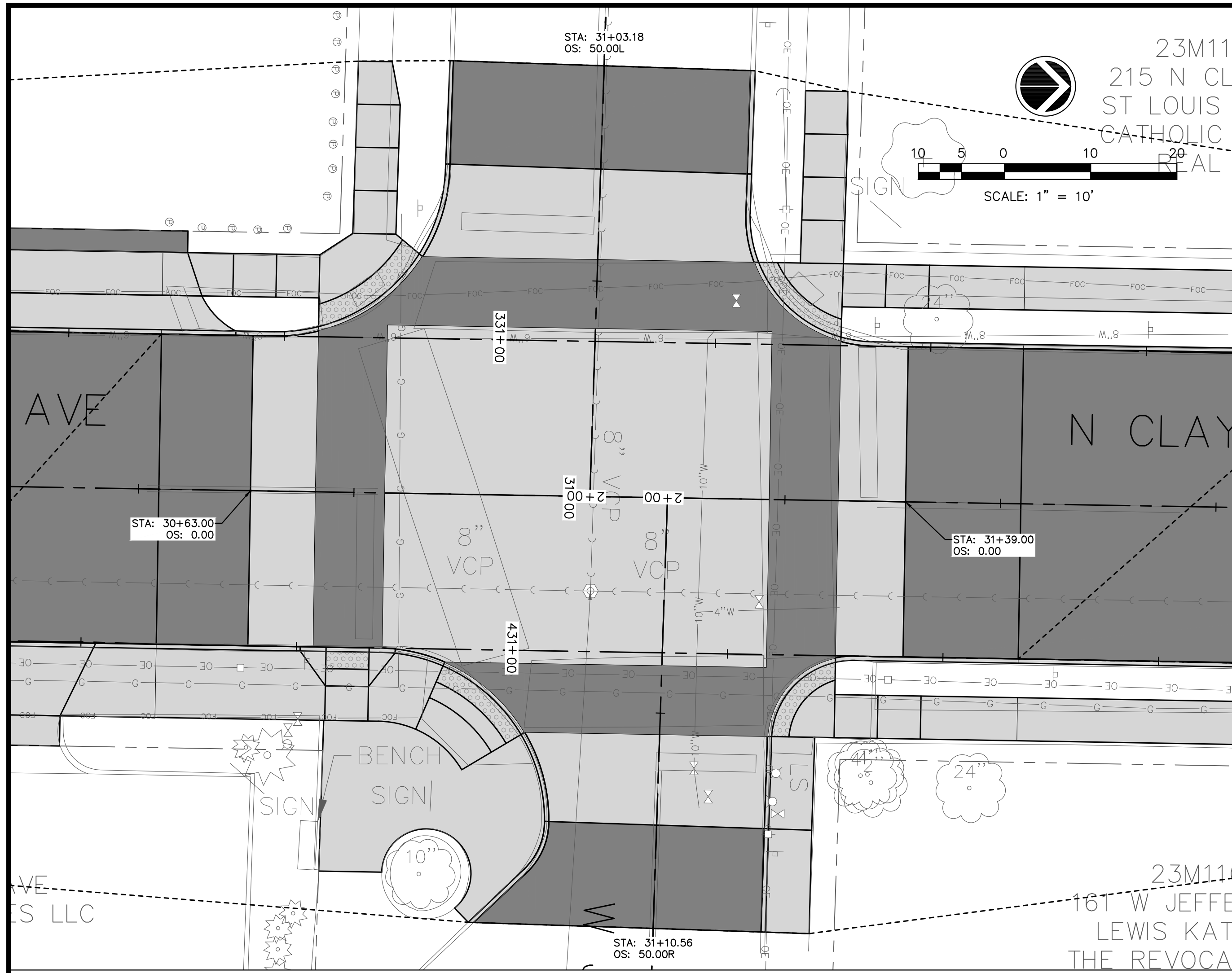
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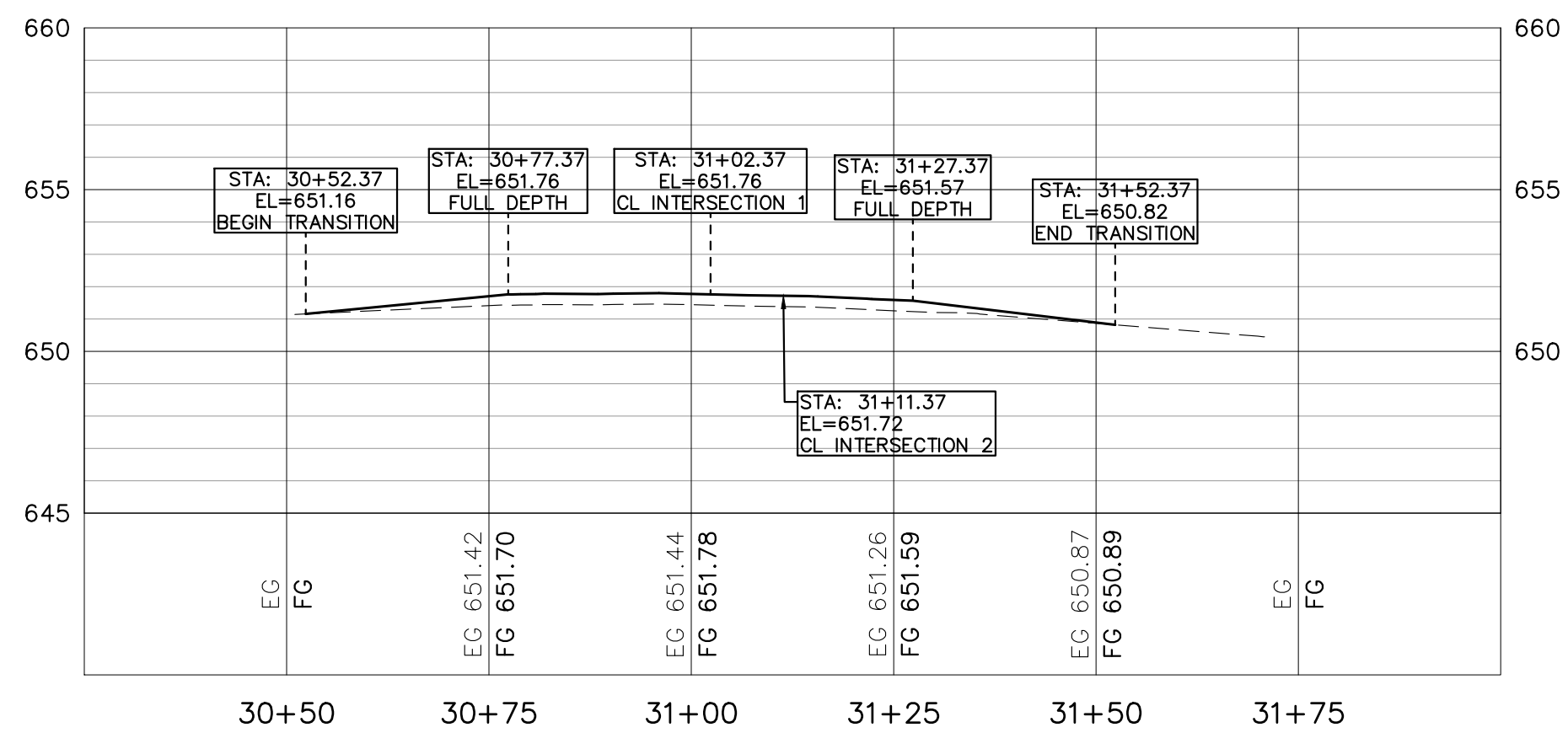
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CLAY AVE & W. ARGONNE AVE PLAN & PROFILE

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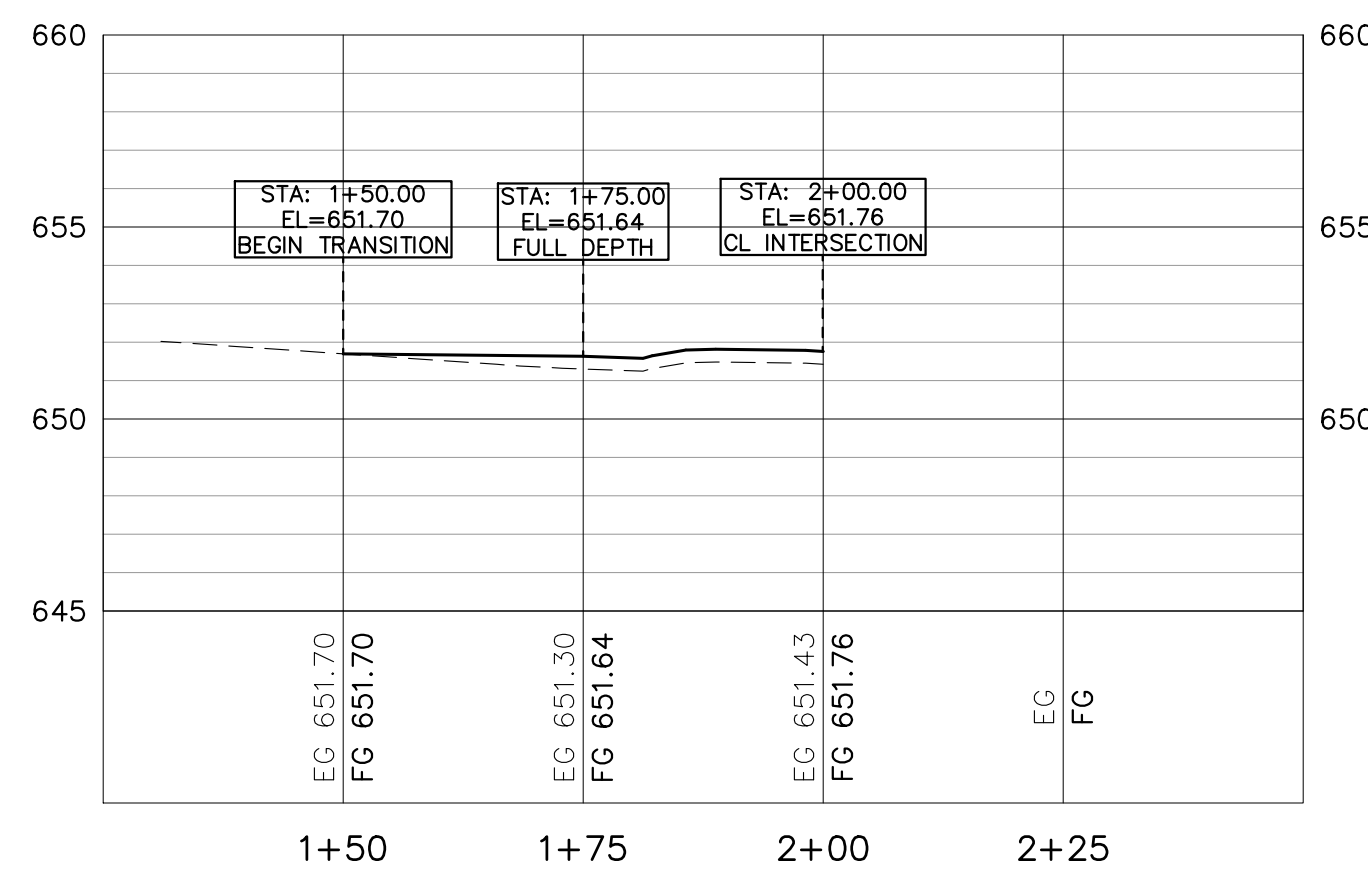
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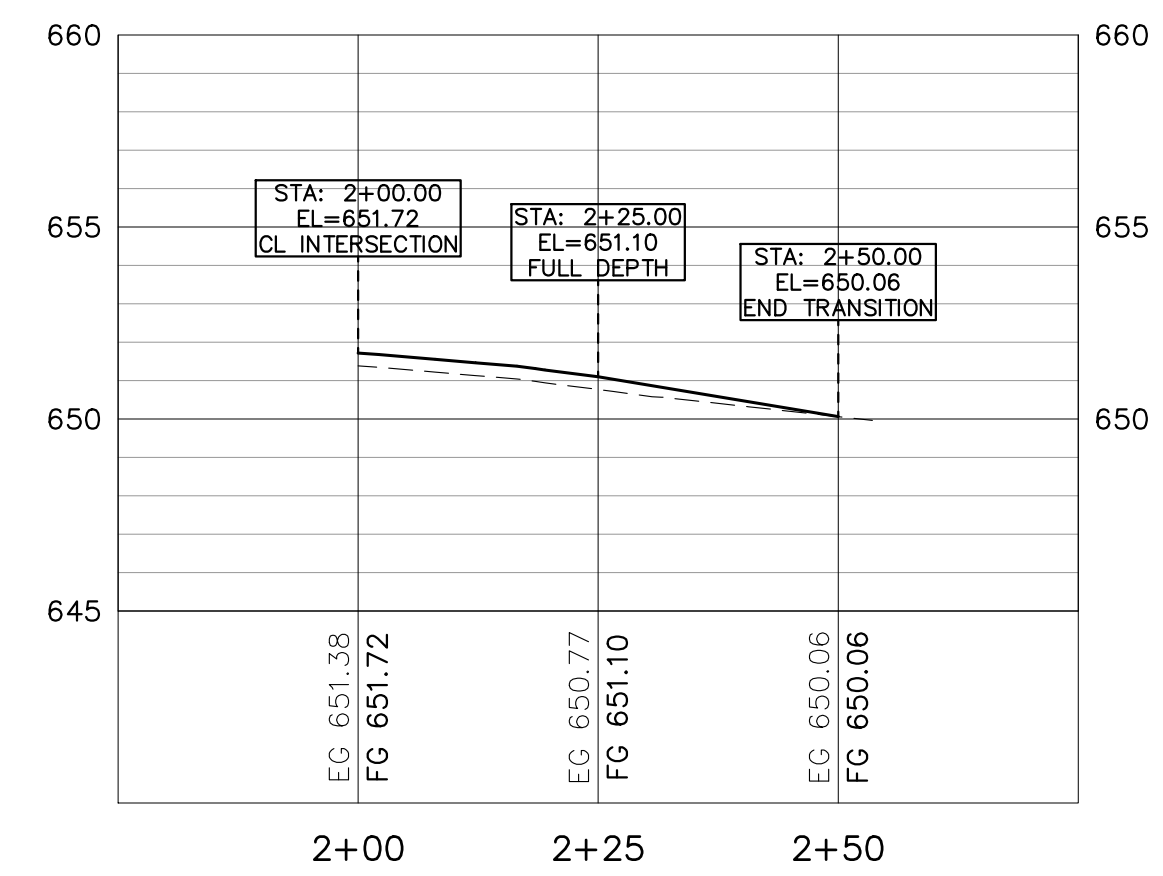
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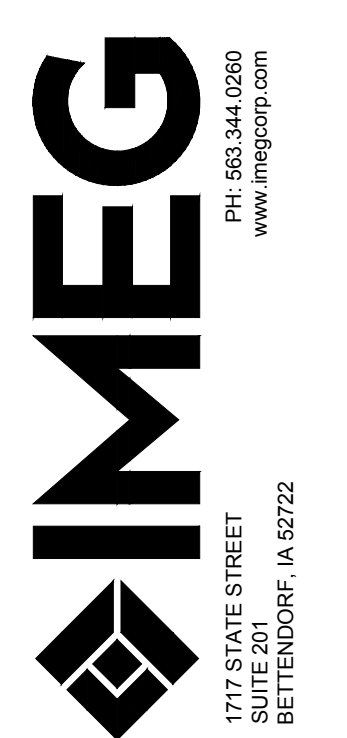
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PROFILE VIEW OF 23001201.00-JEFFERSON AVE 2
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 VERT. SCALE: 1" = 2'



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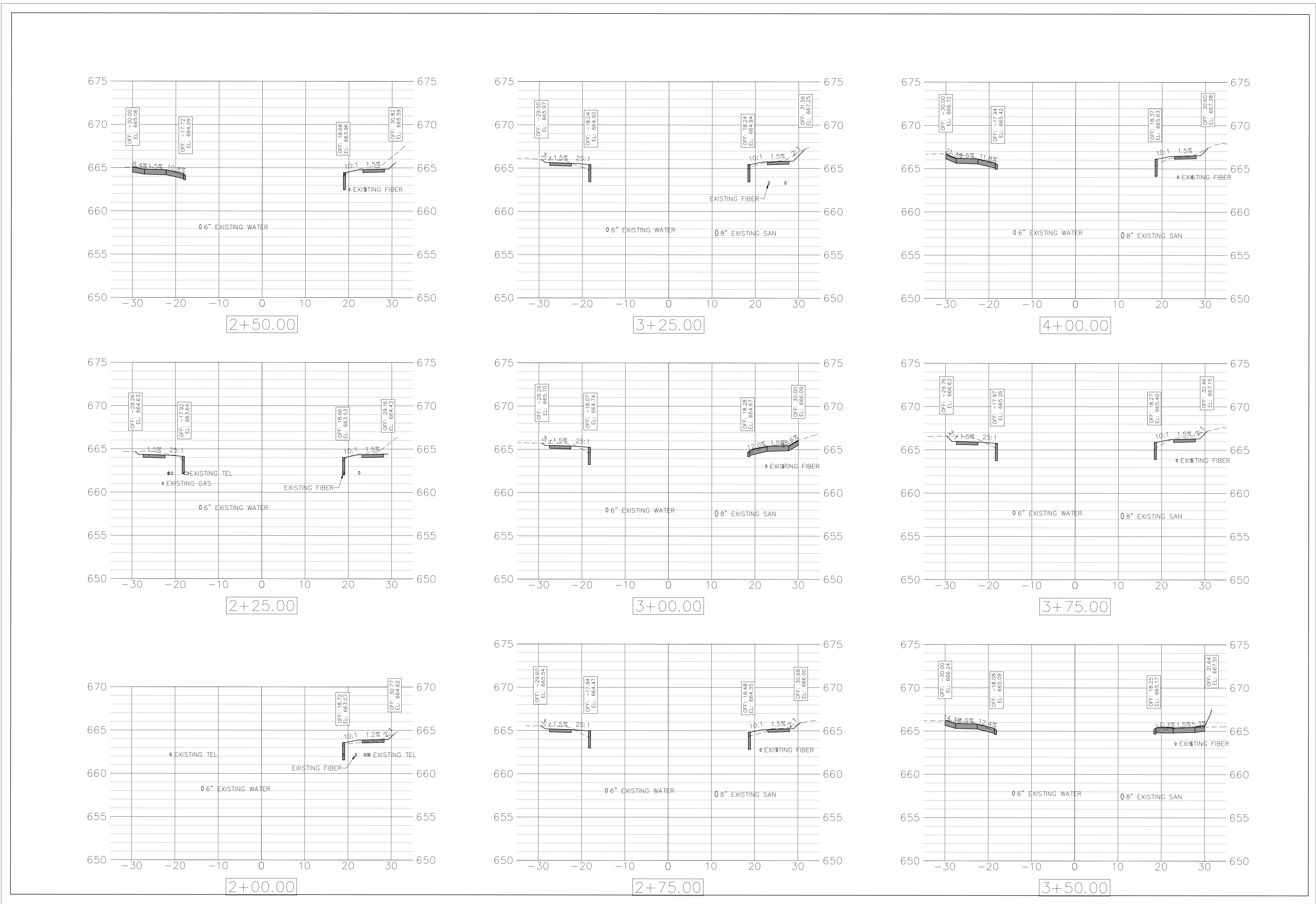


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 KIRKWOOD, MISSOURI
 CLAY AVE & W. JEFFERSON AVE PLAN & PROFILE

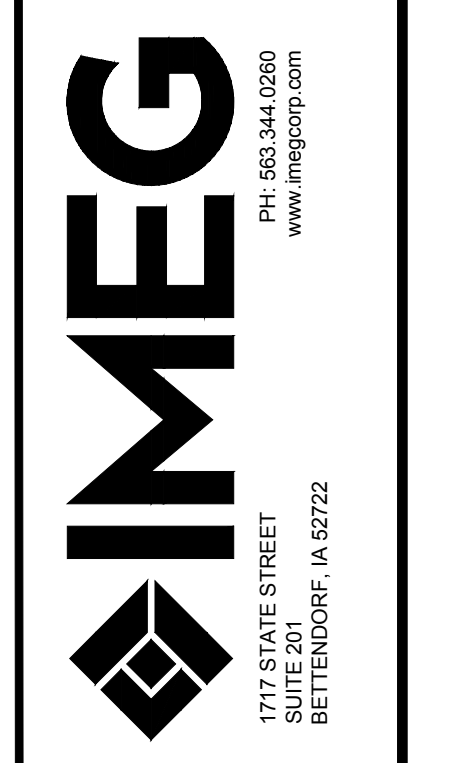
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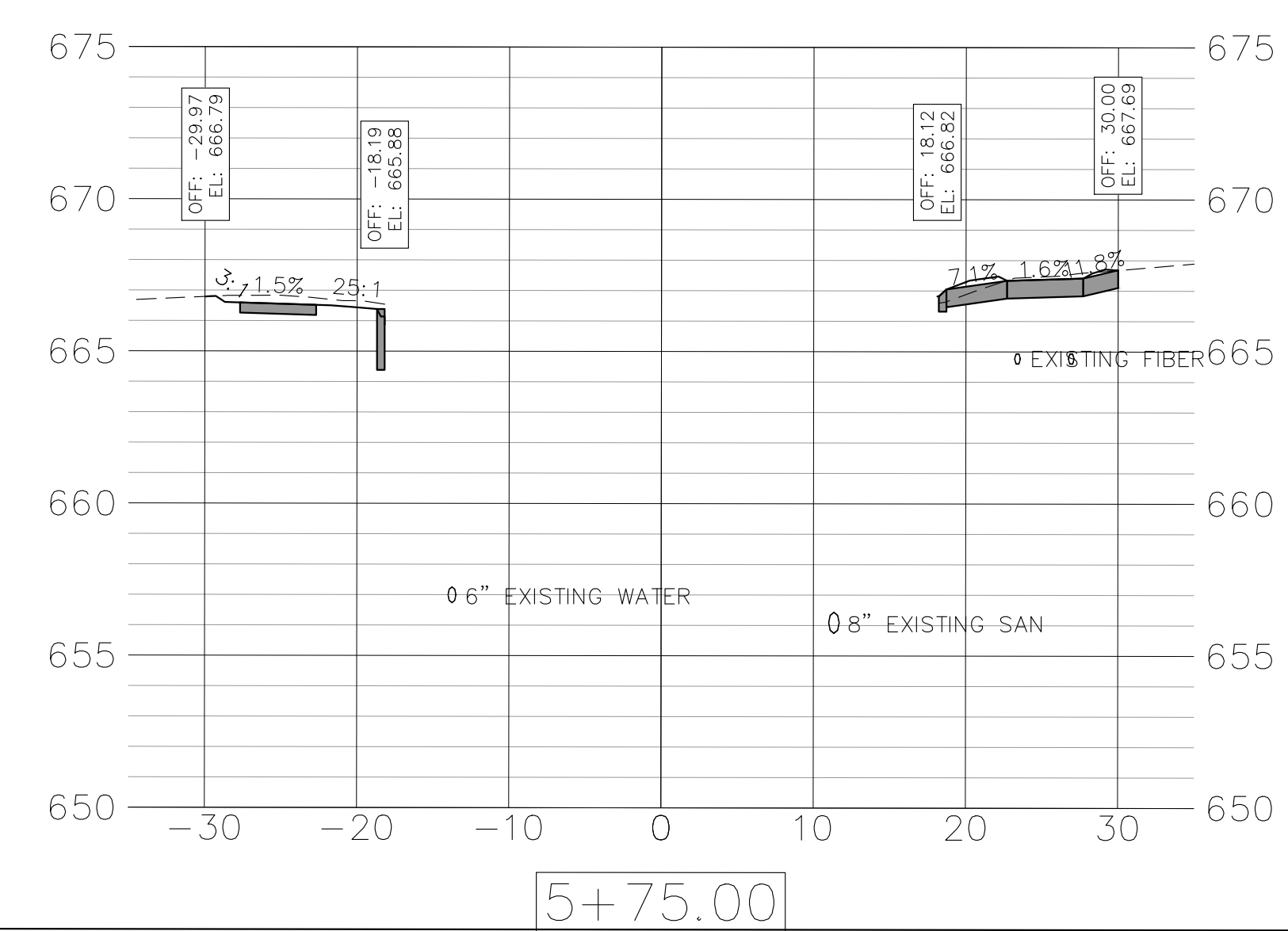
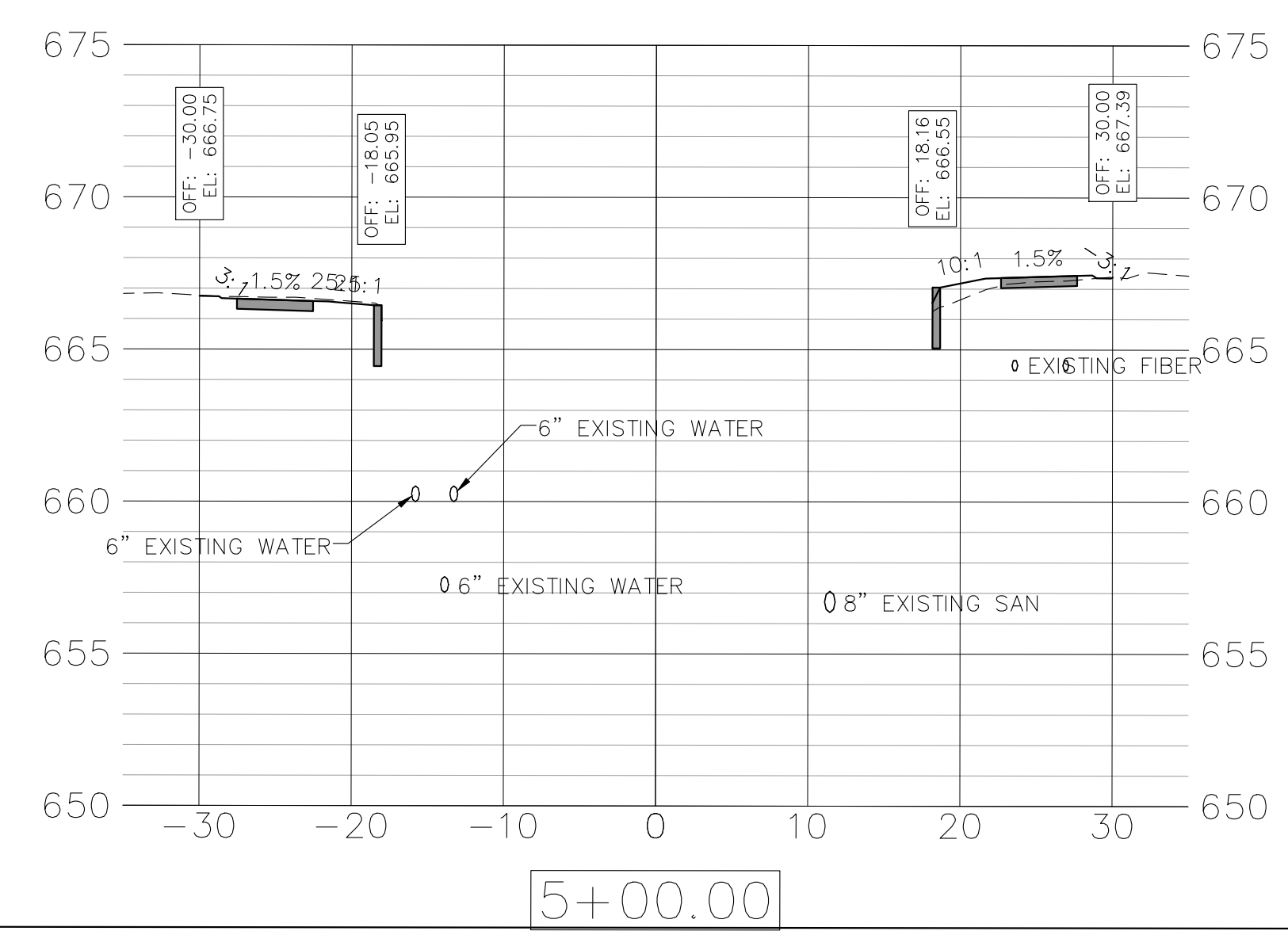
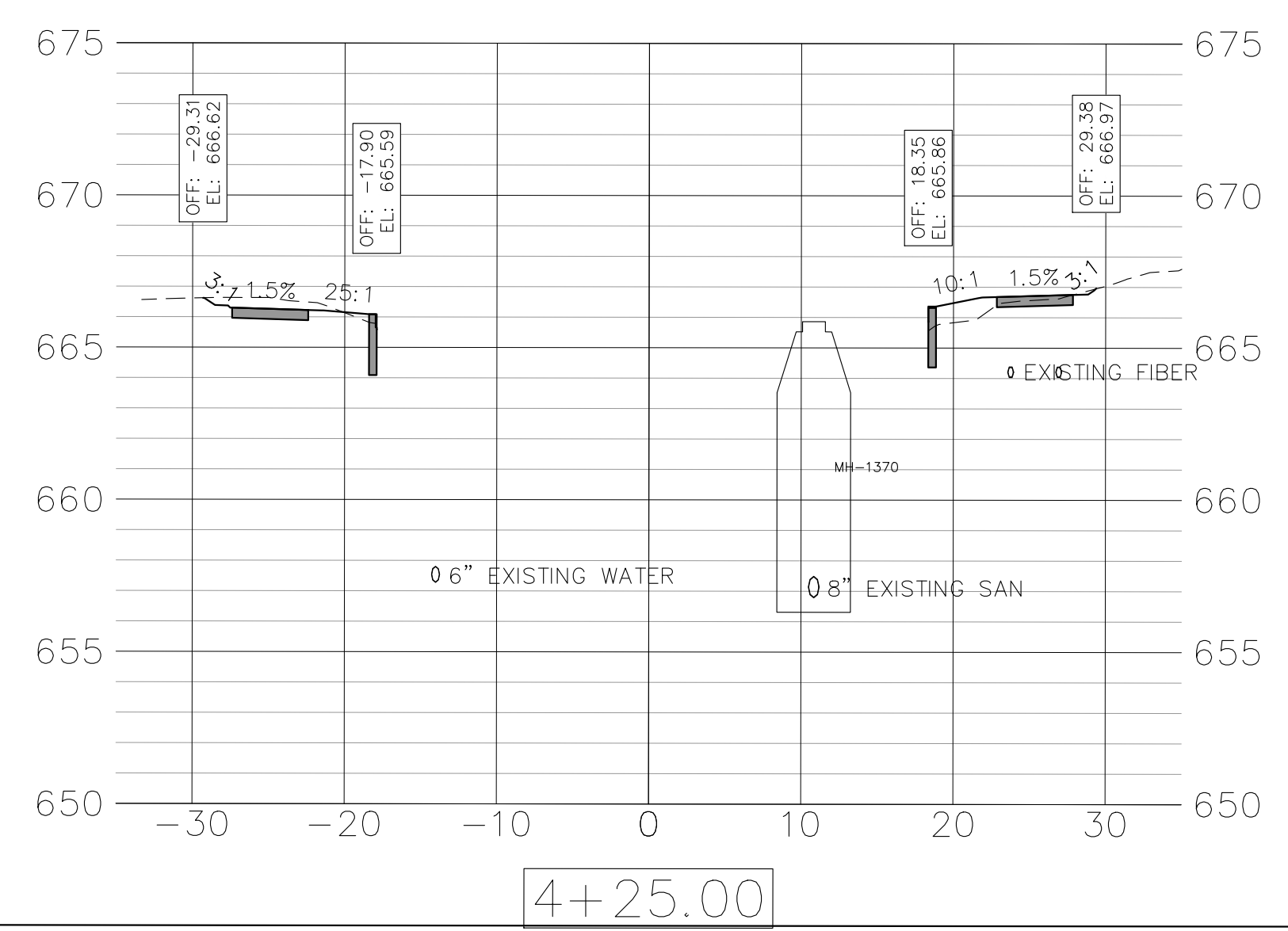
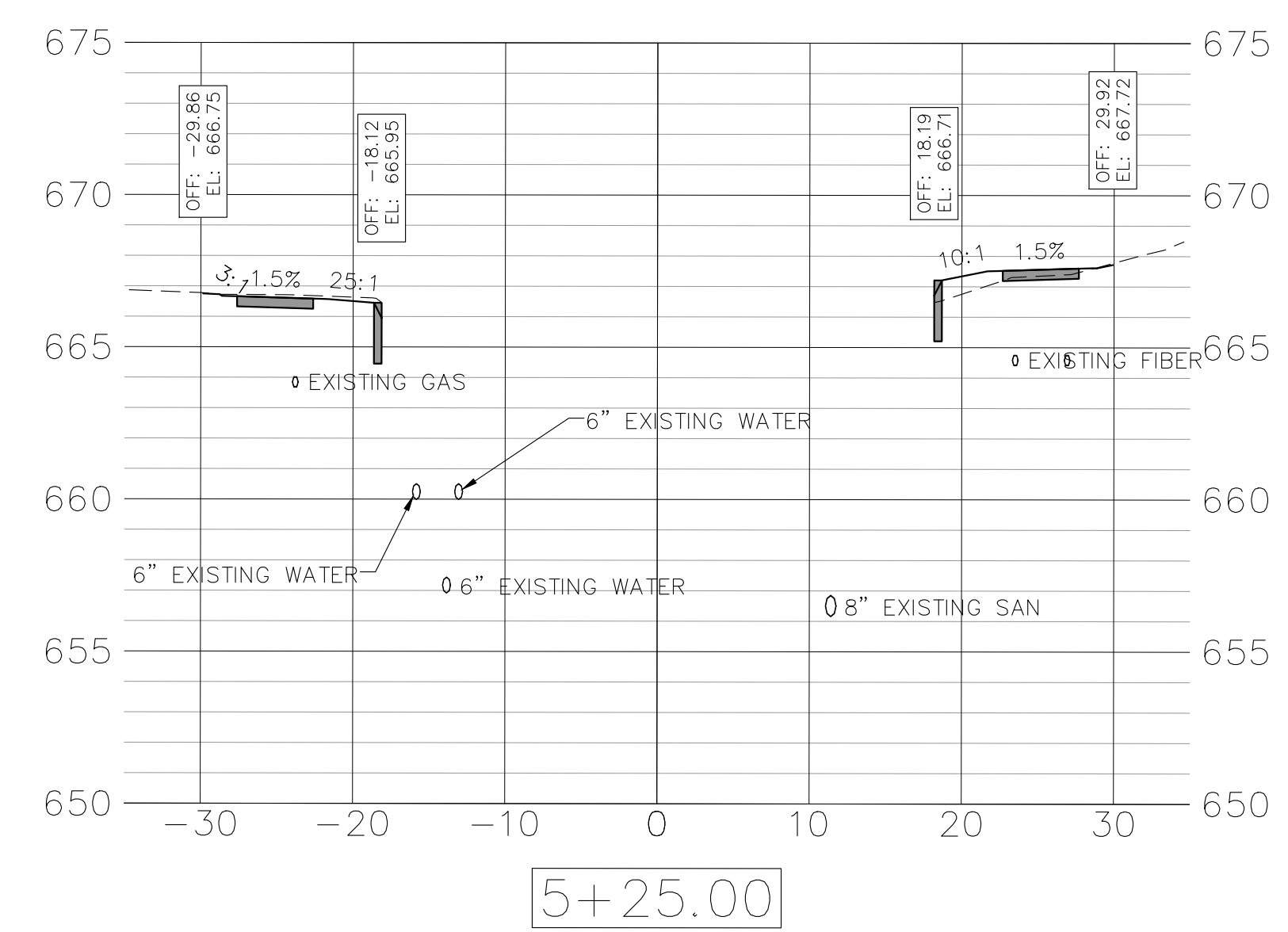
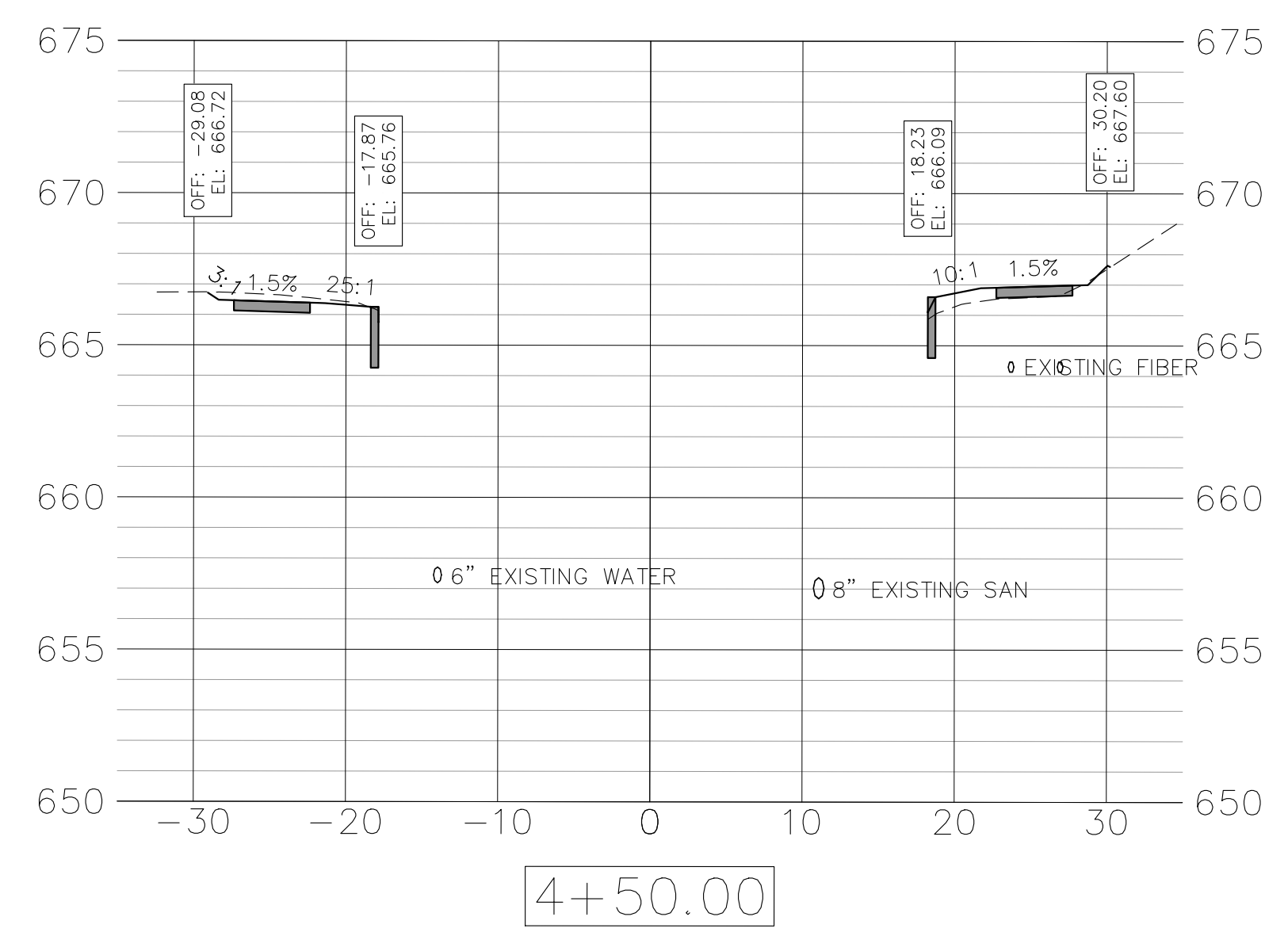
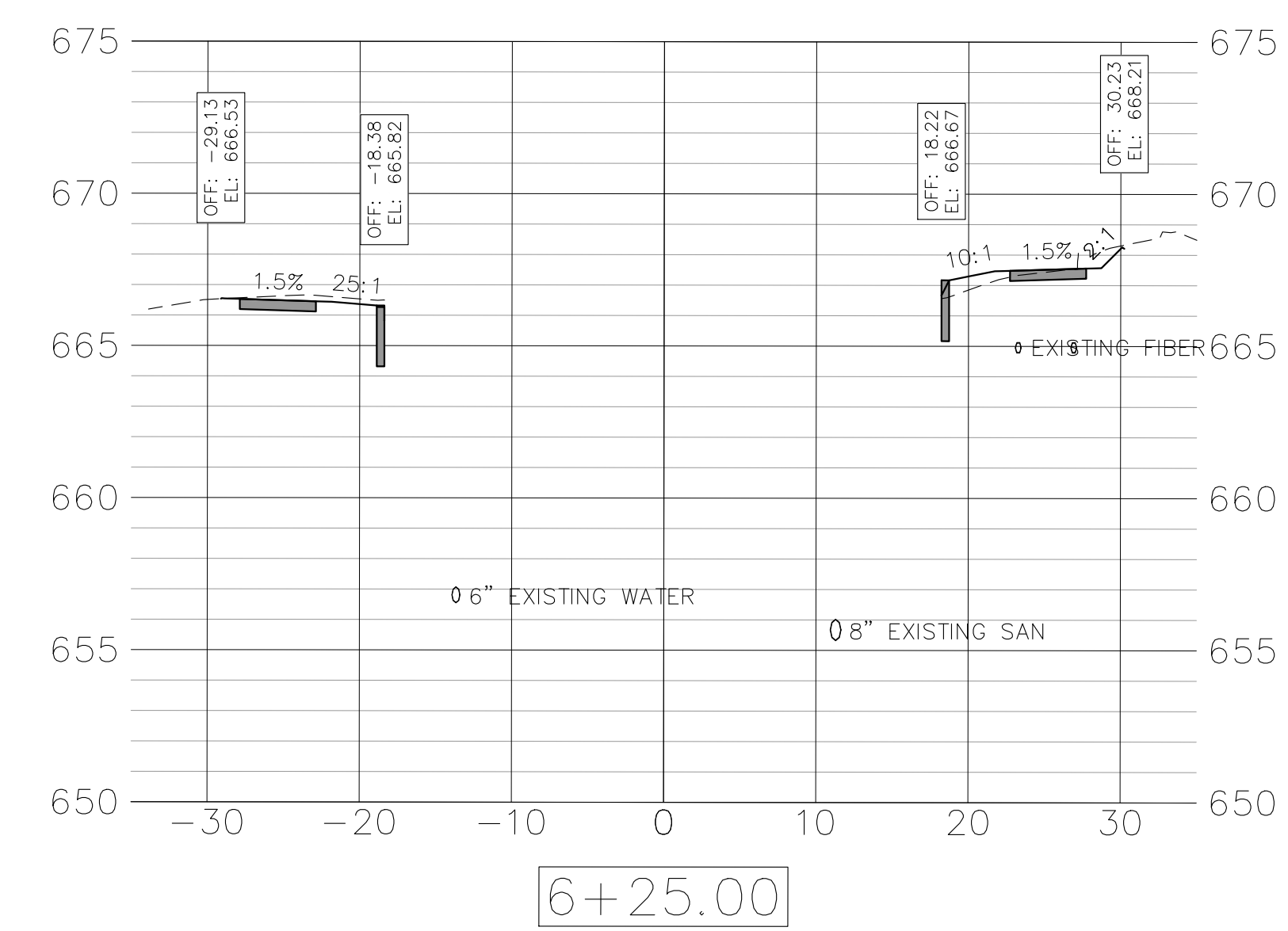
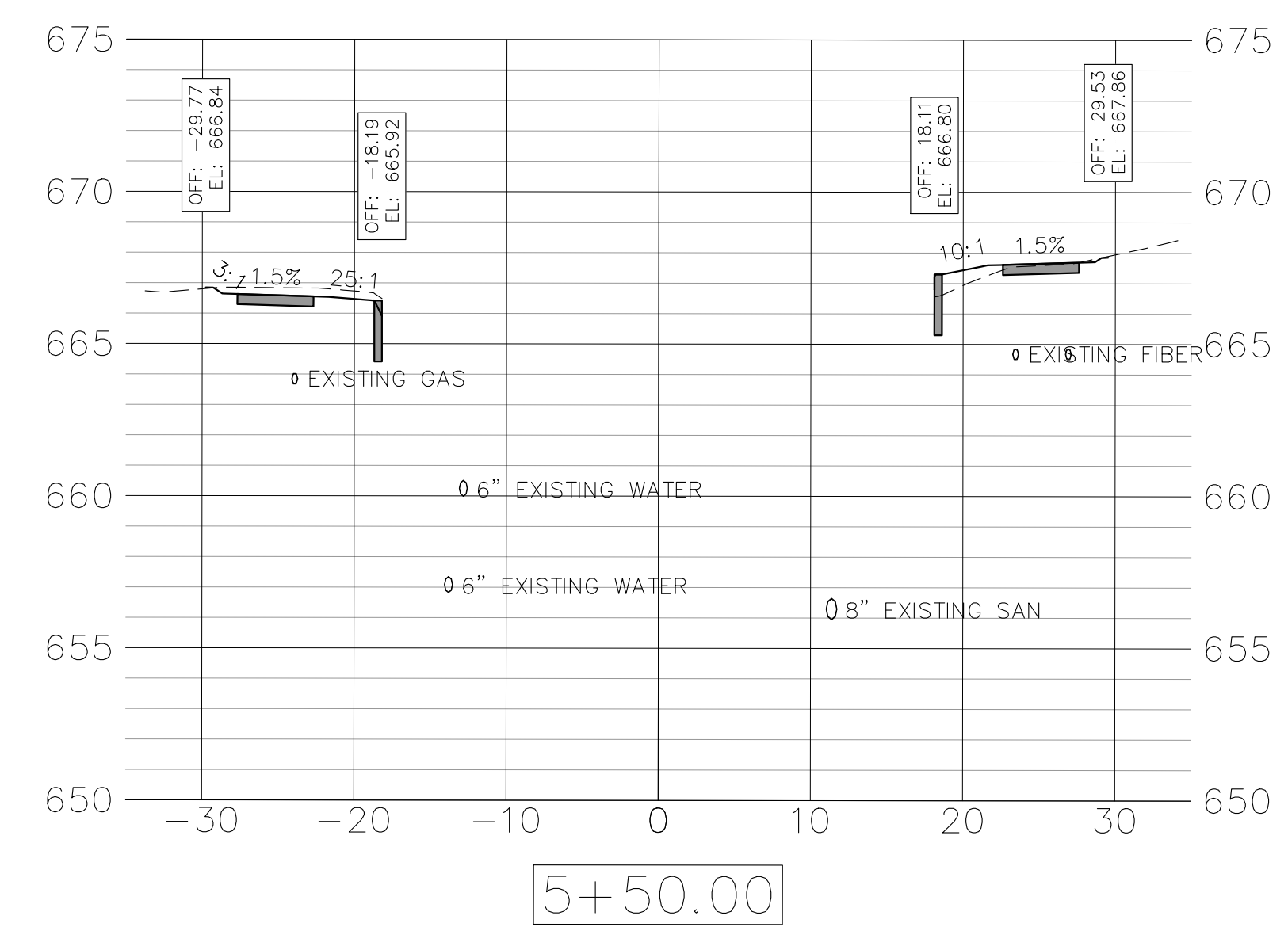
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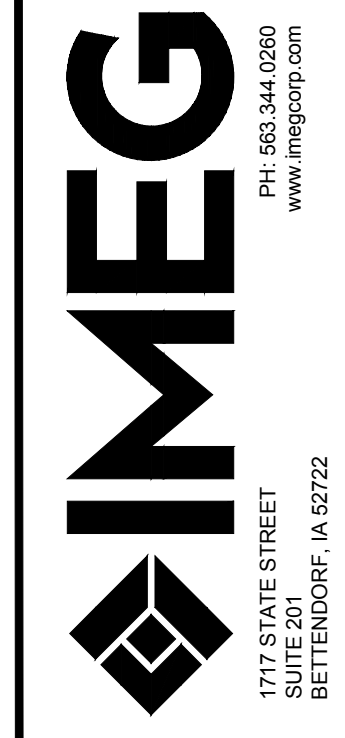
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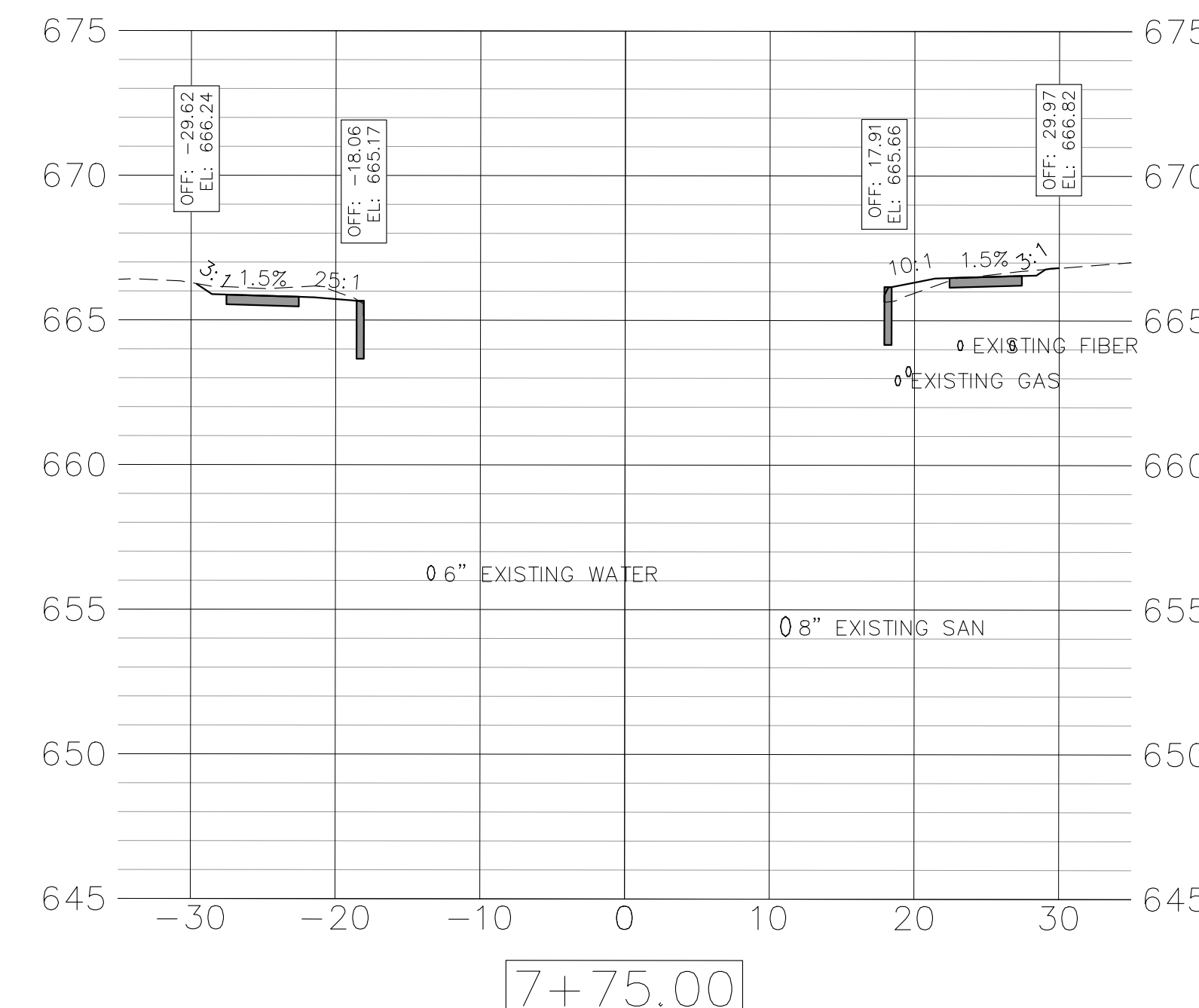
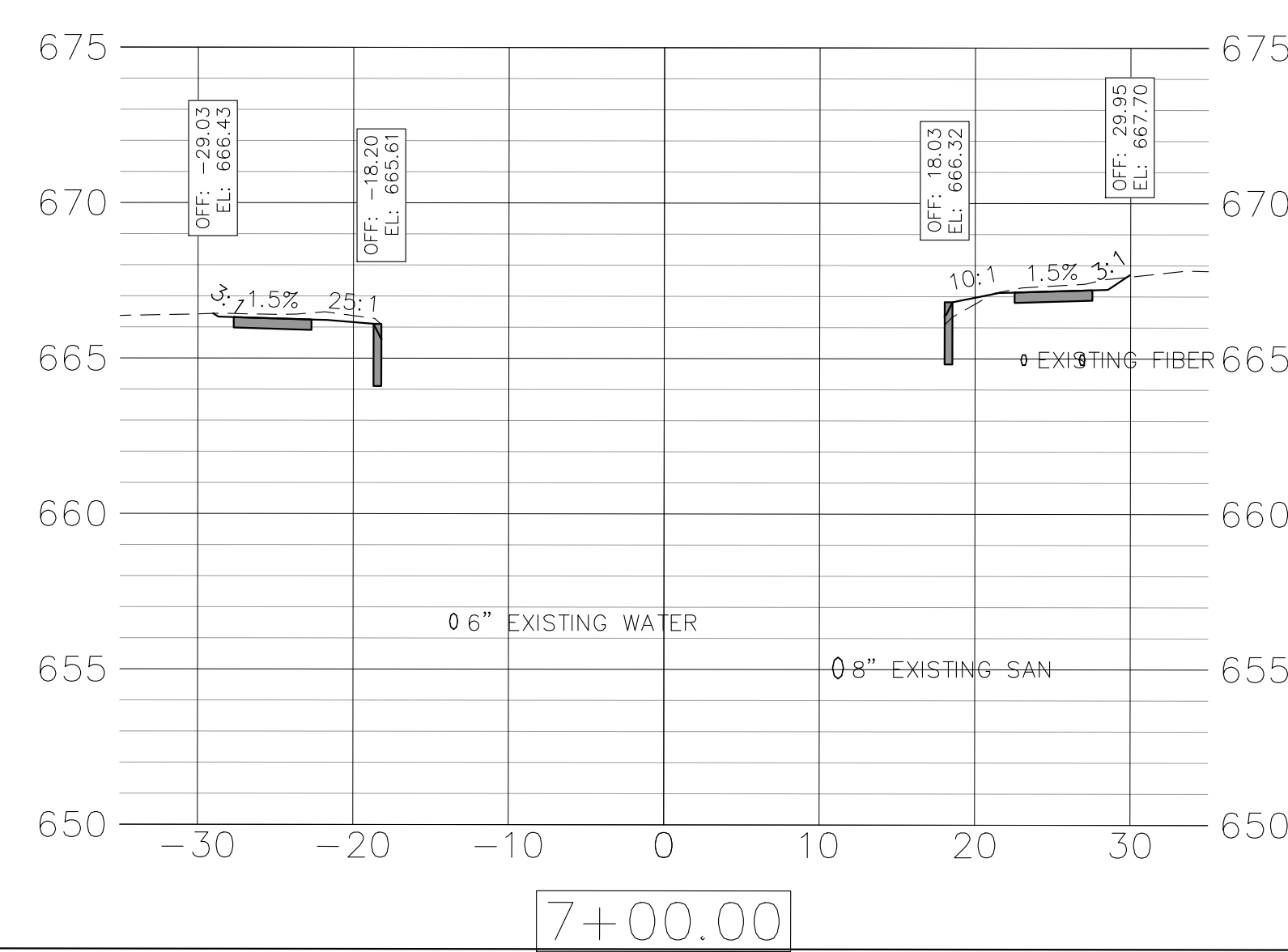
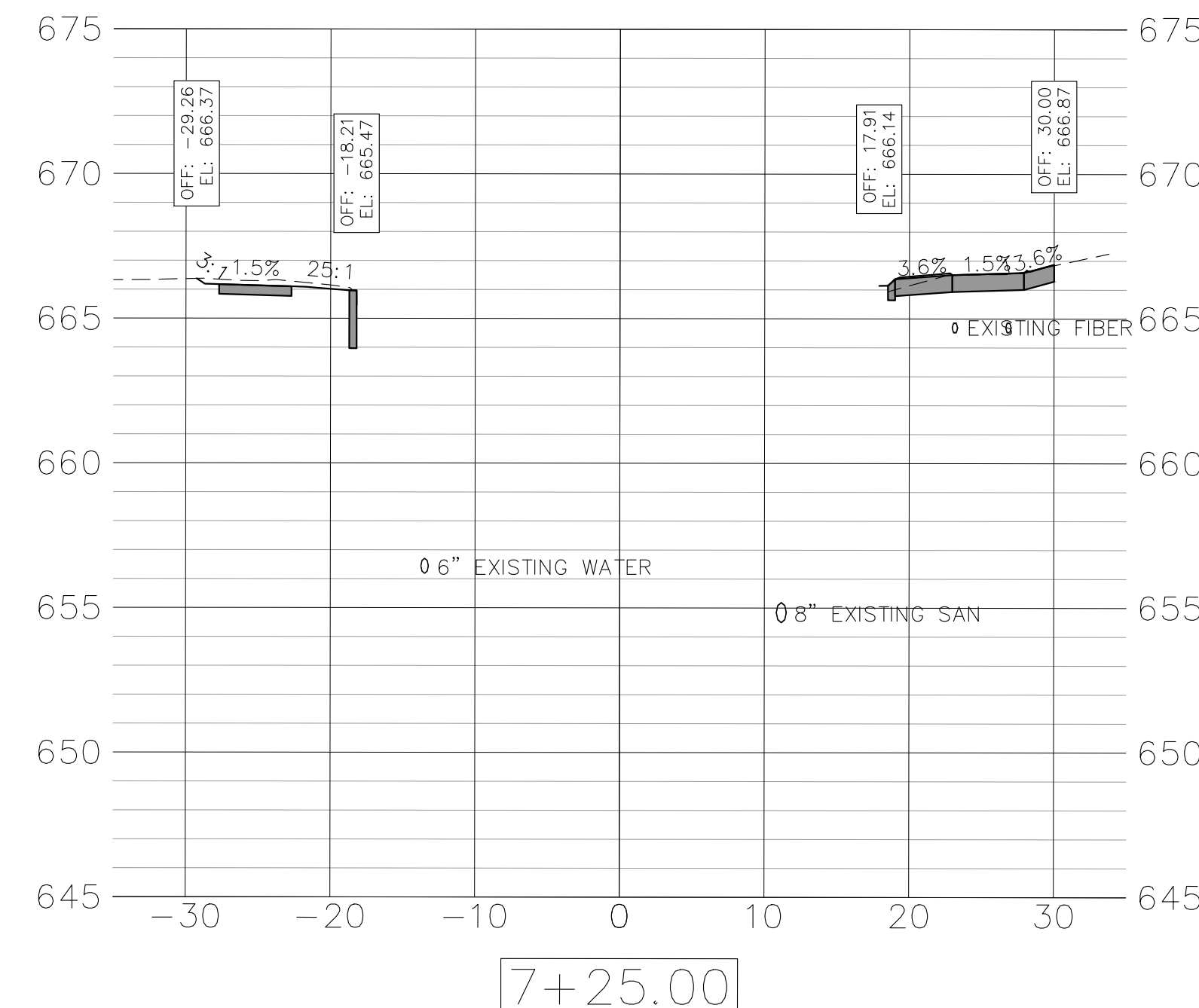
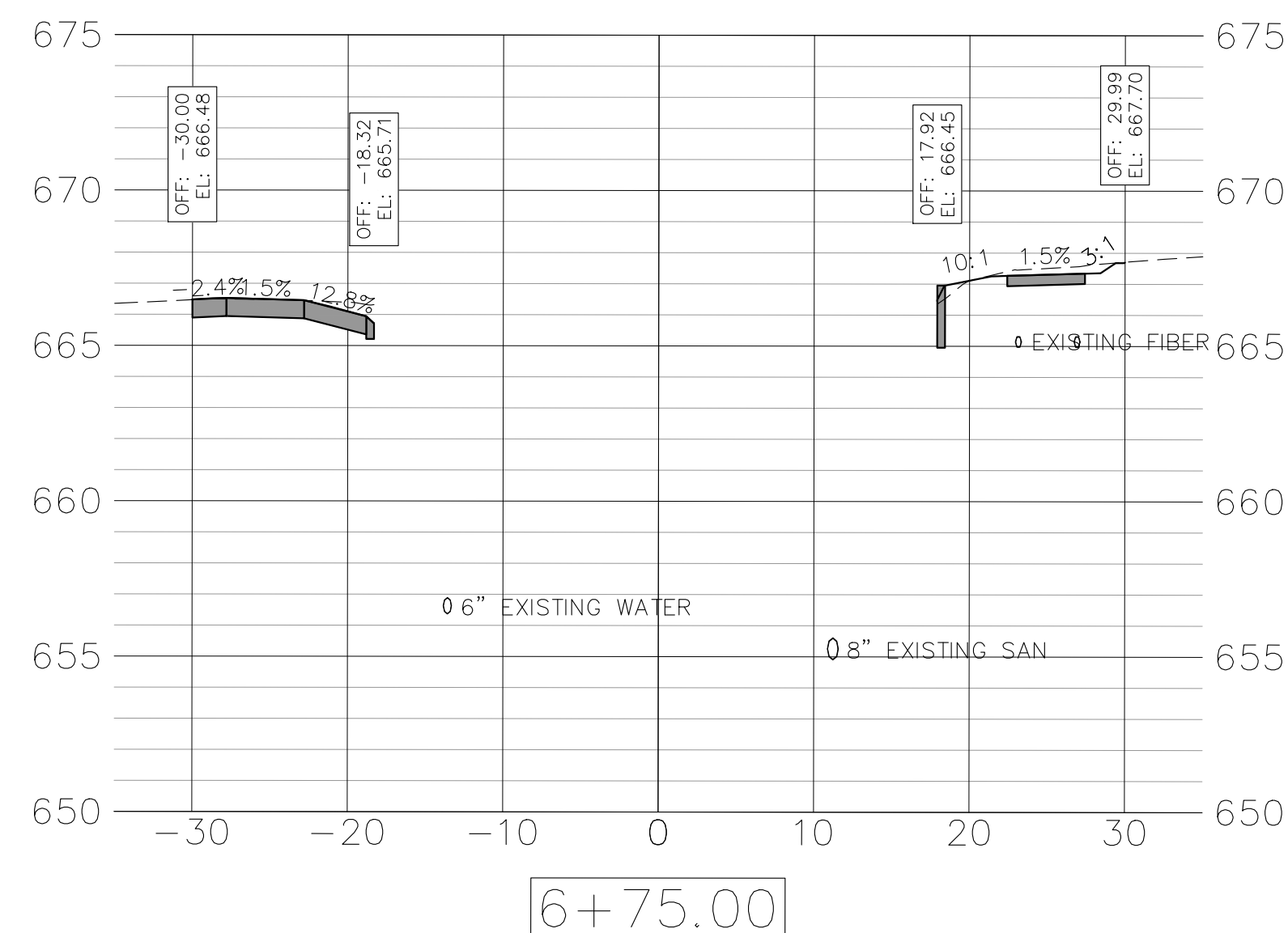
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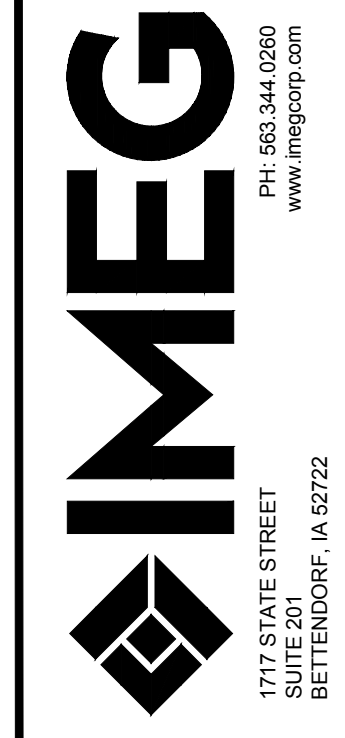
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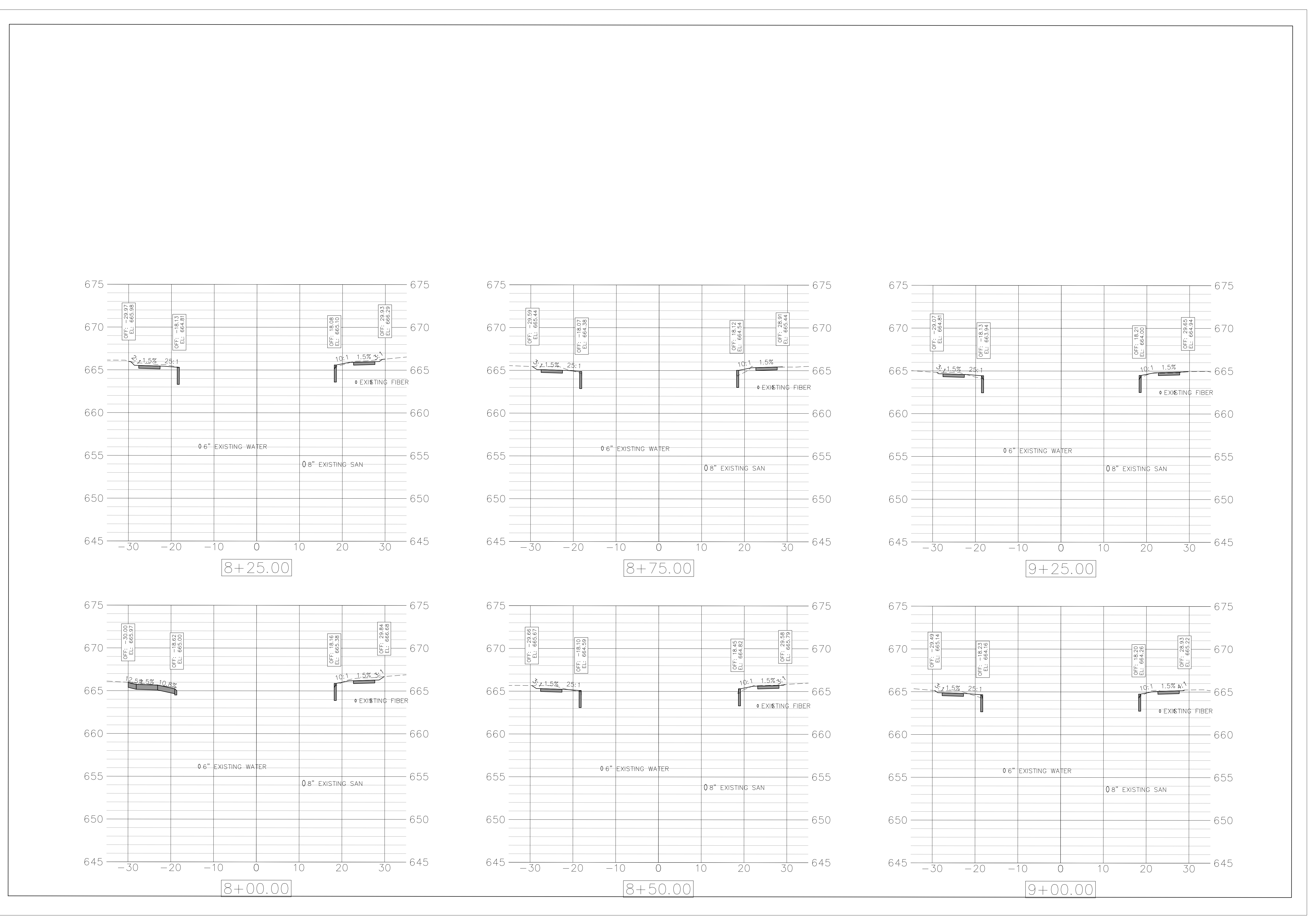
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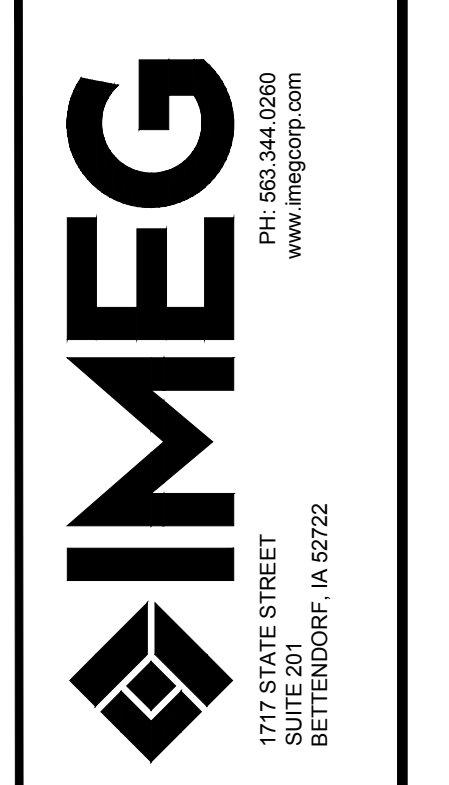
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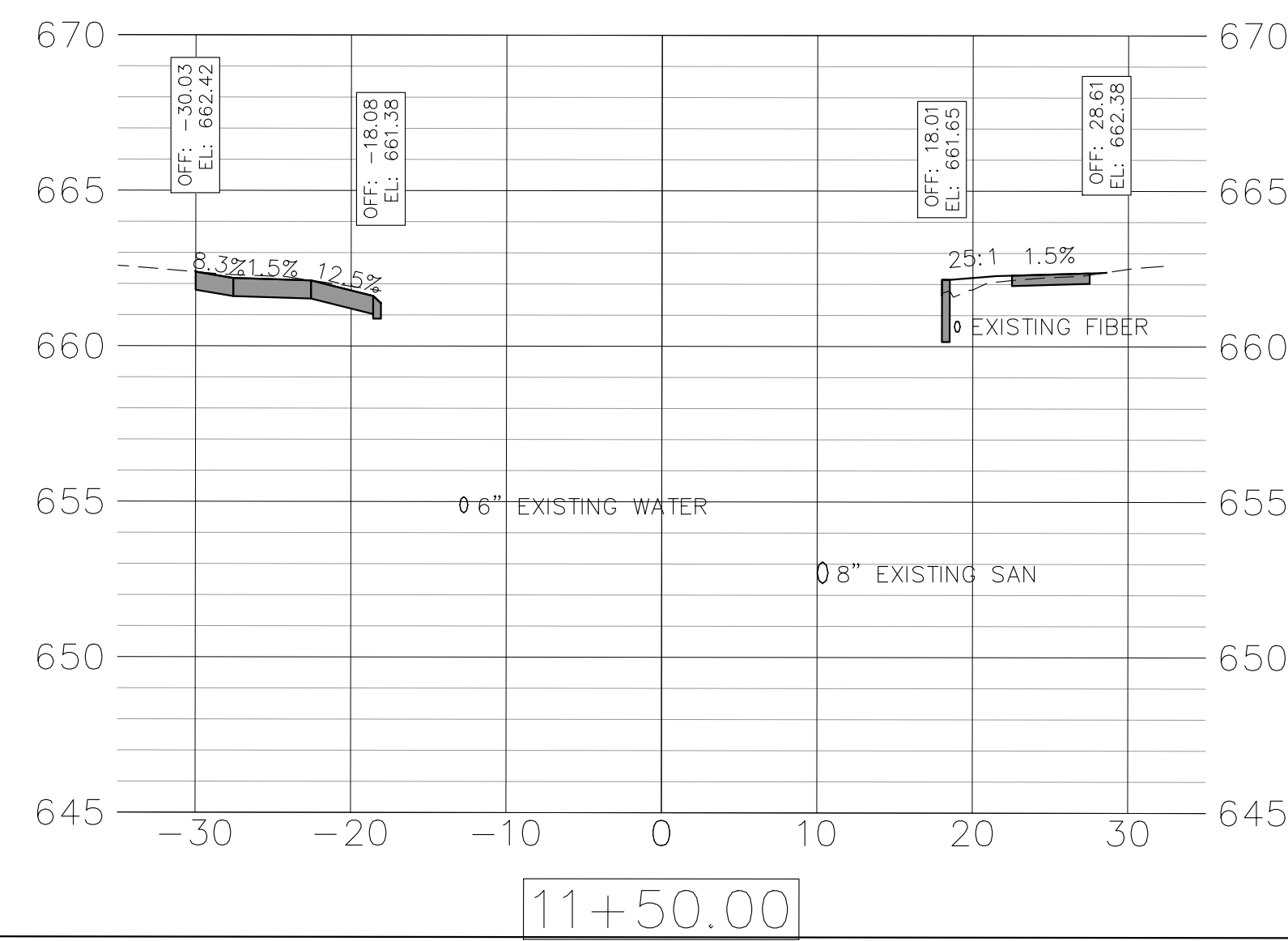
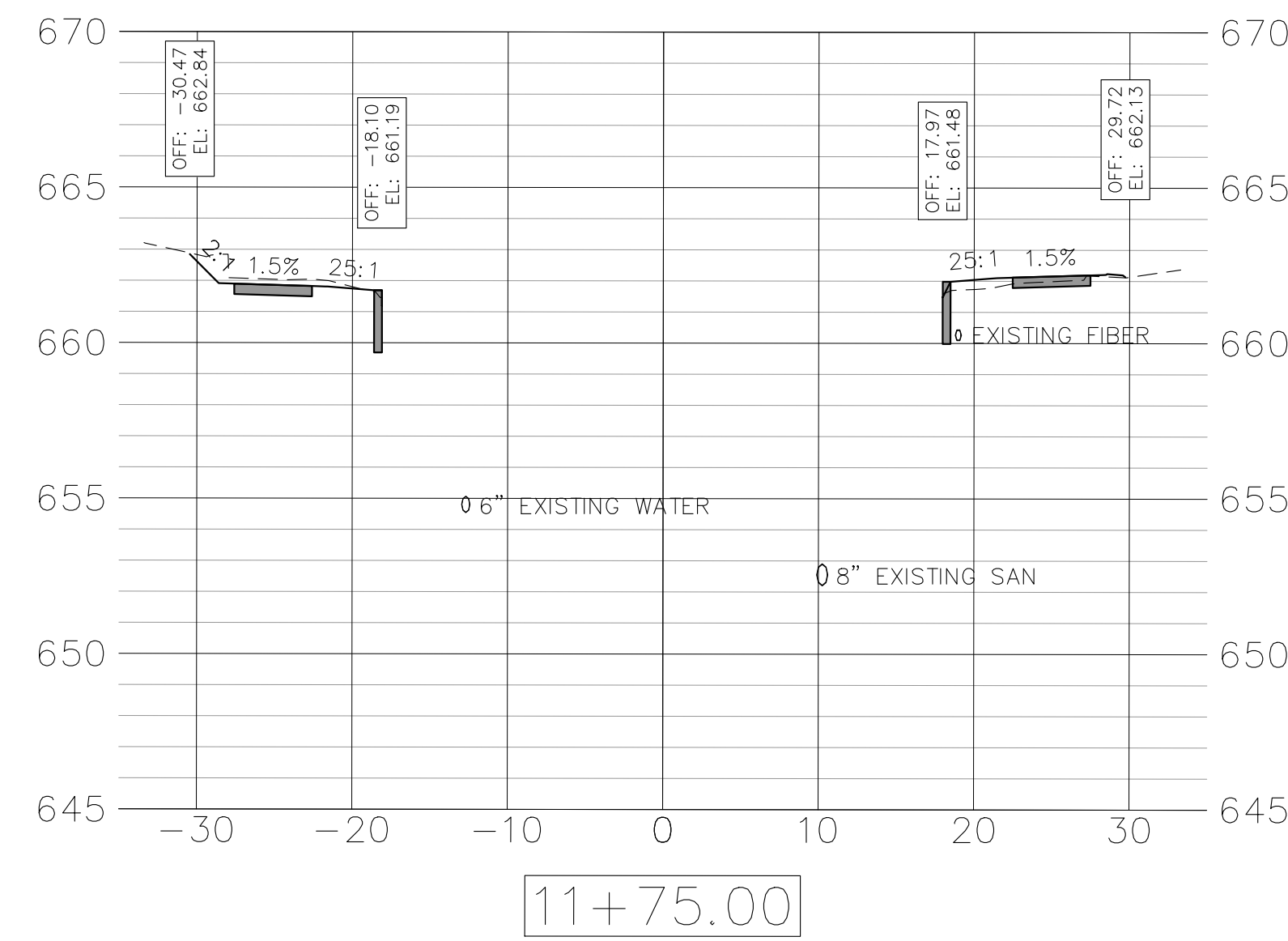
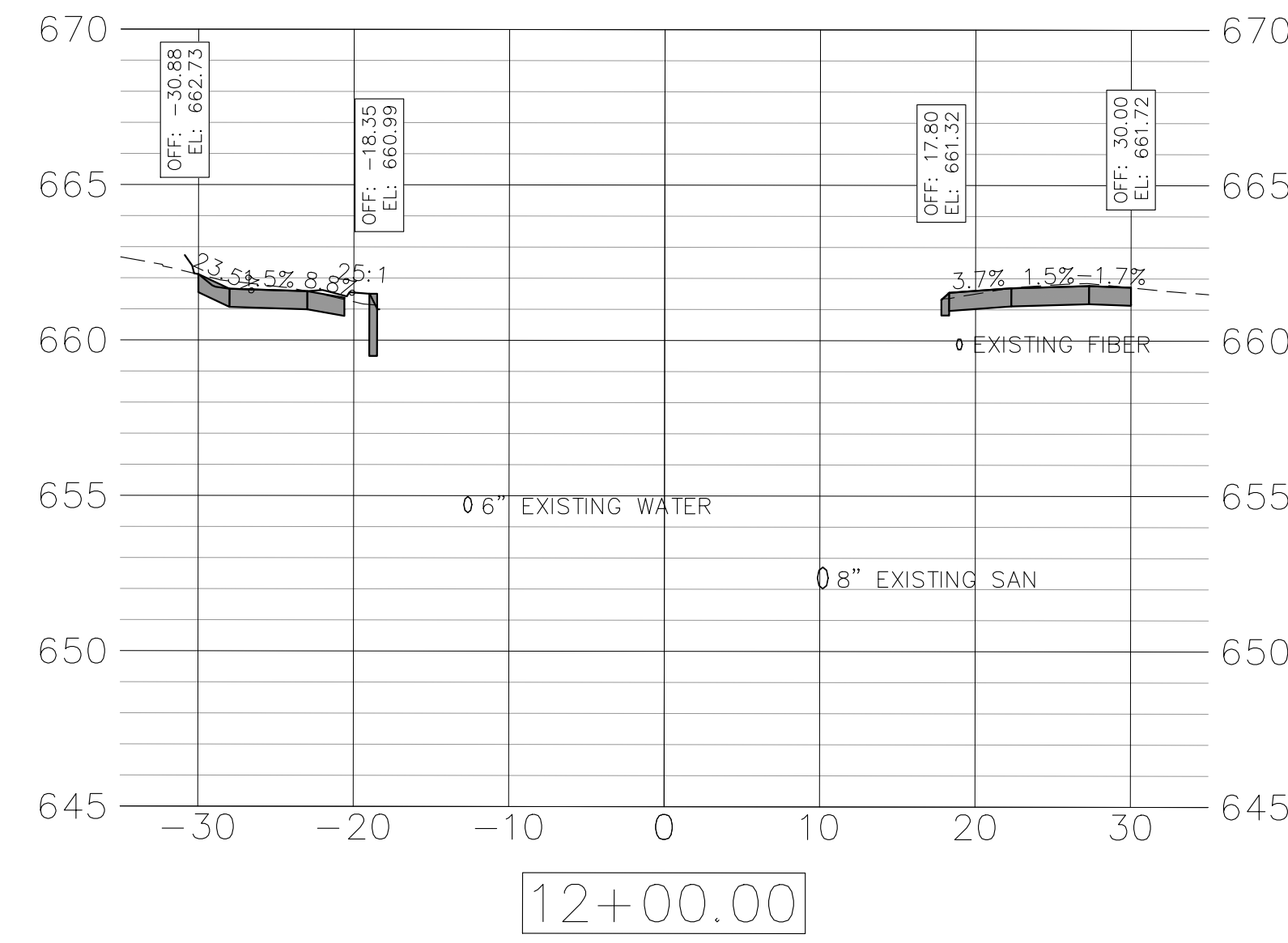
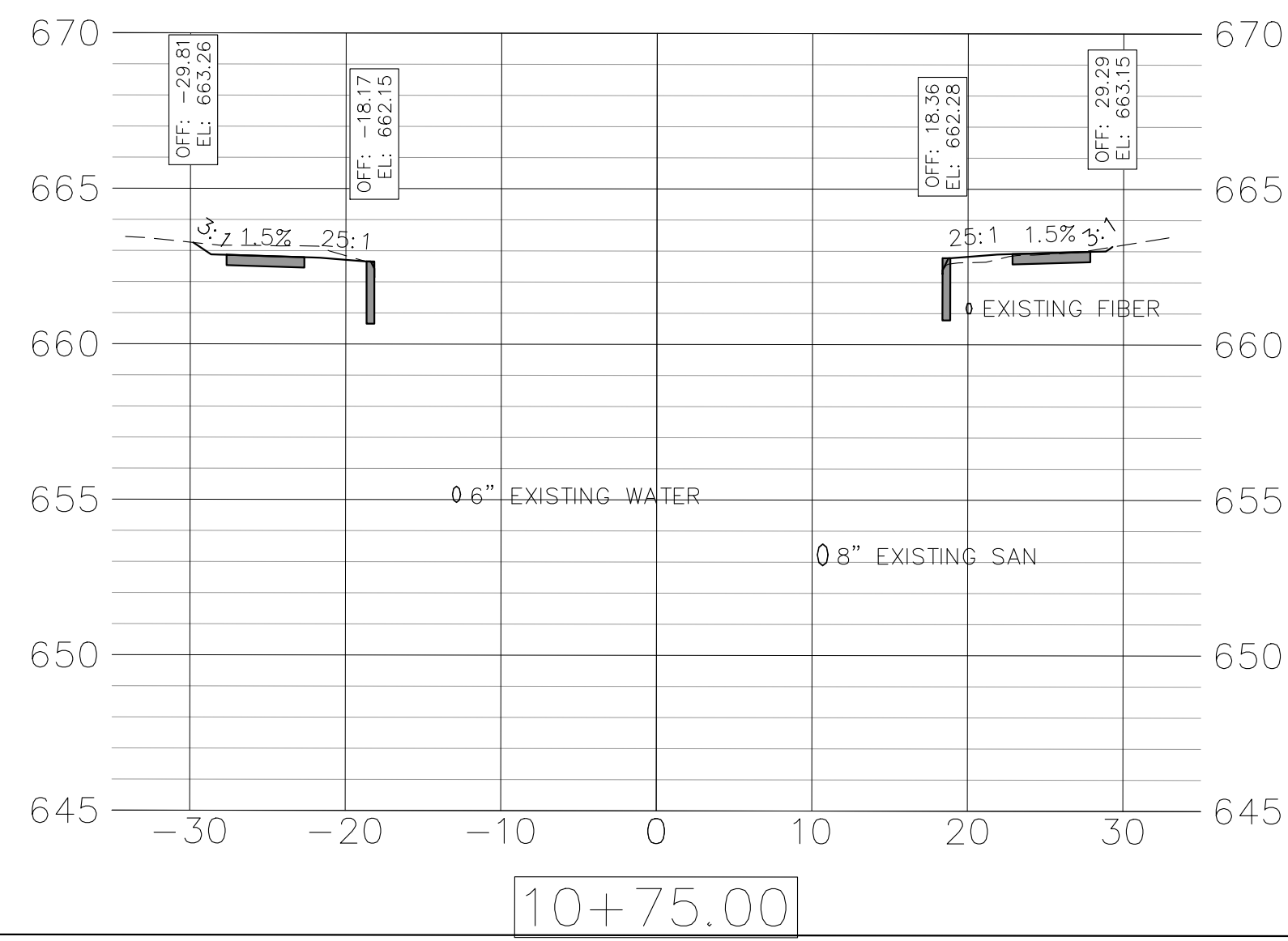
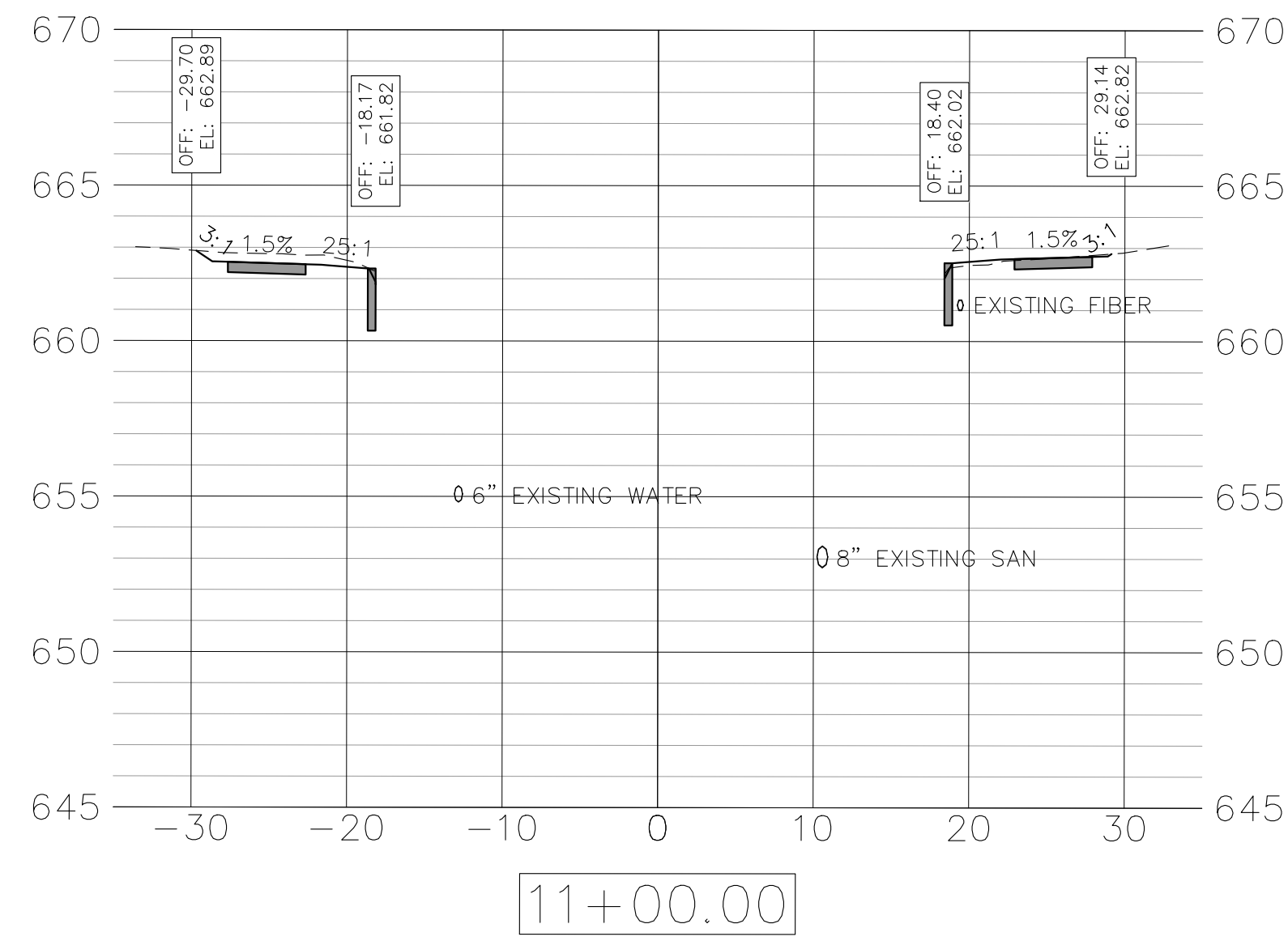
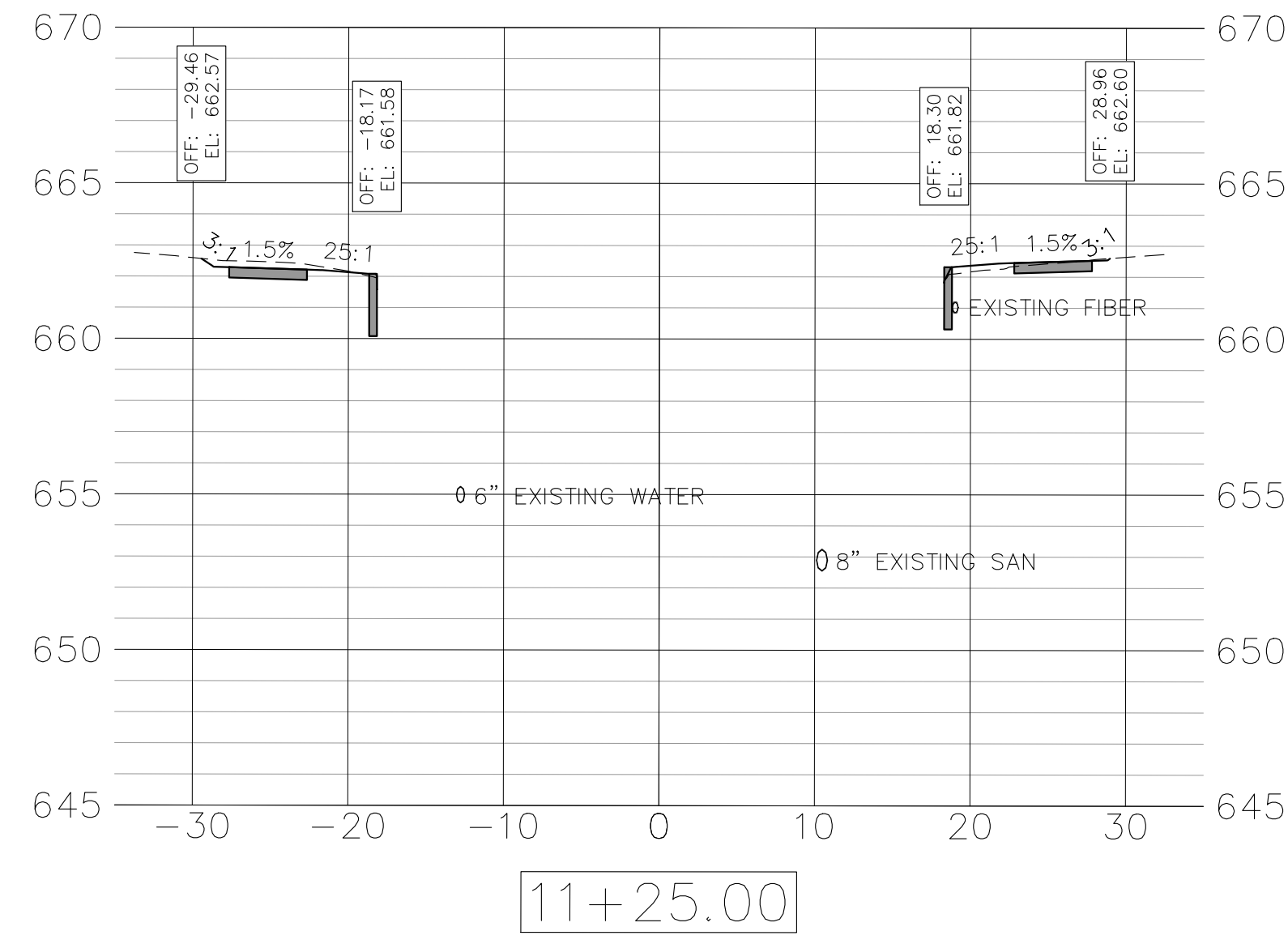
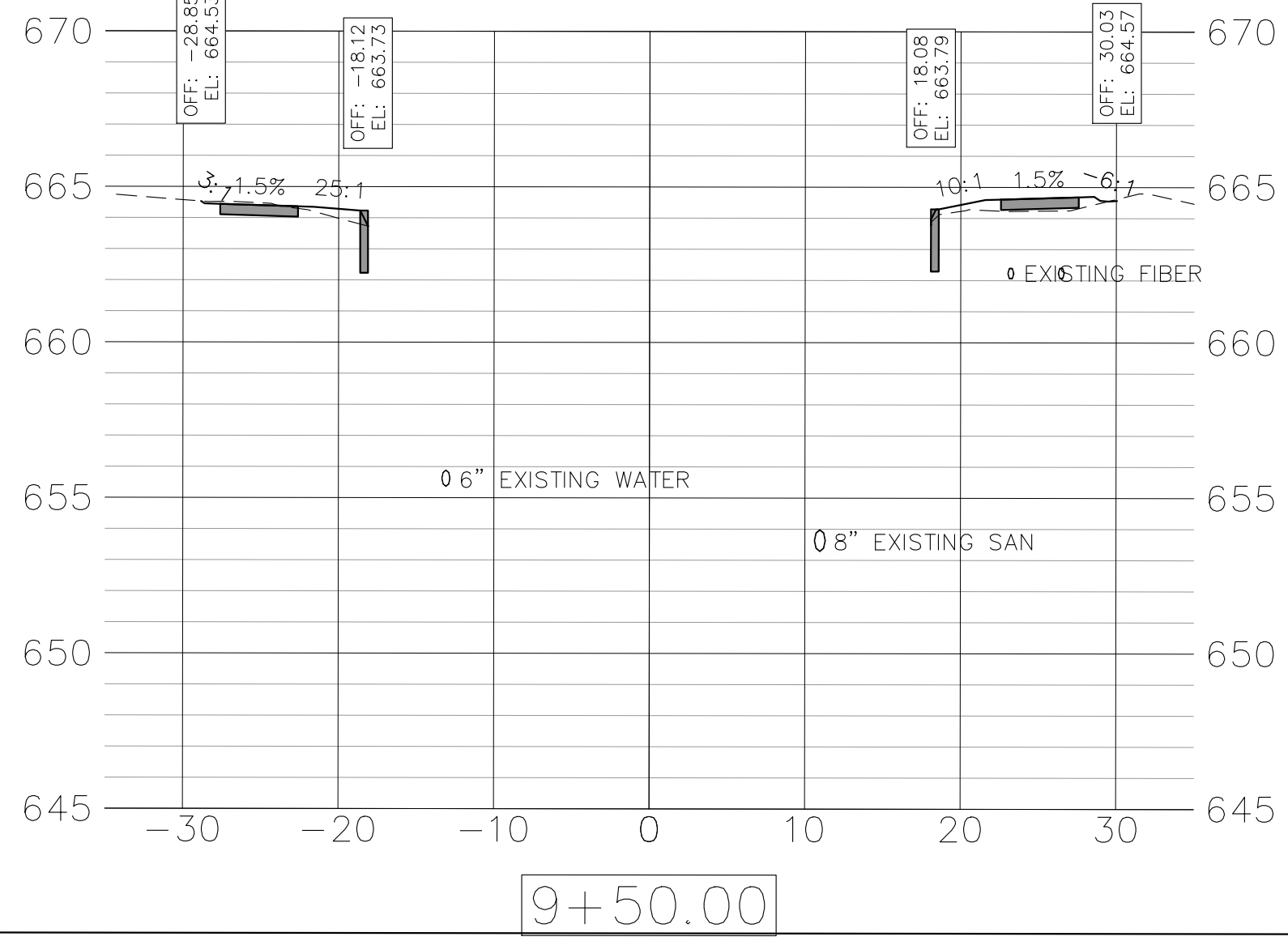
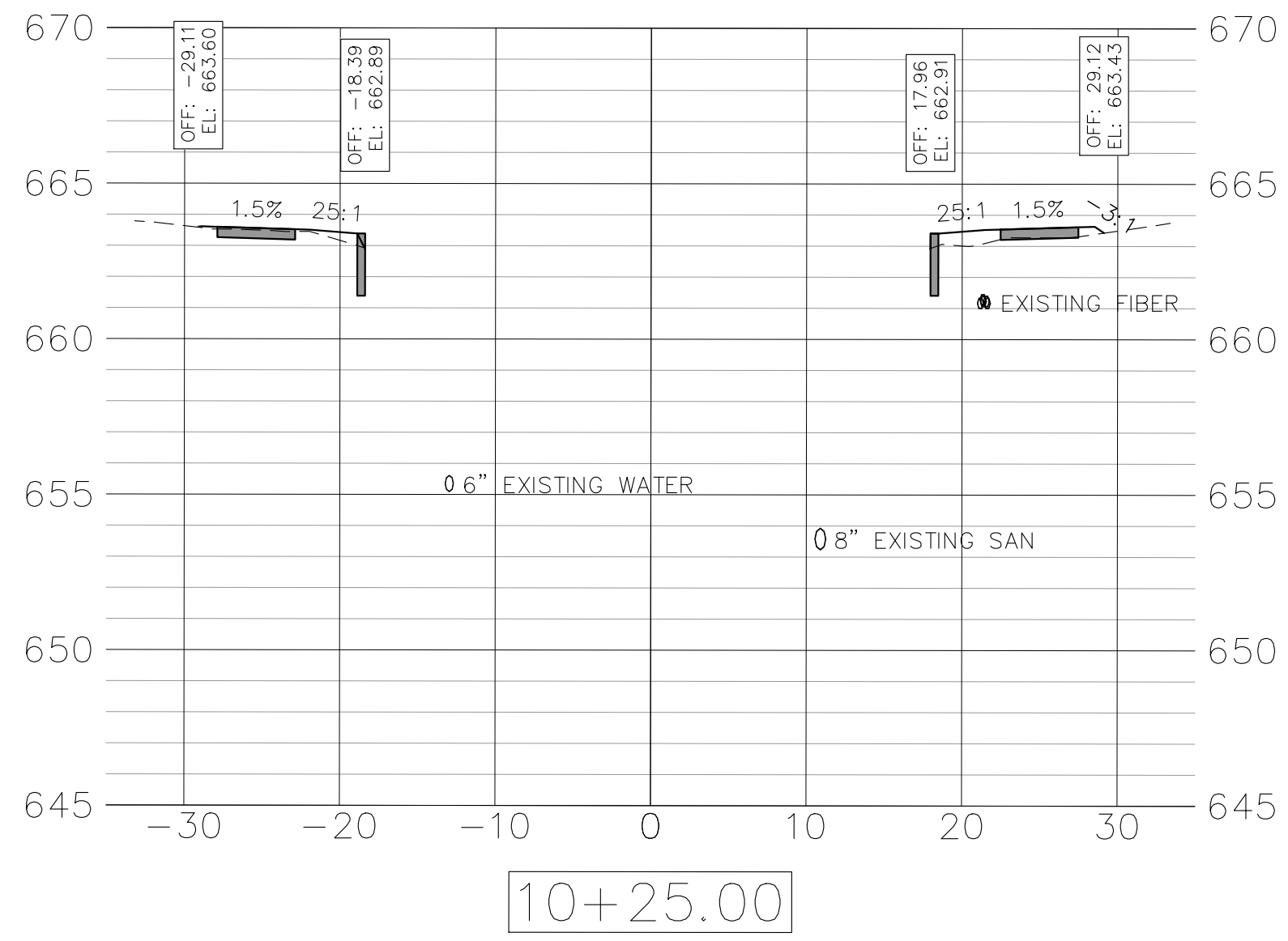


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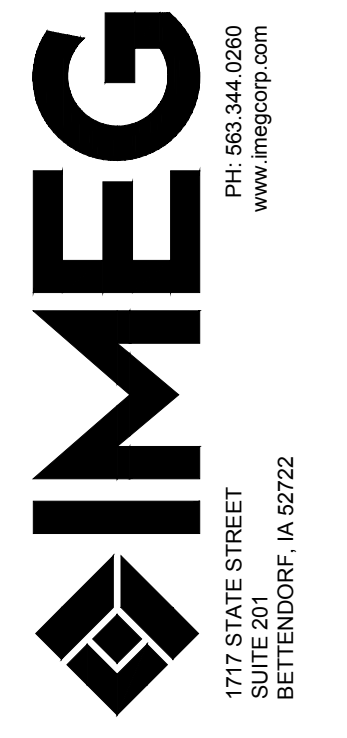


SOUTH CLAY AVENUE RESURFACING PROJECT
 KIRKWOOD, MISSOURI
WOODBINE - MADISON CROSS SECTIONS

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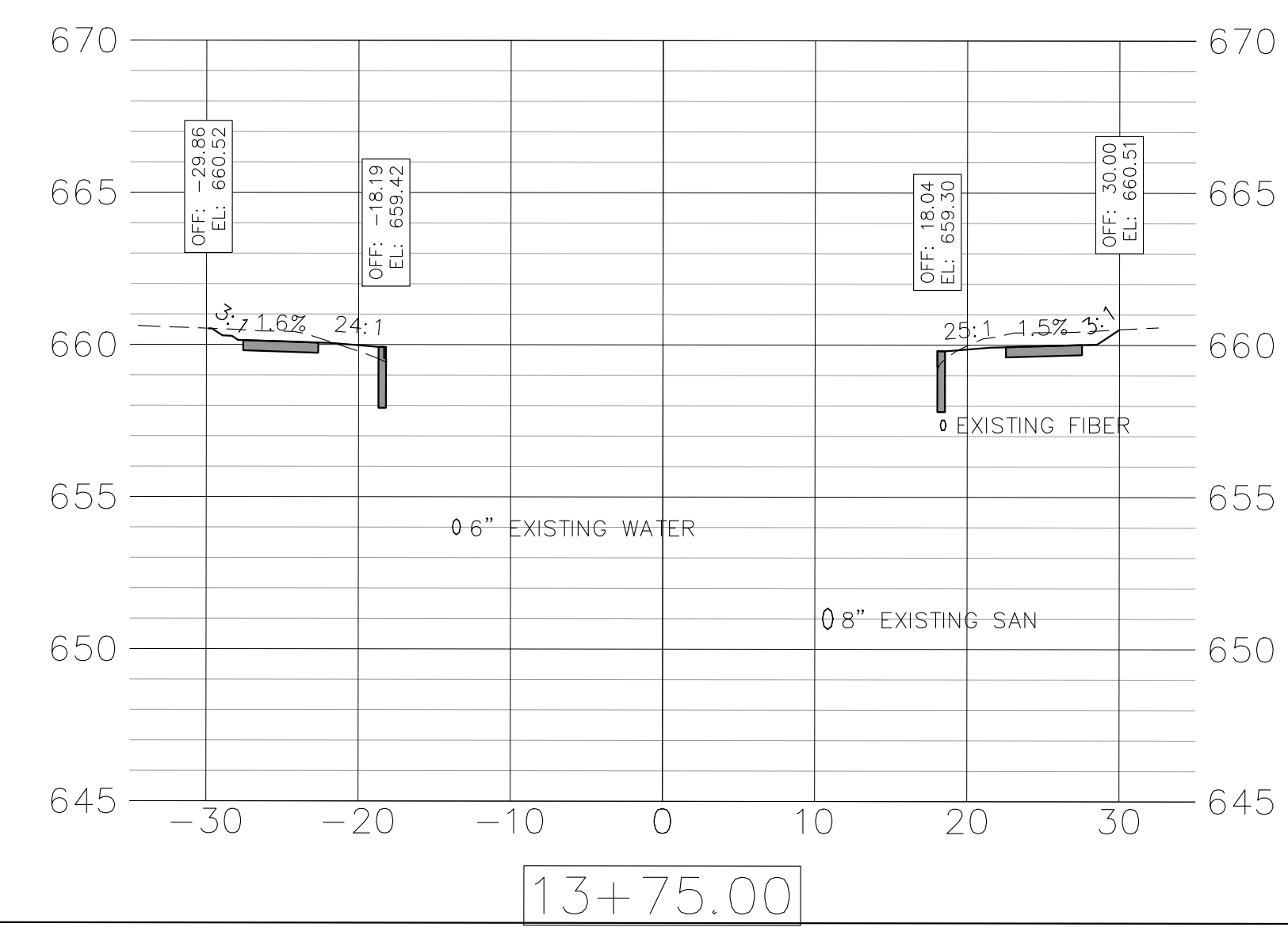
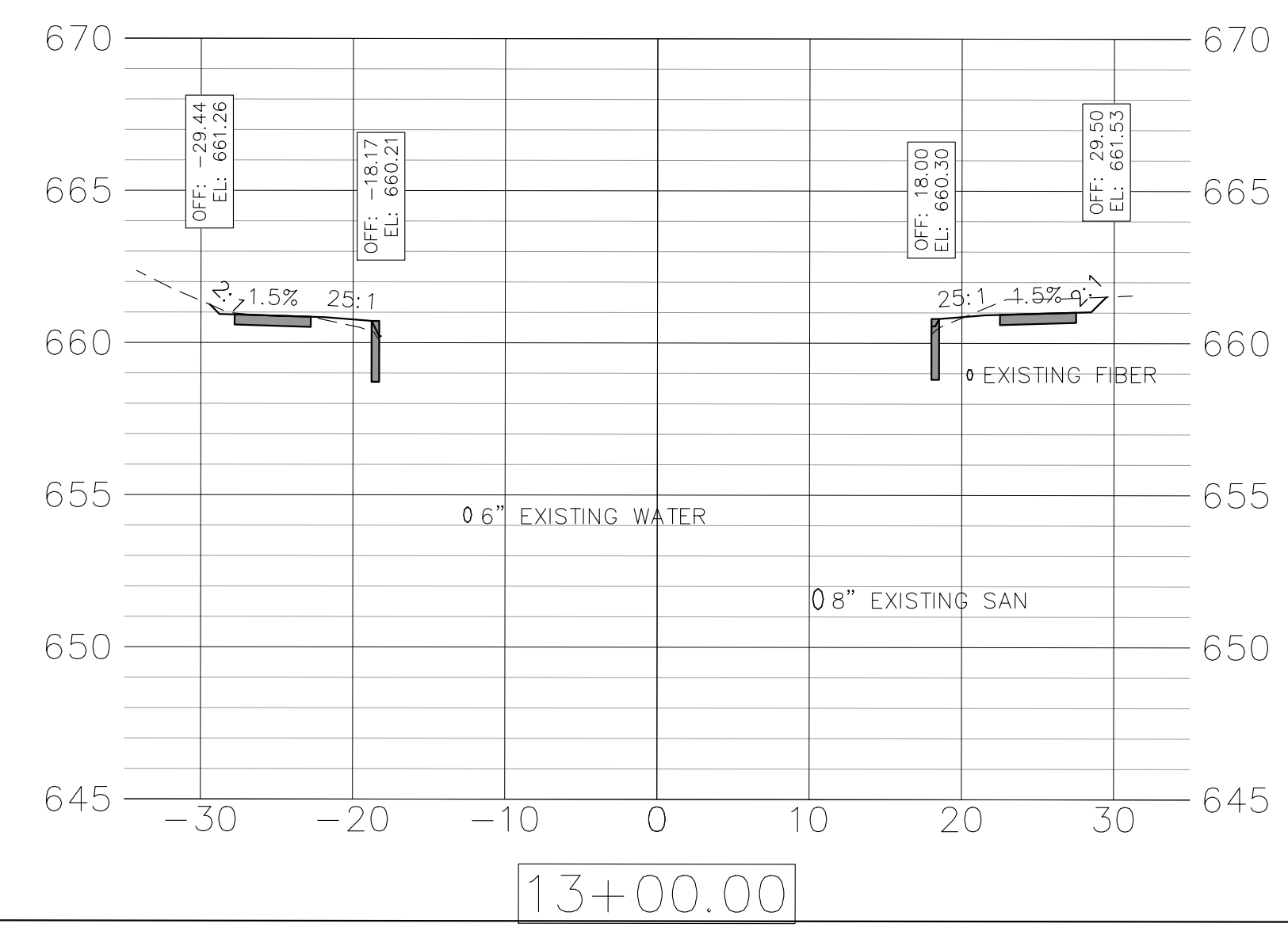
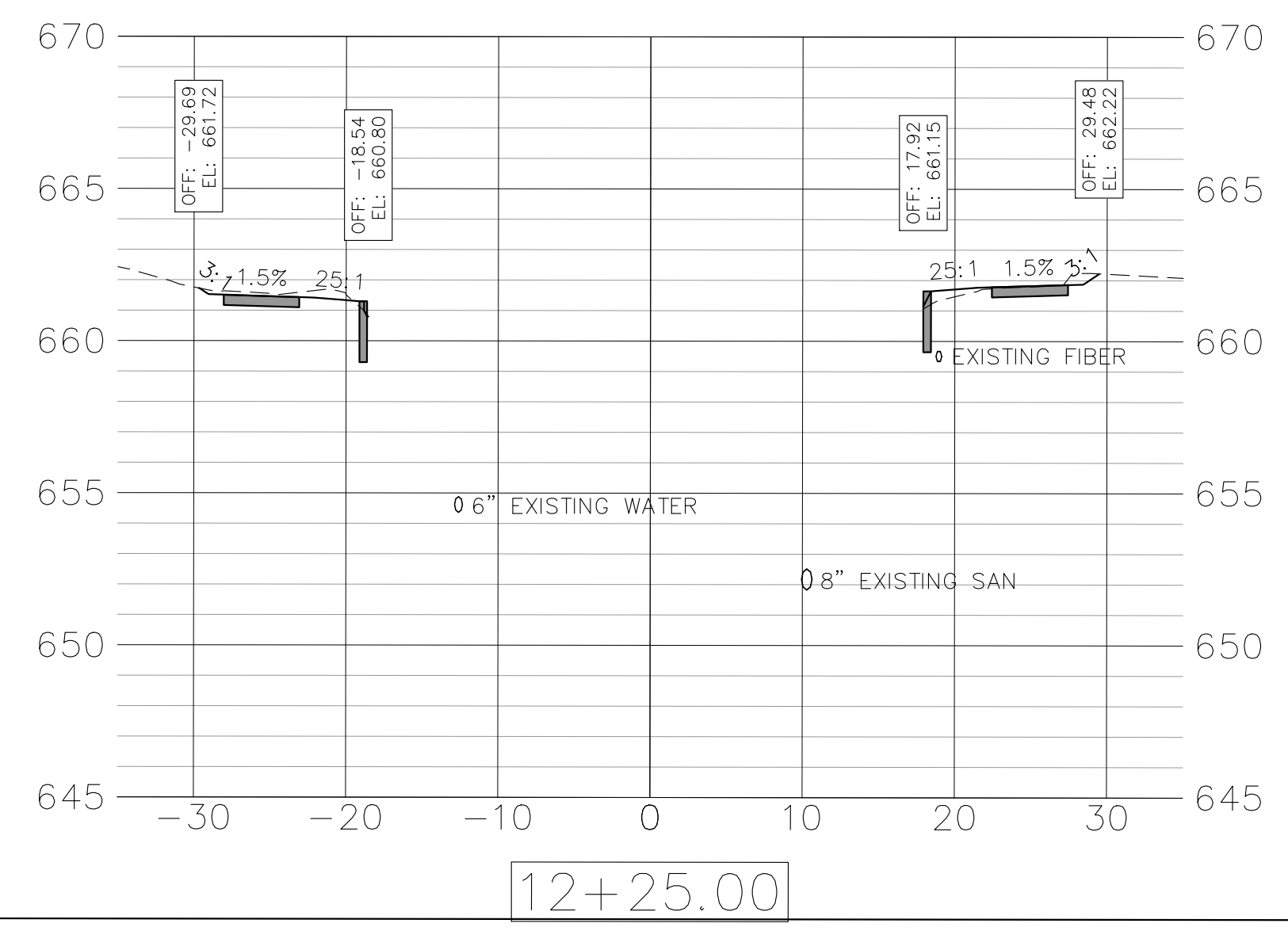
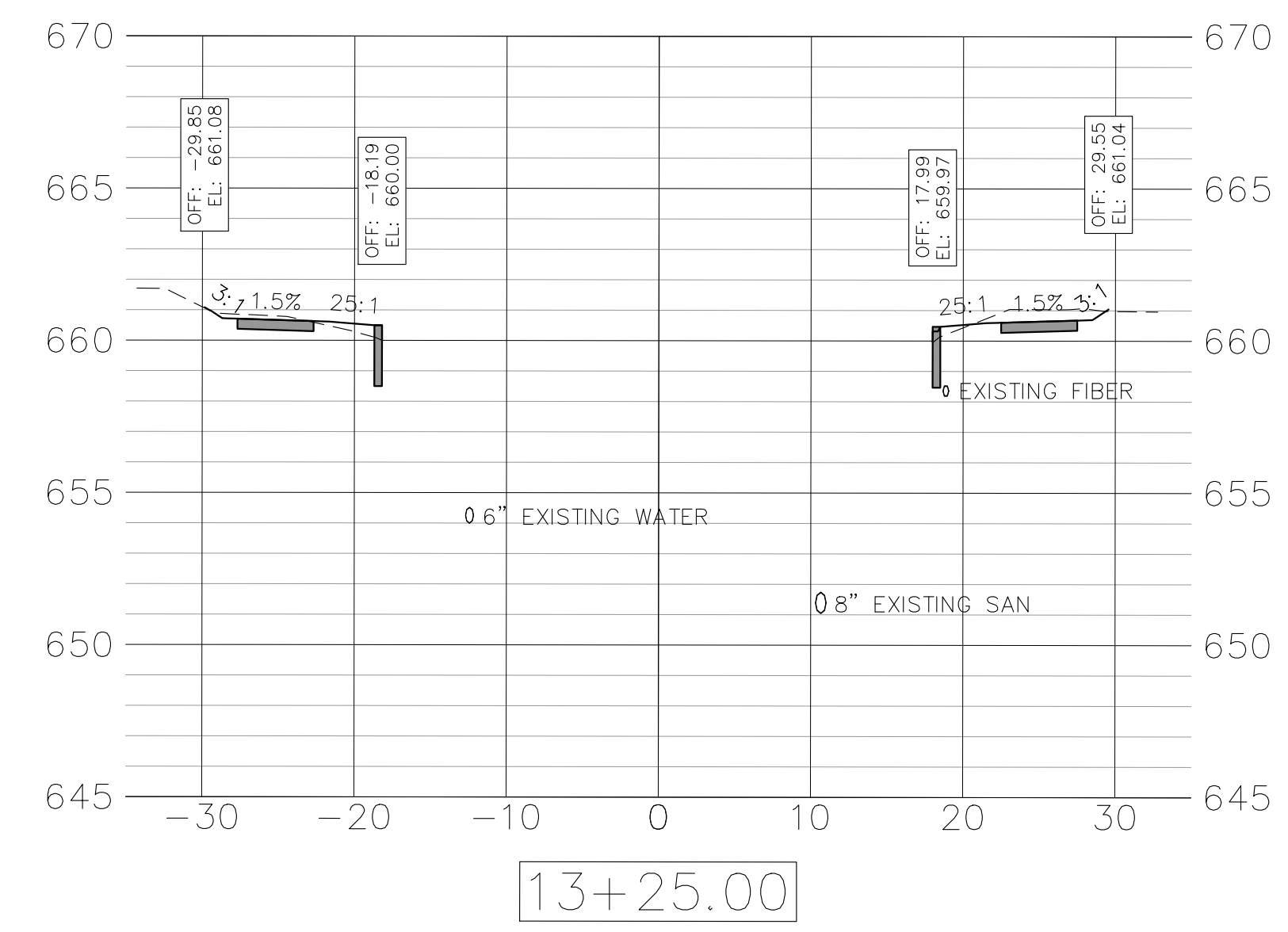
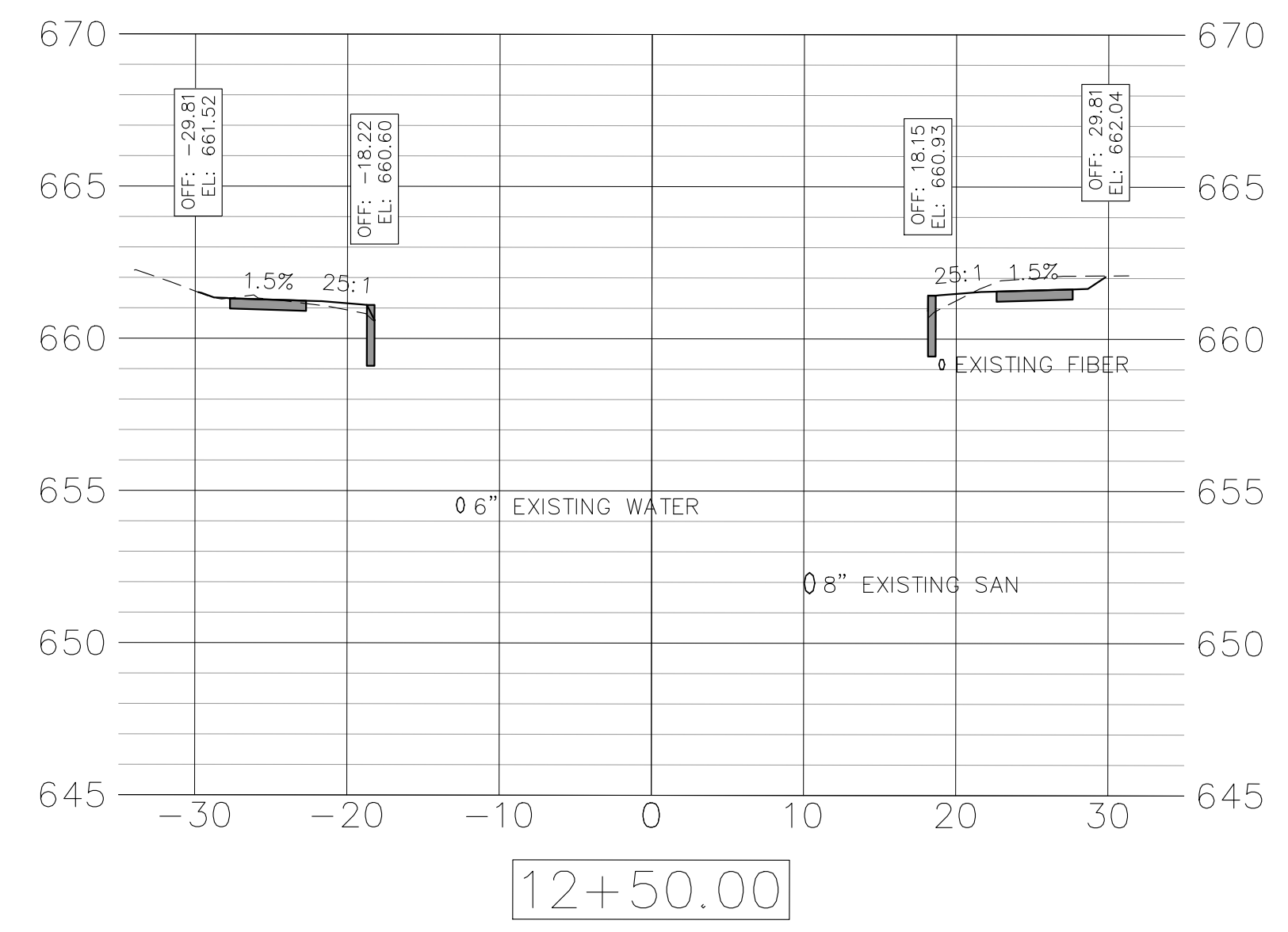
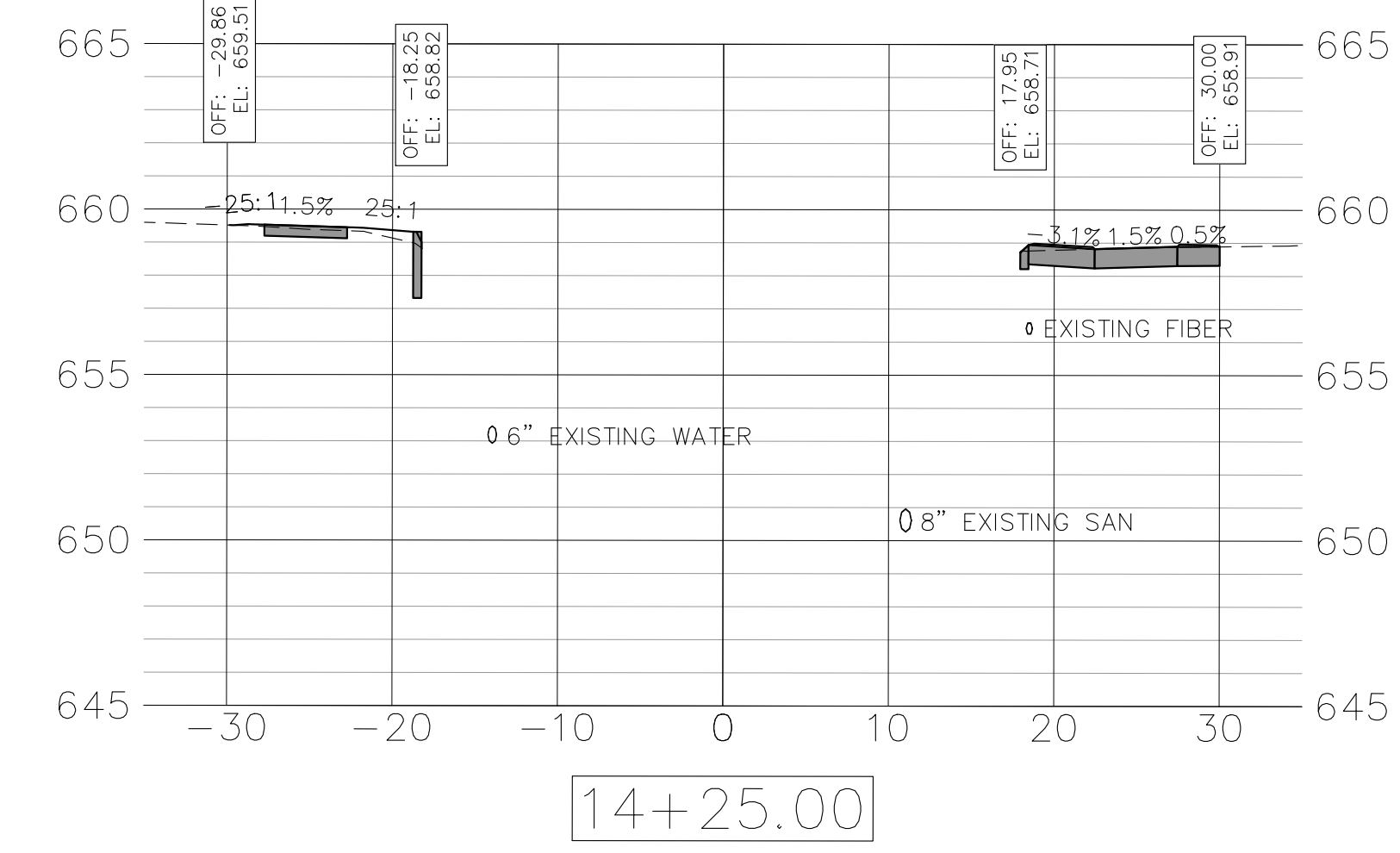
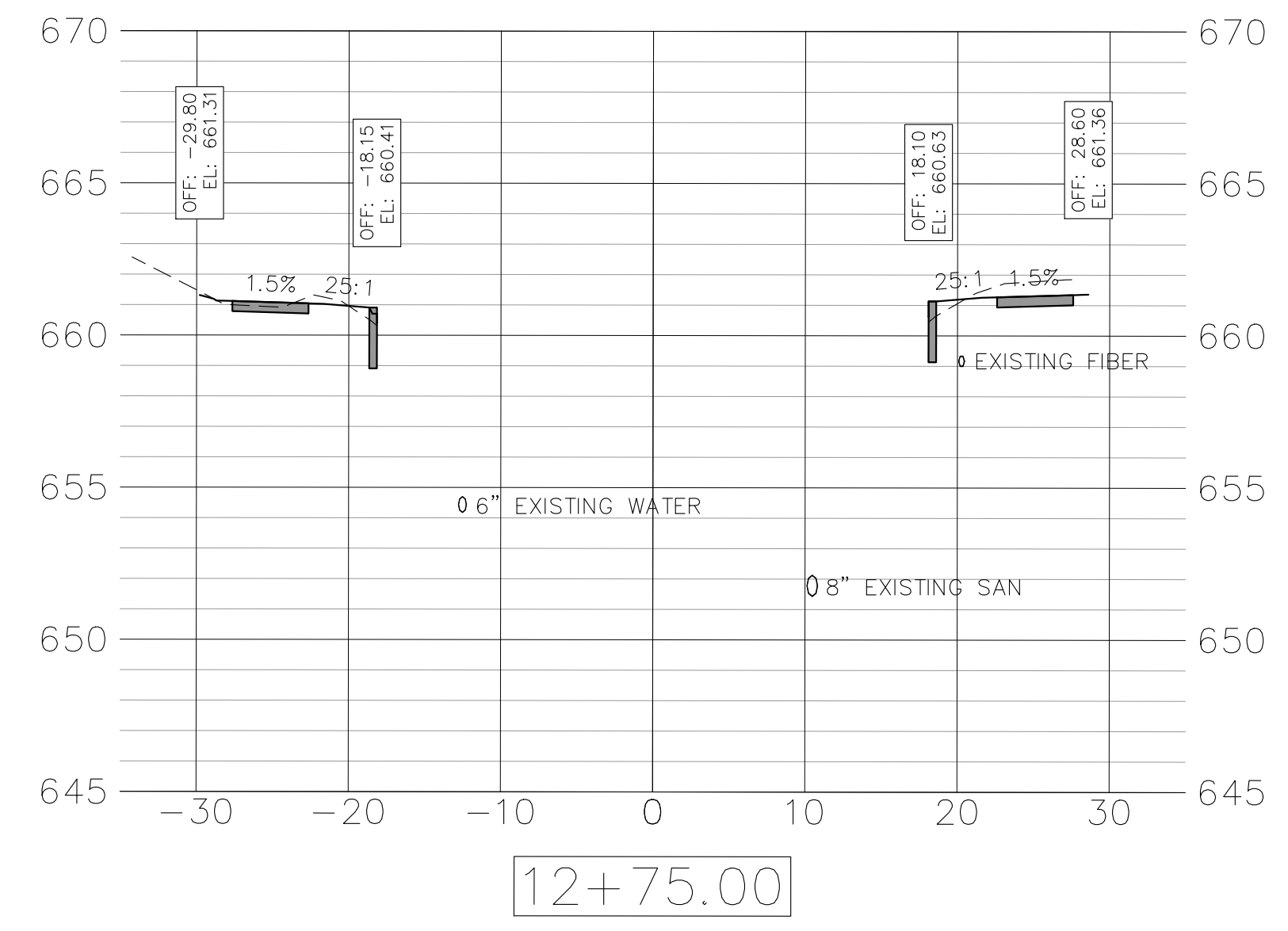


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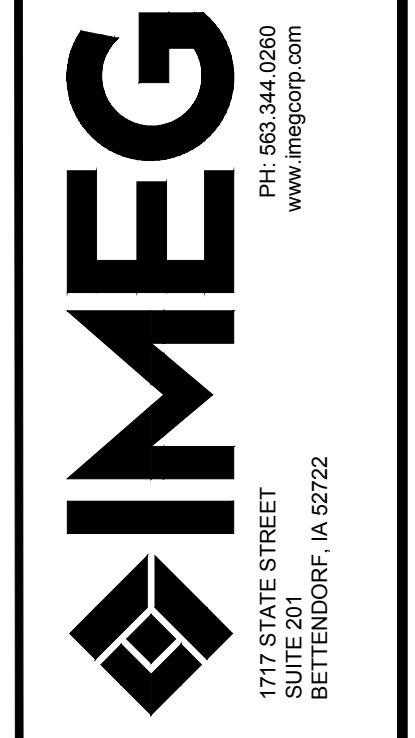


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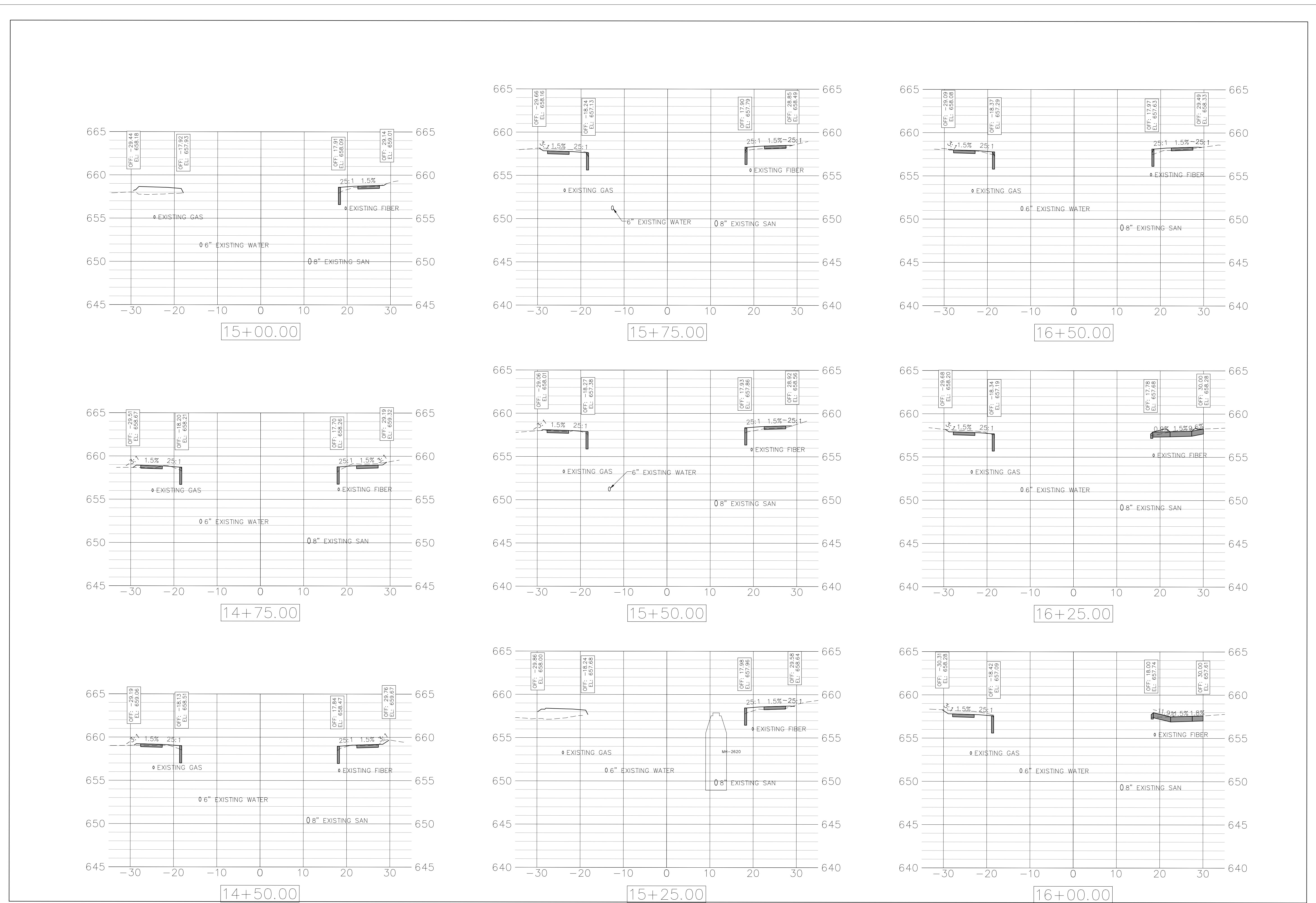


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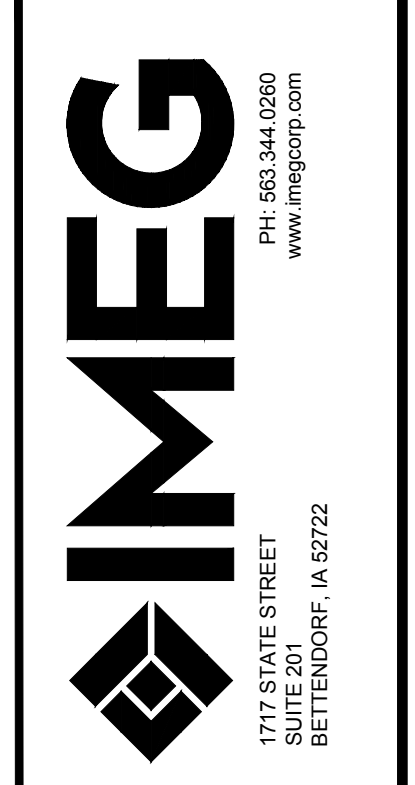


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 KIRKWOOD, MISSOURI
WOODBINE - MADISON CROSS SECTIONS

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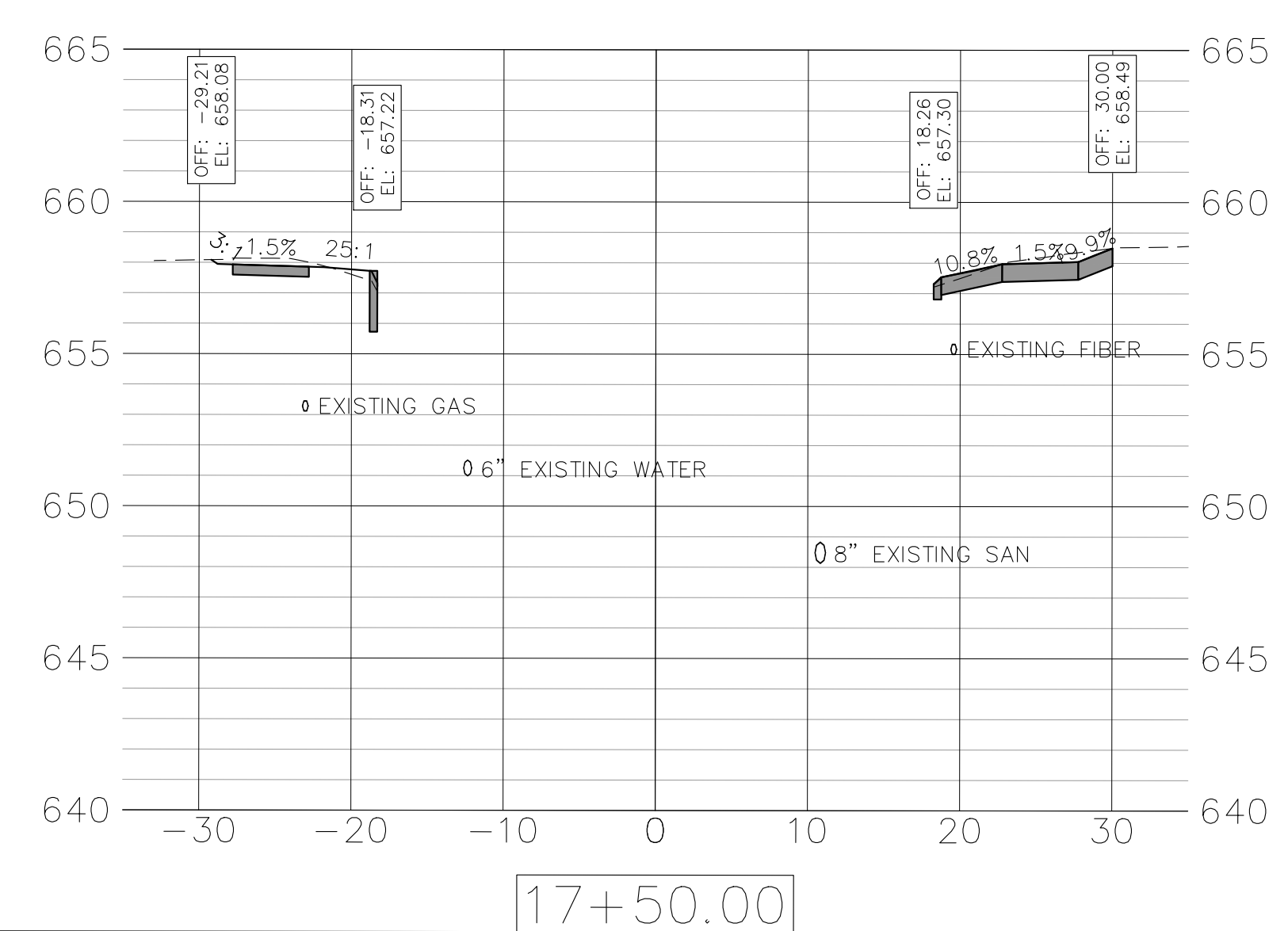
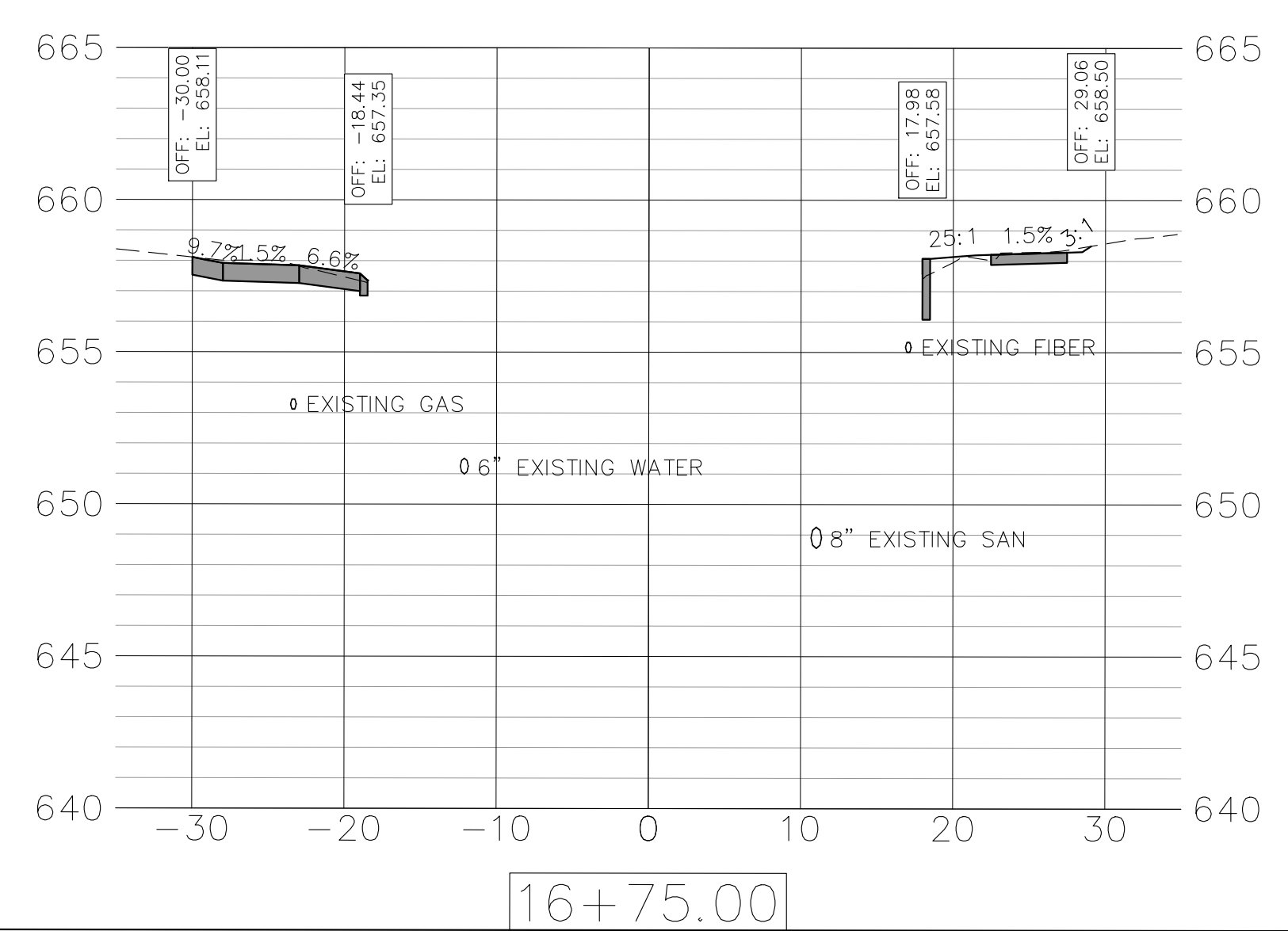
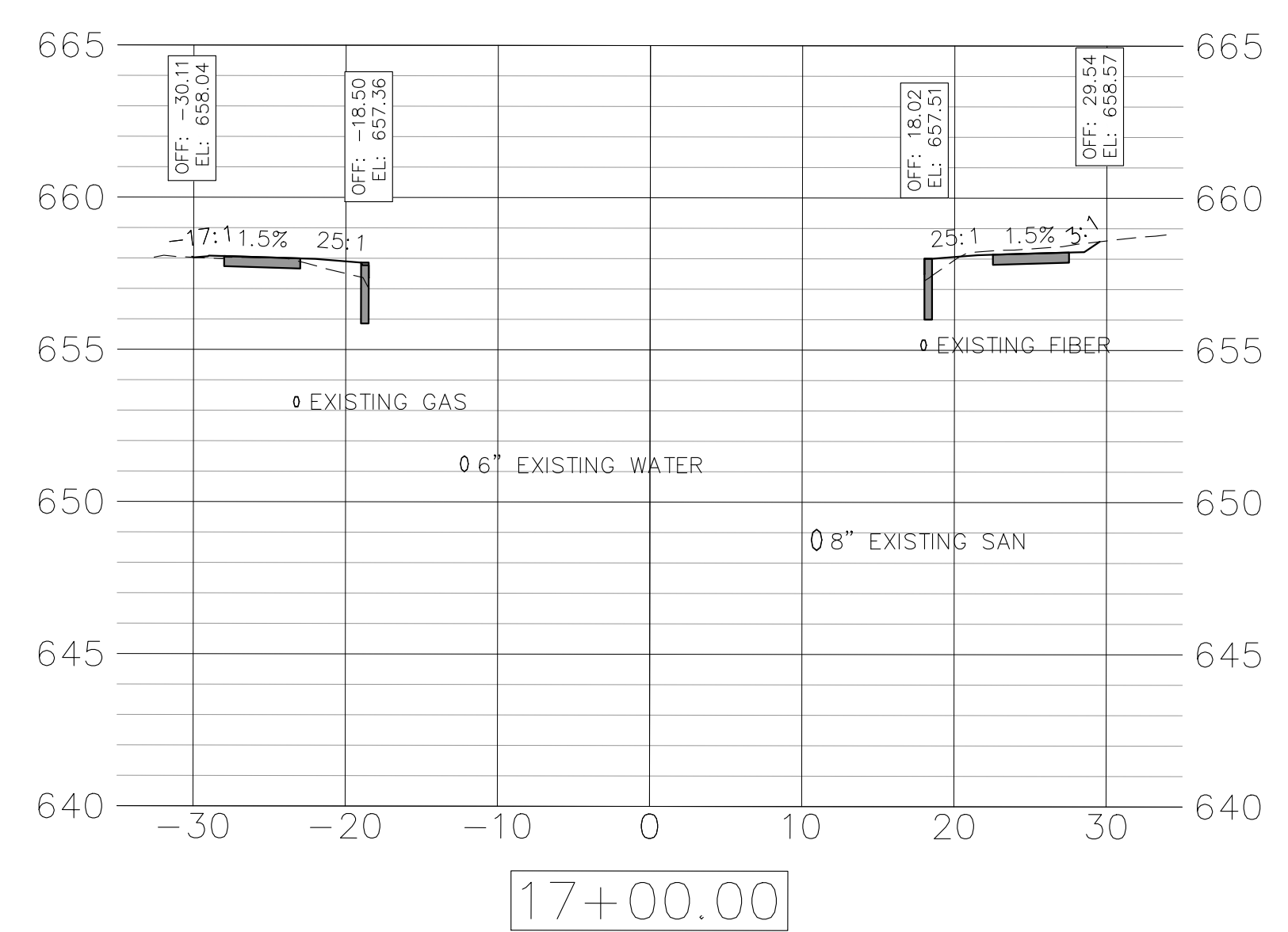
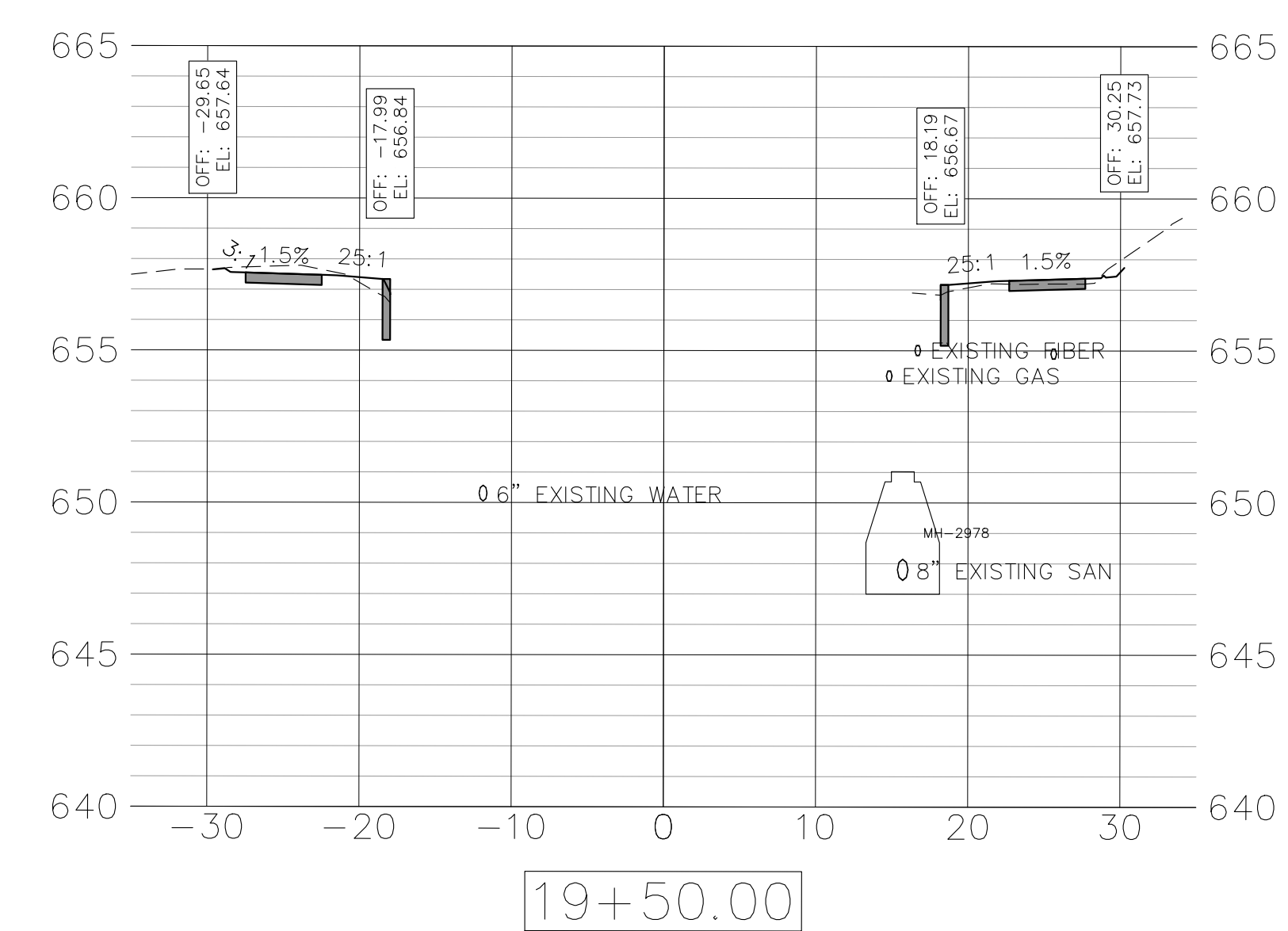
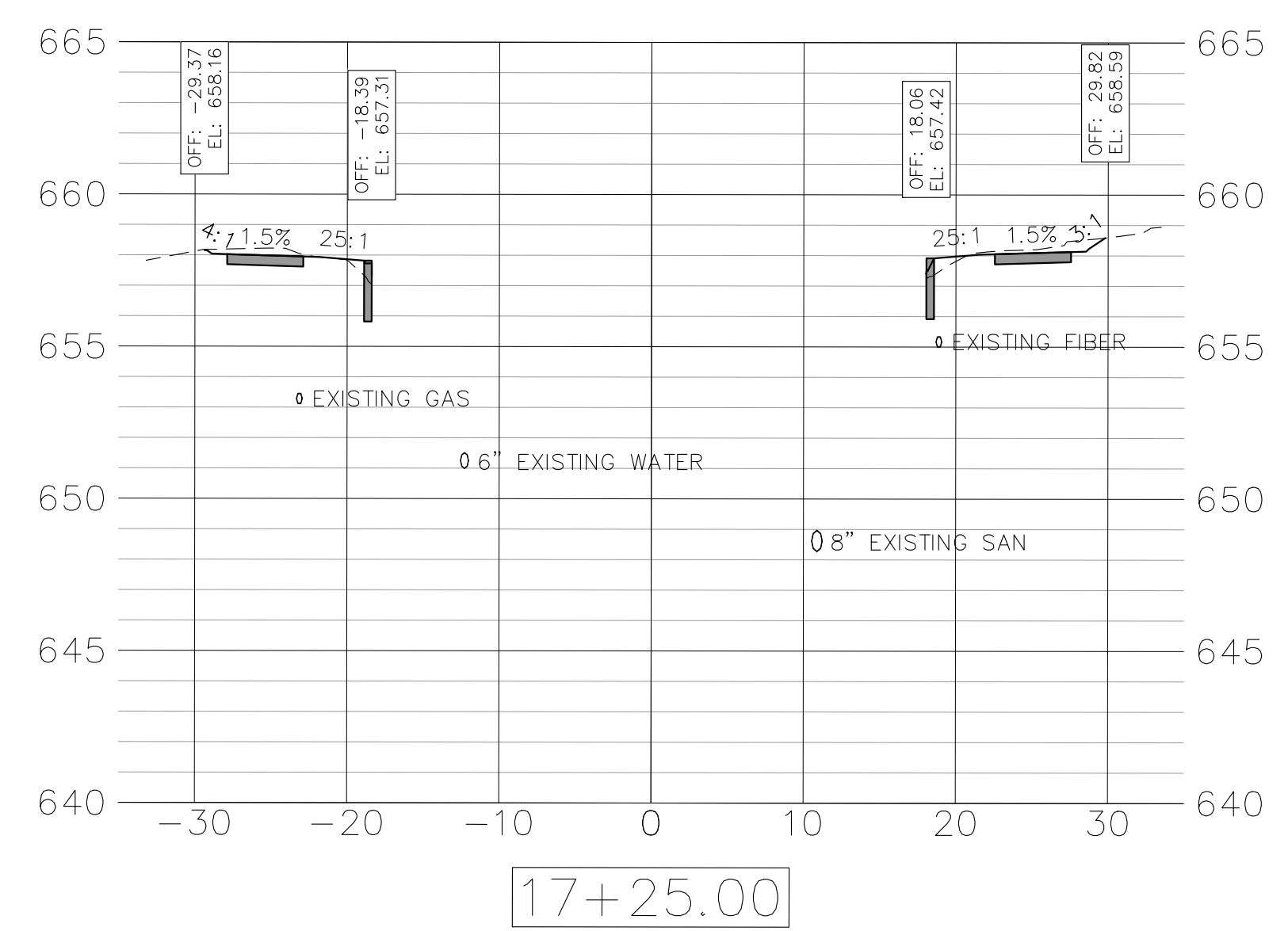
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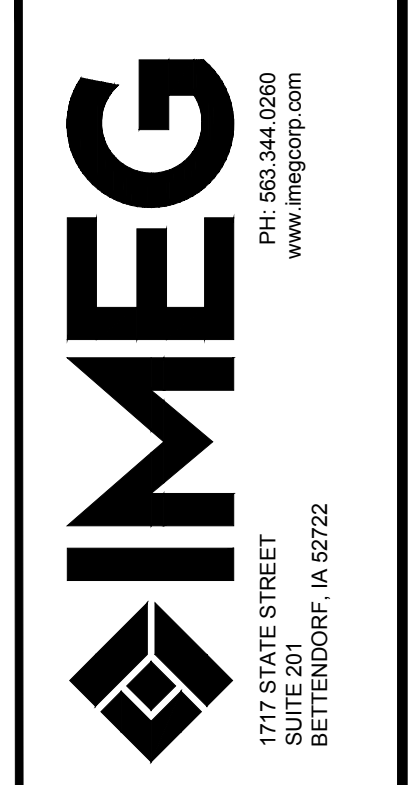
SOUTH CLAY AVENUE RESURFACING PROJECT
 KIRKWOOD, MISSOURI
WOODBINE - MADISON CROSS SECTIONS

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 KIRKWOOD, MISSOURI

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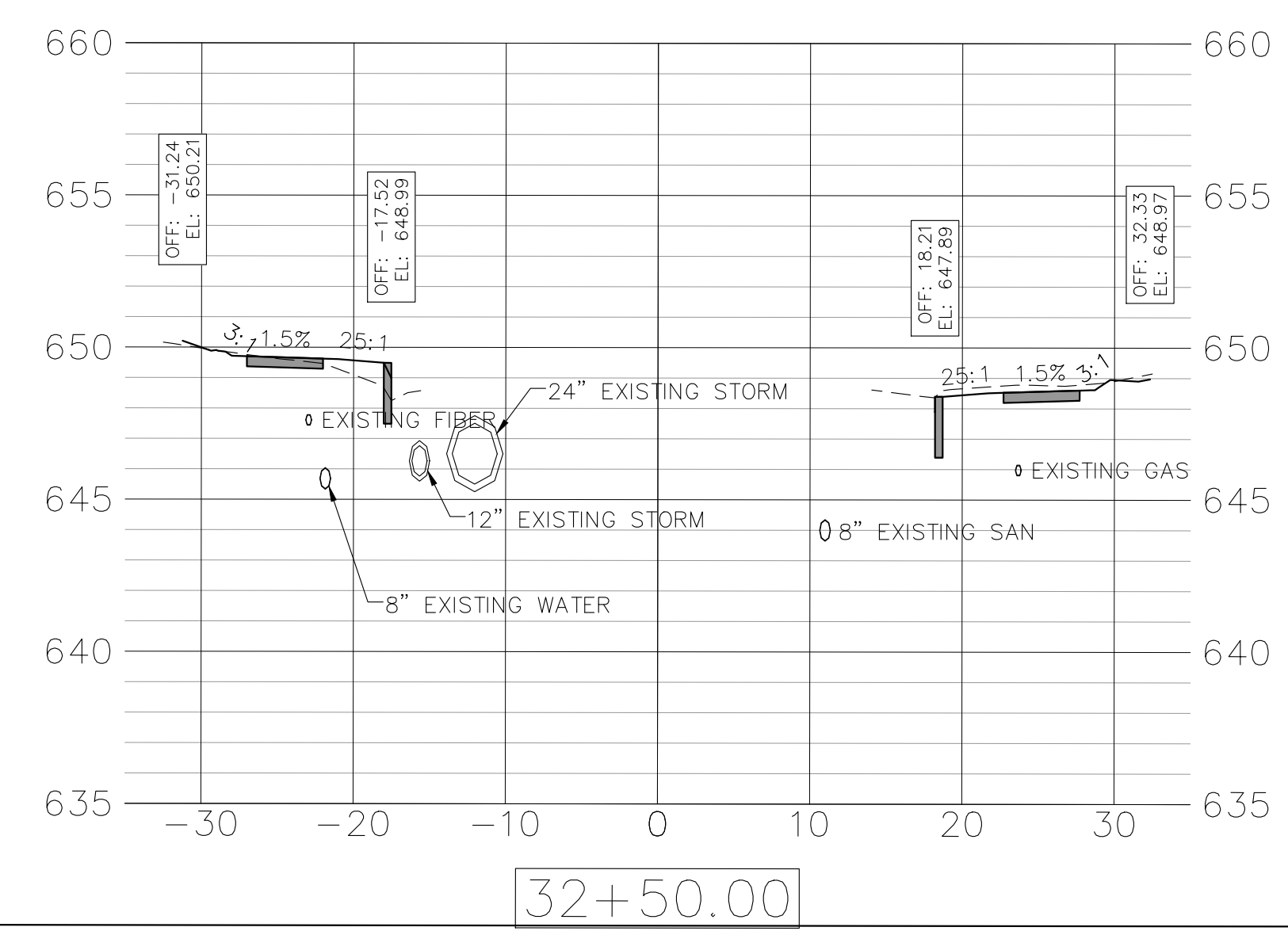
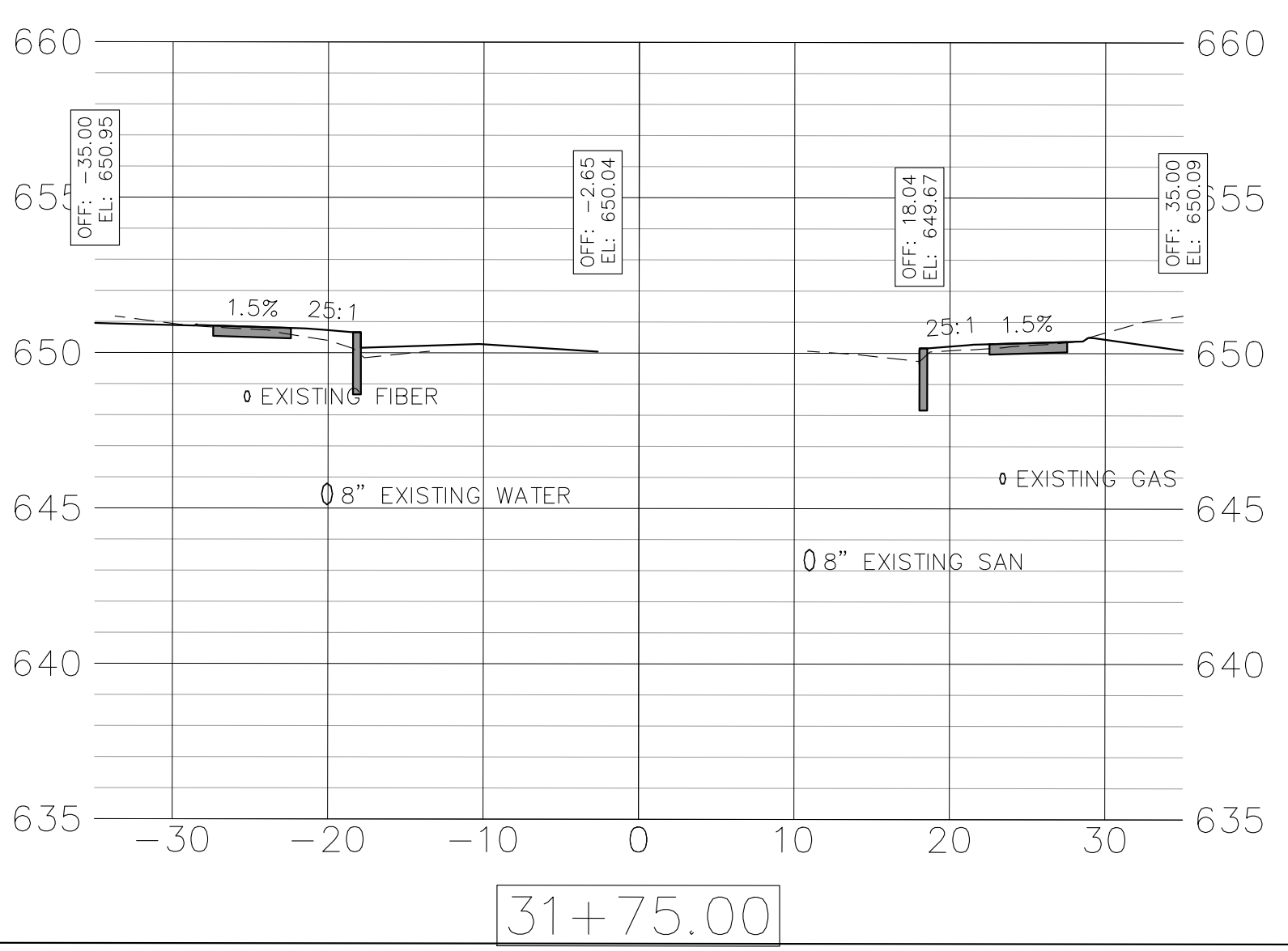
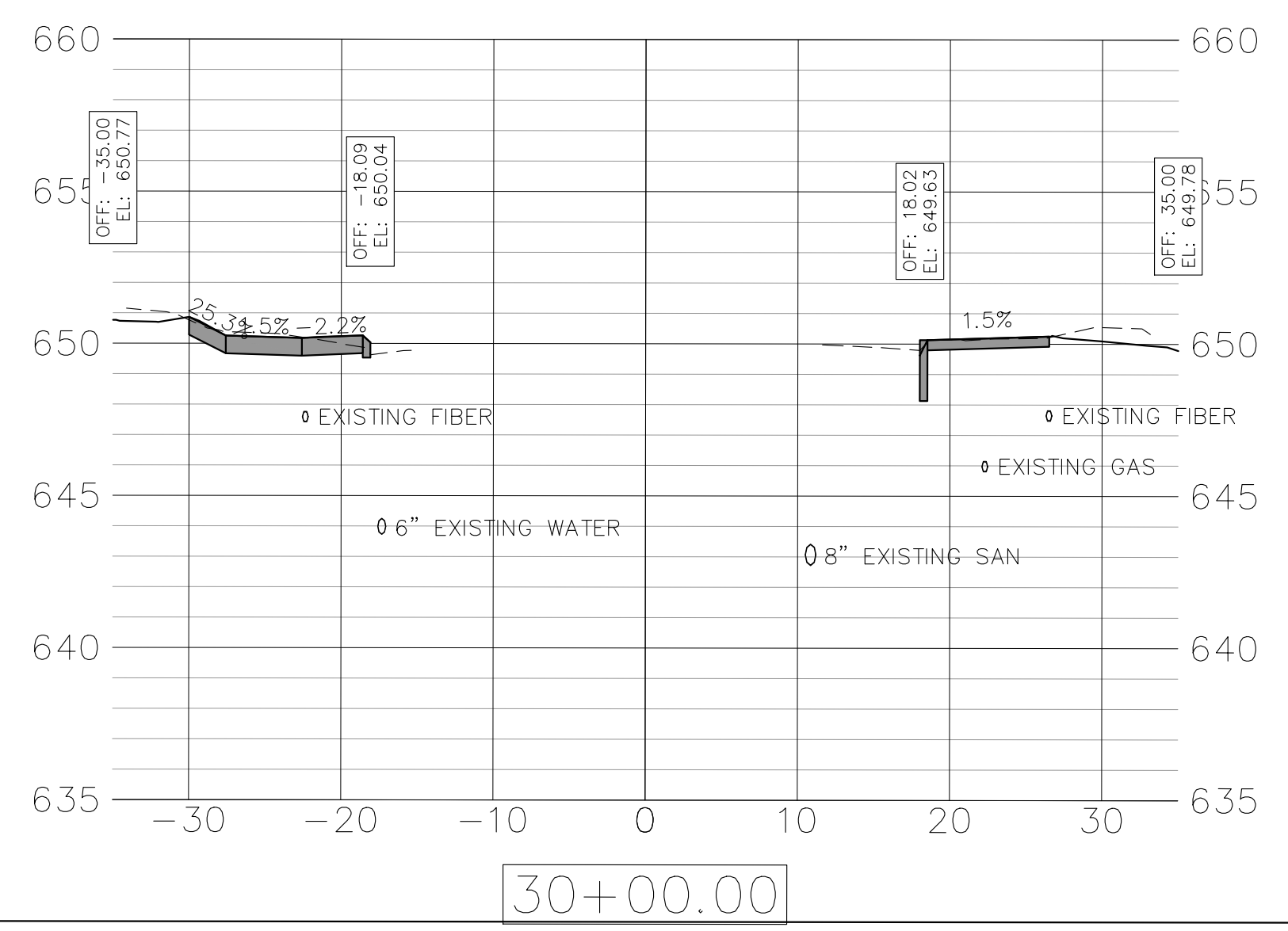
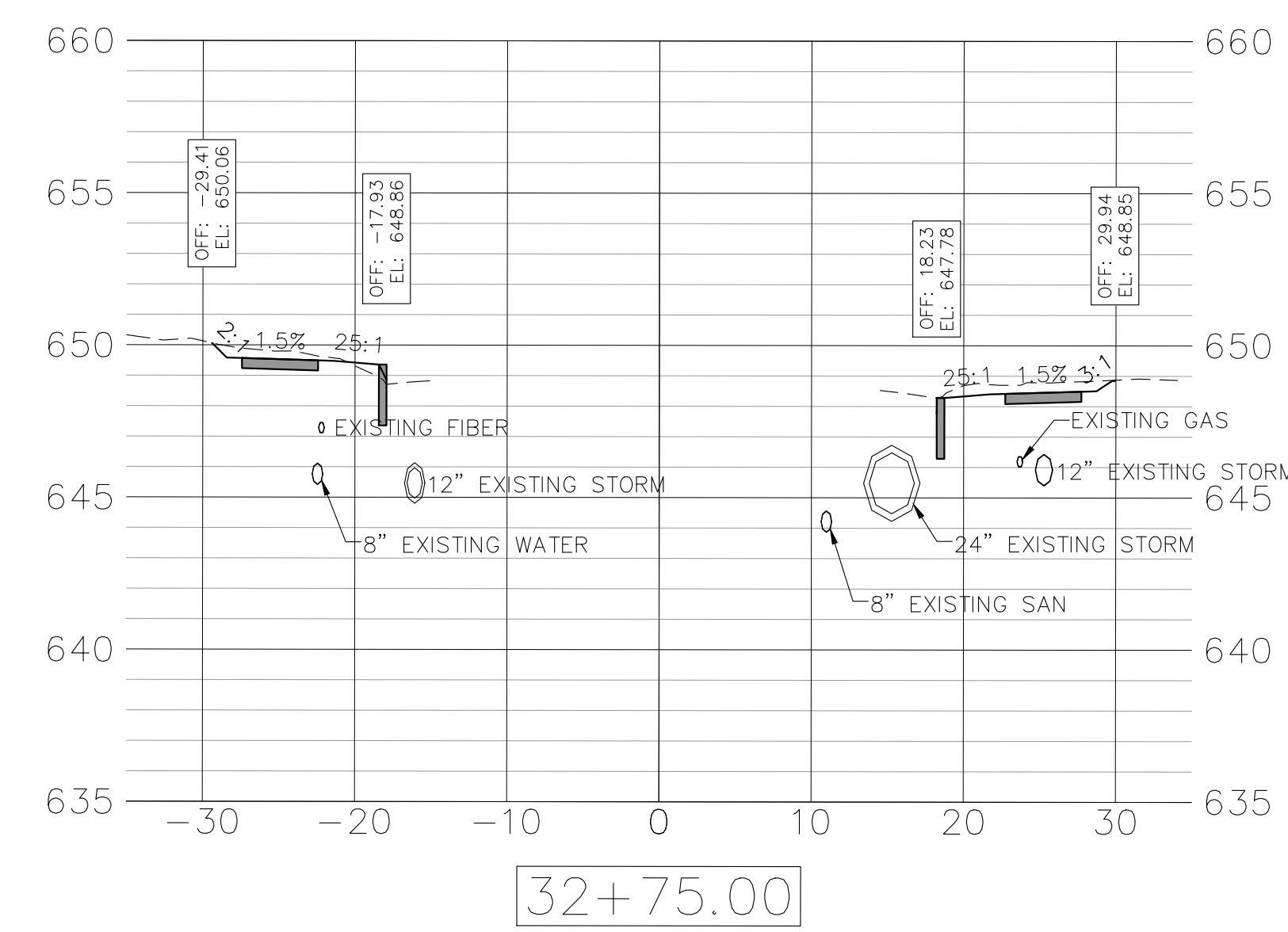
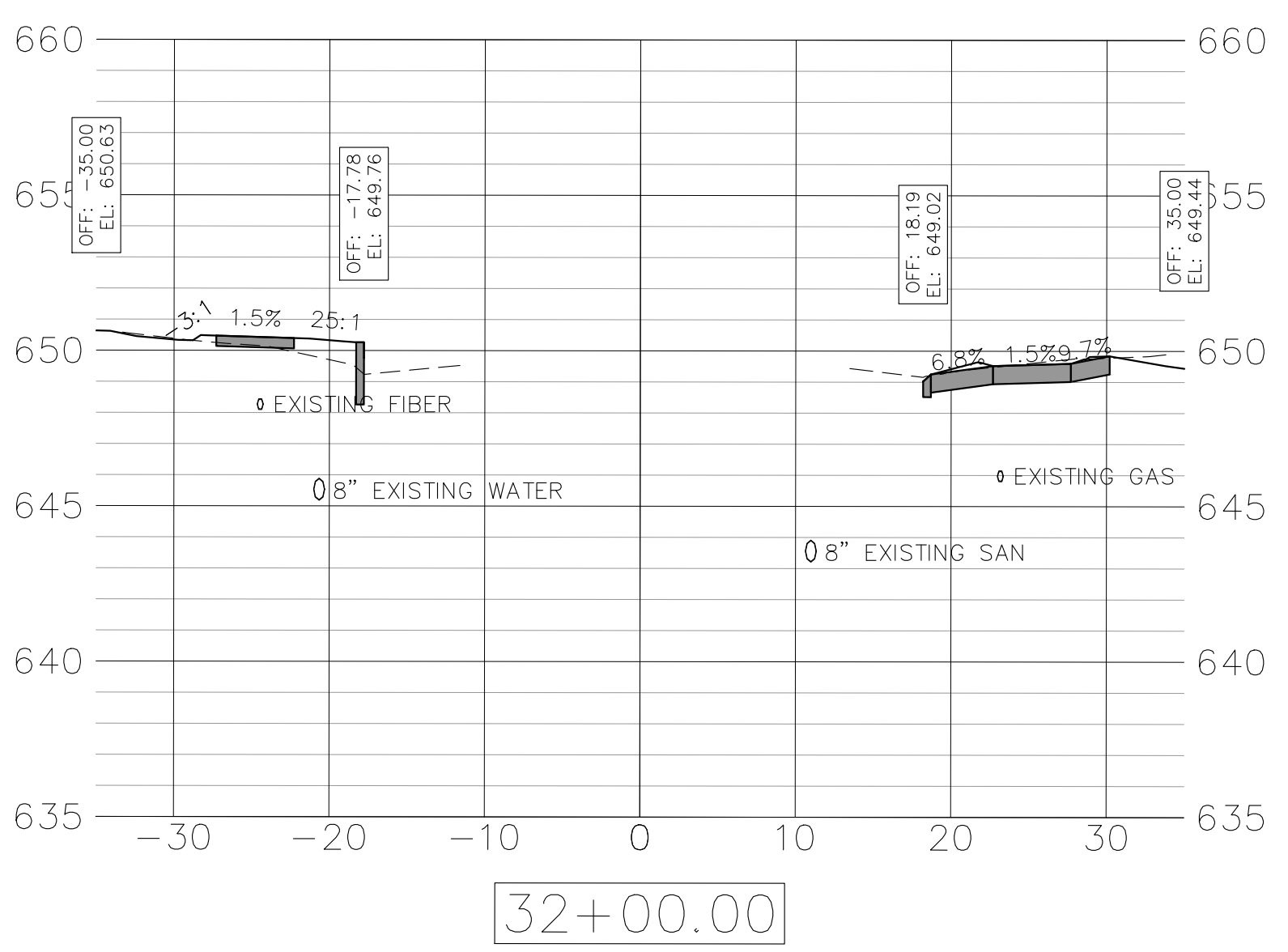
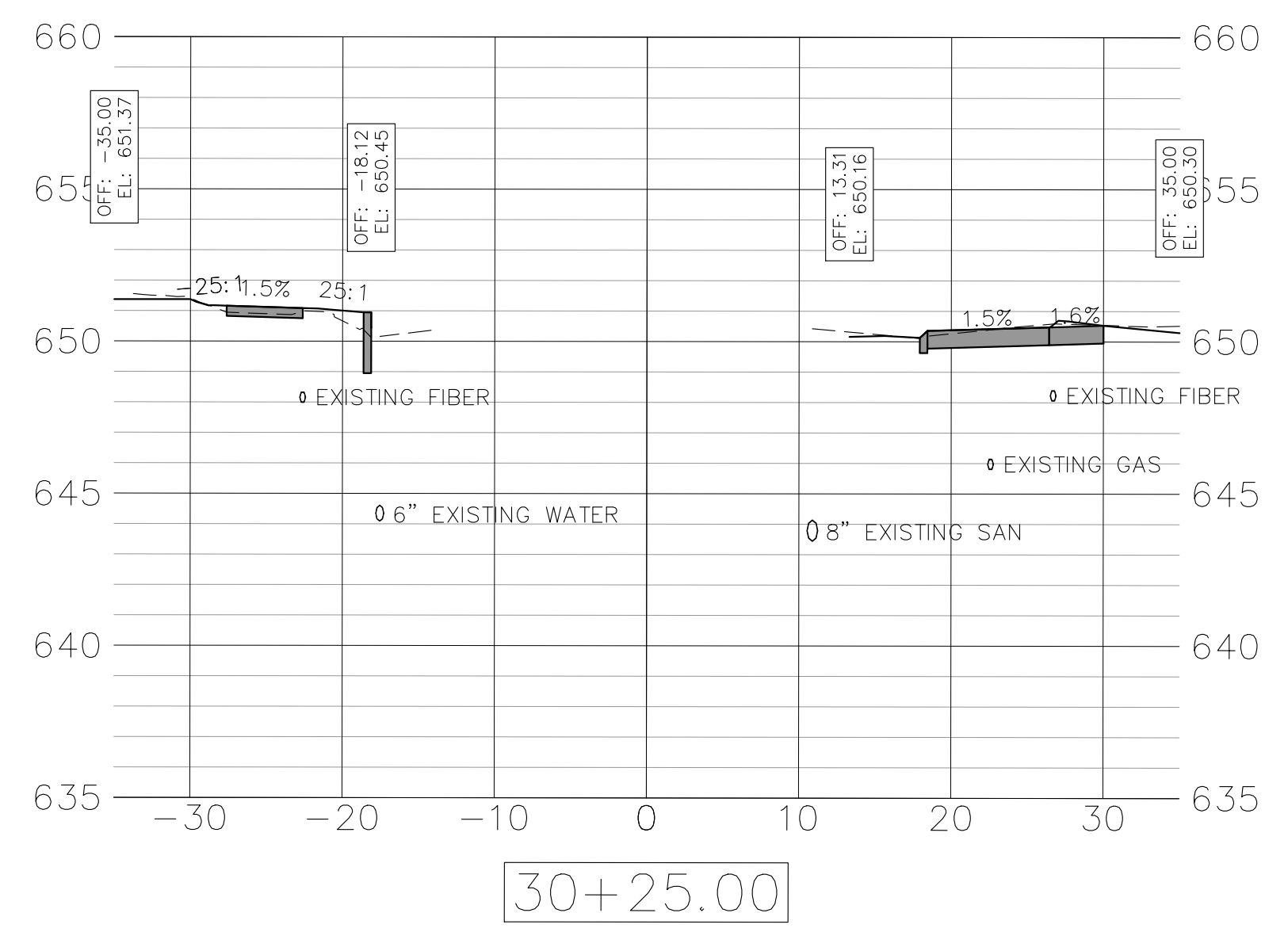
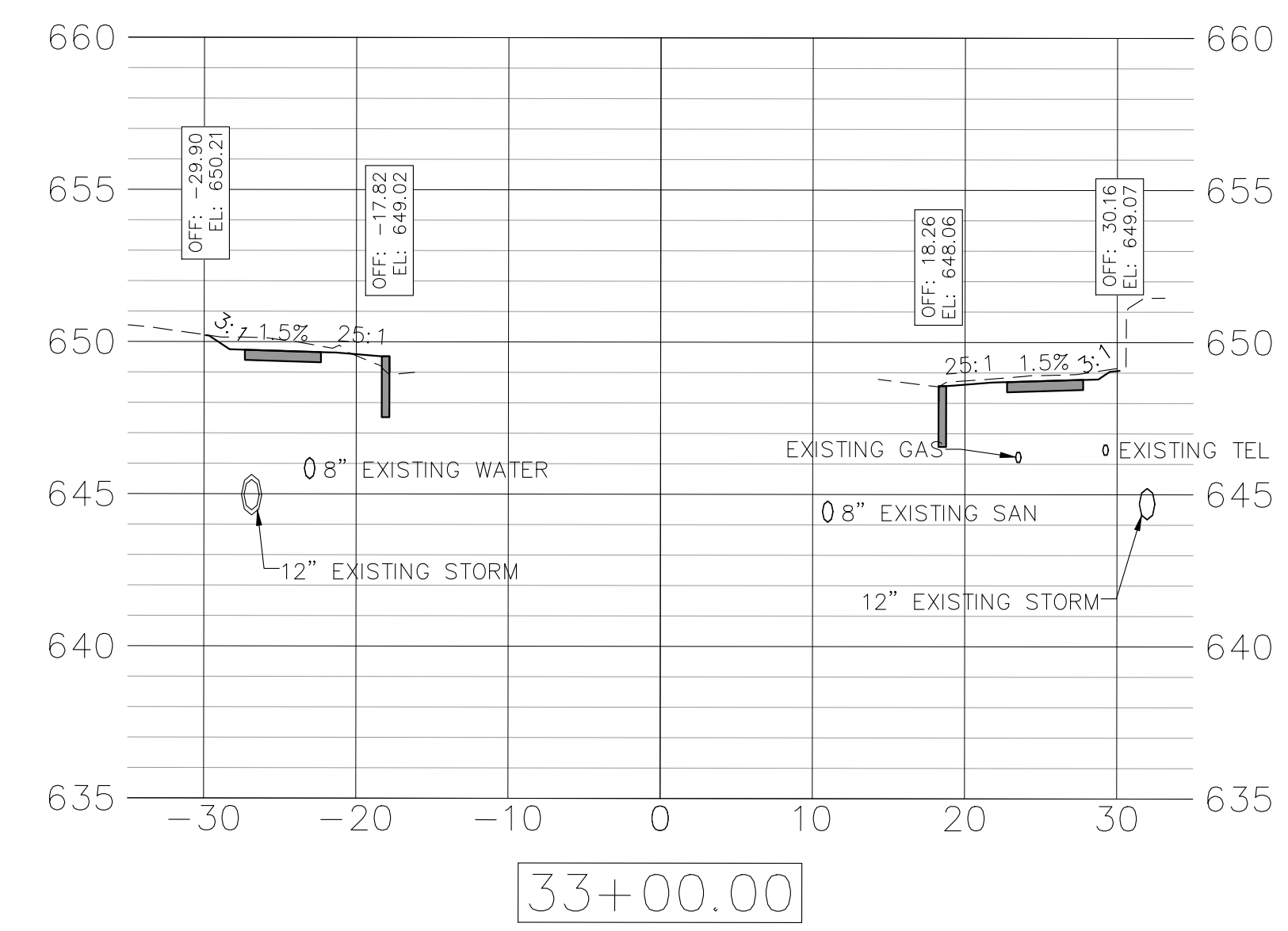
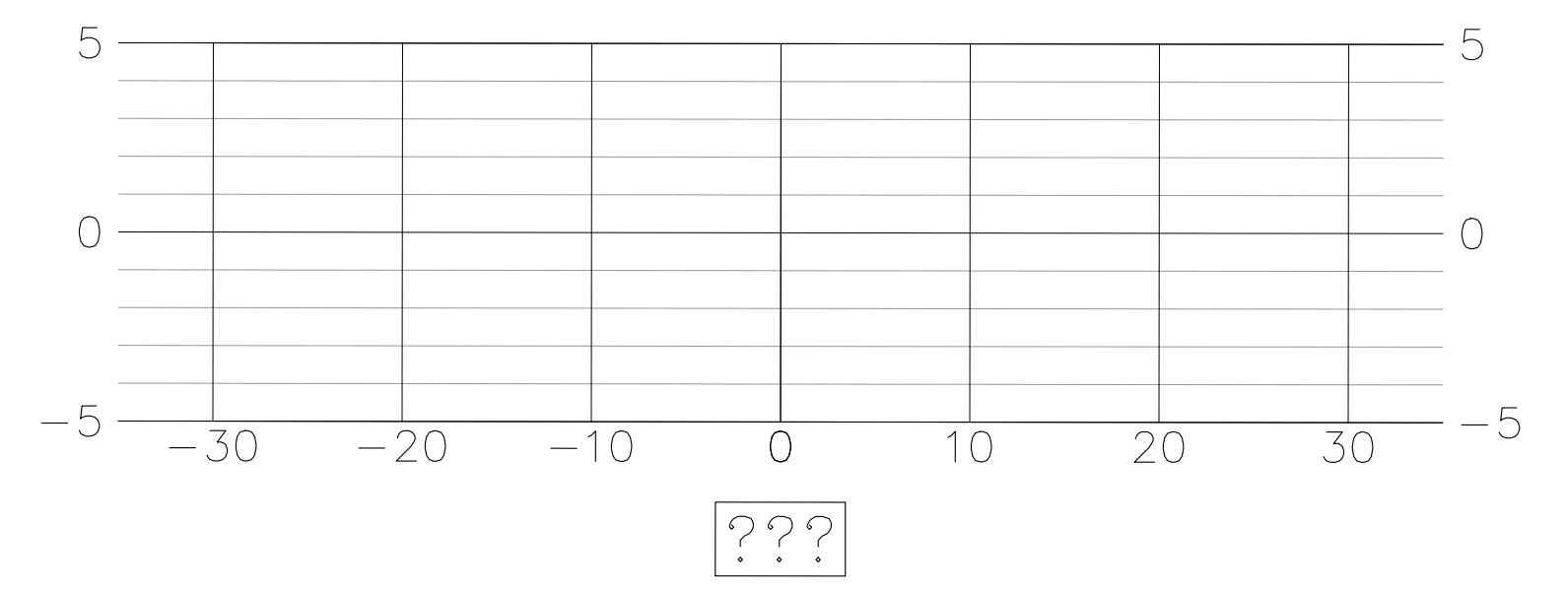
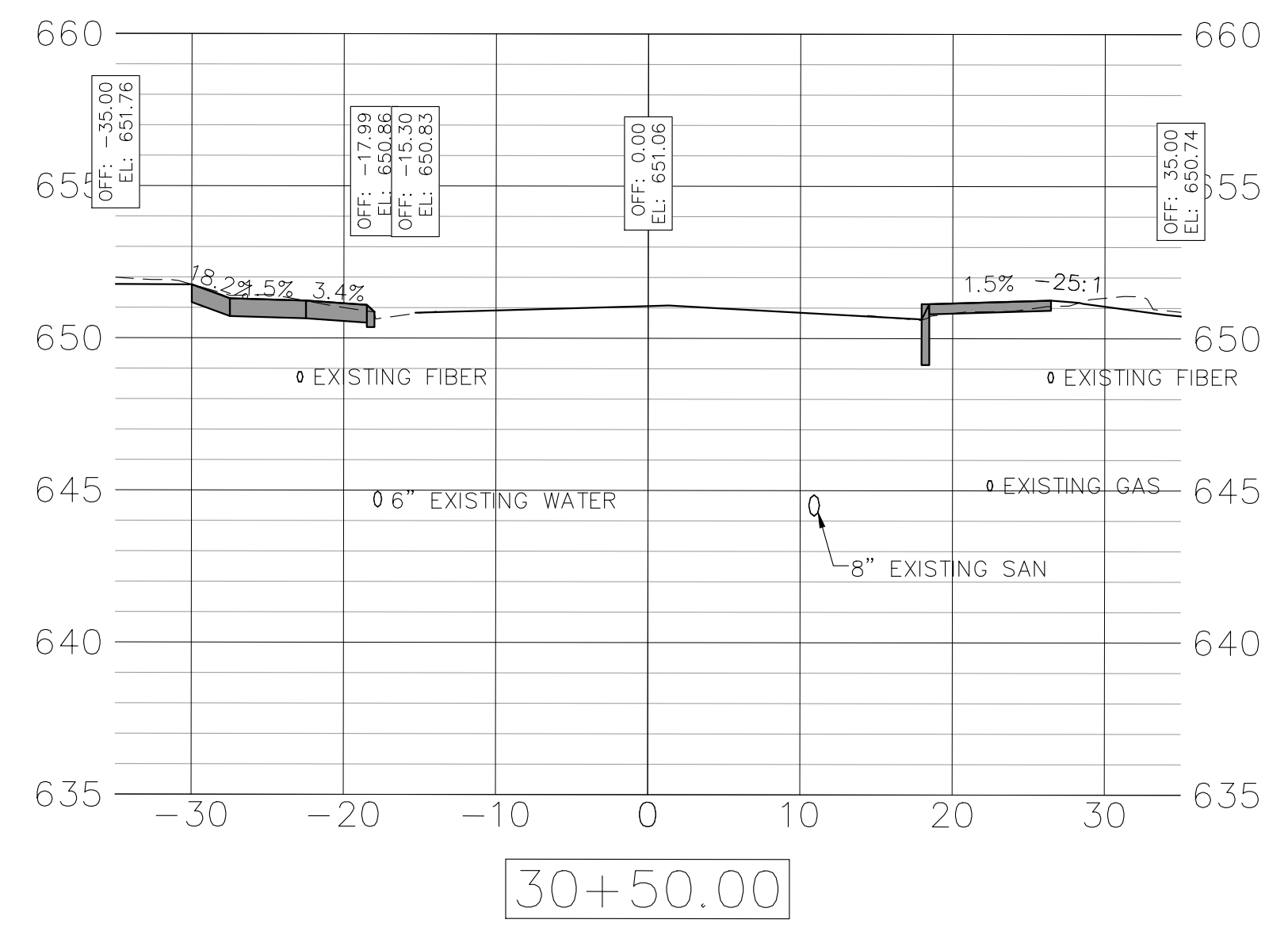
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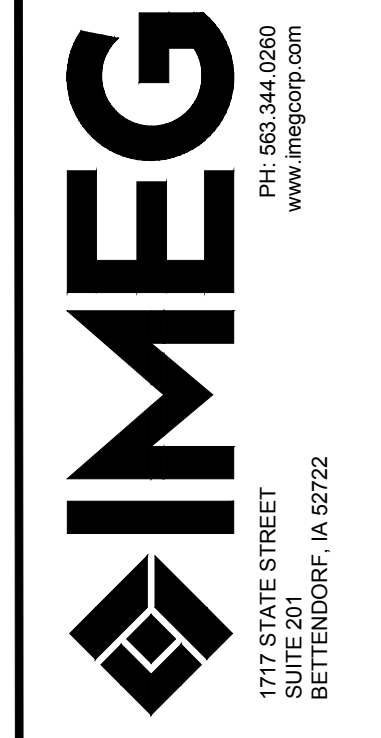
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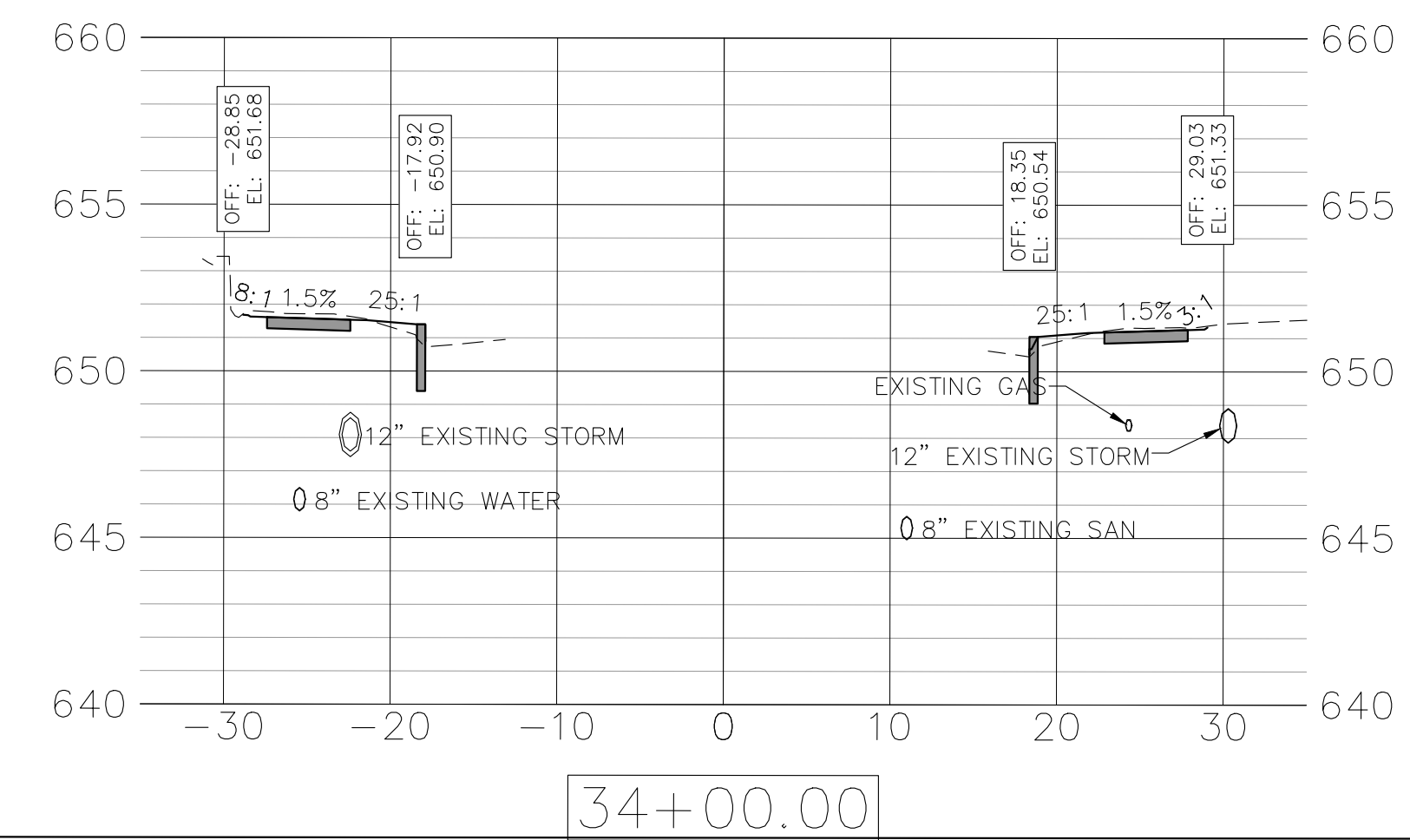
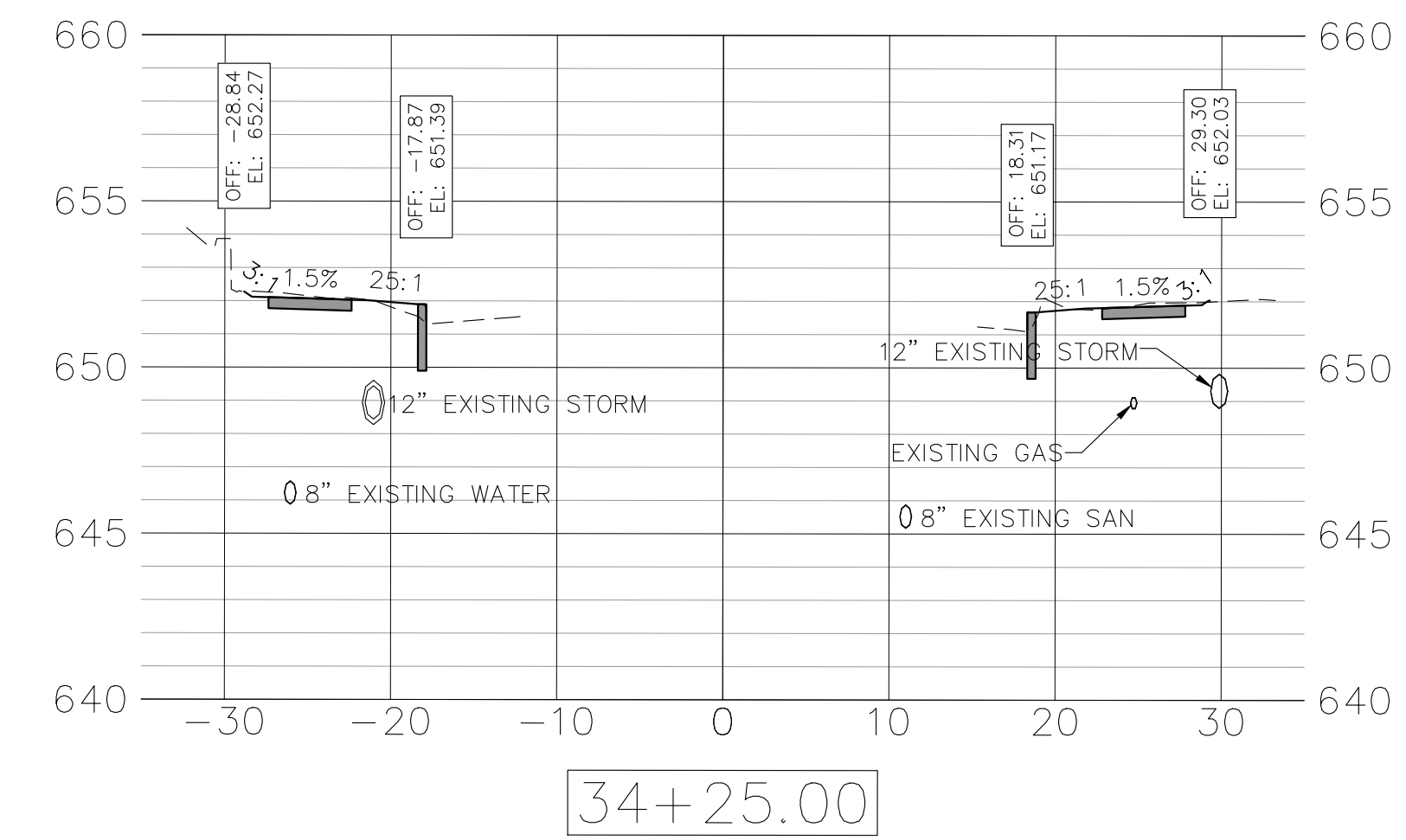
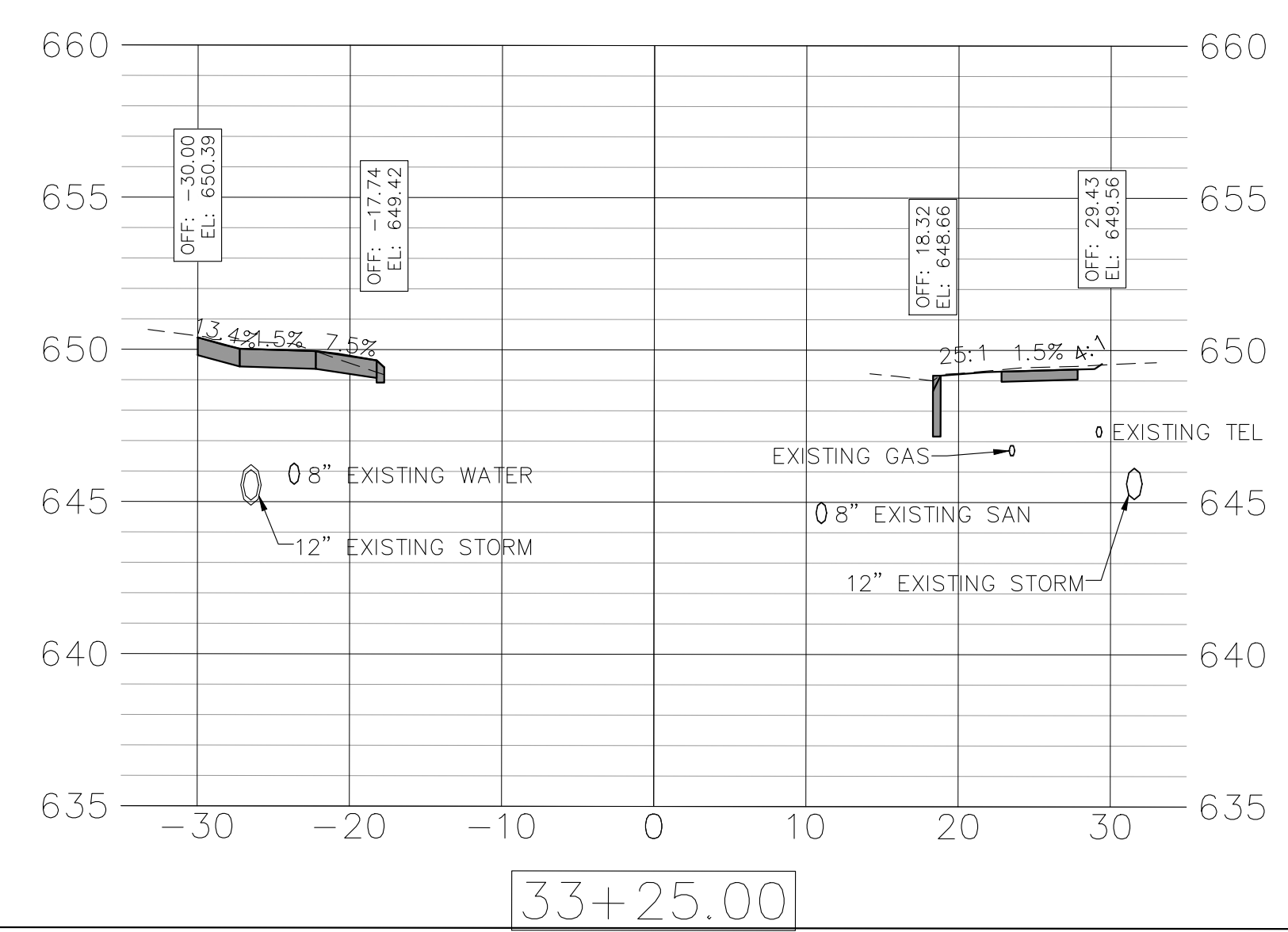
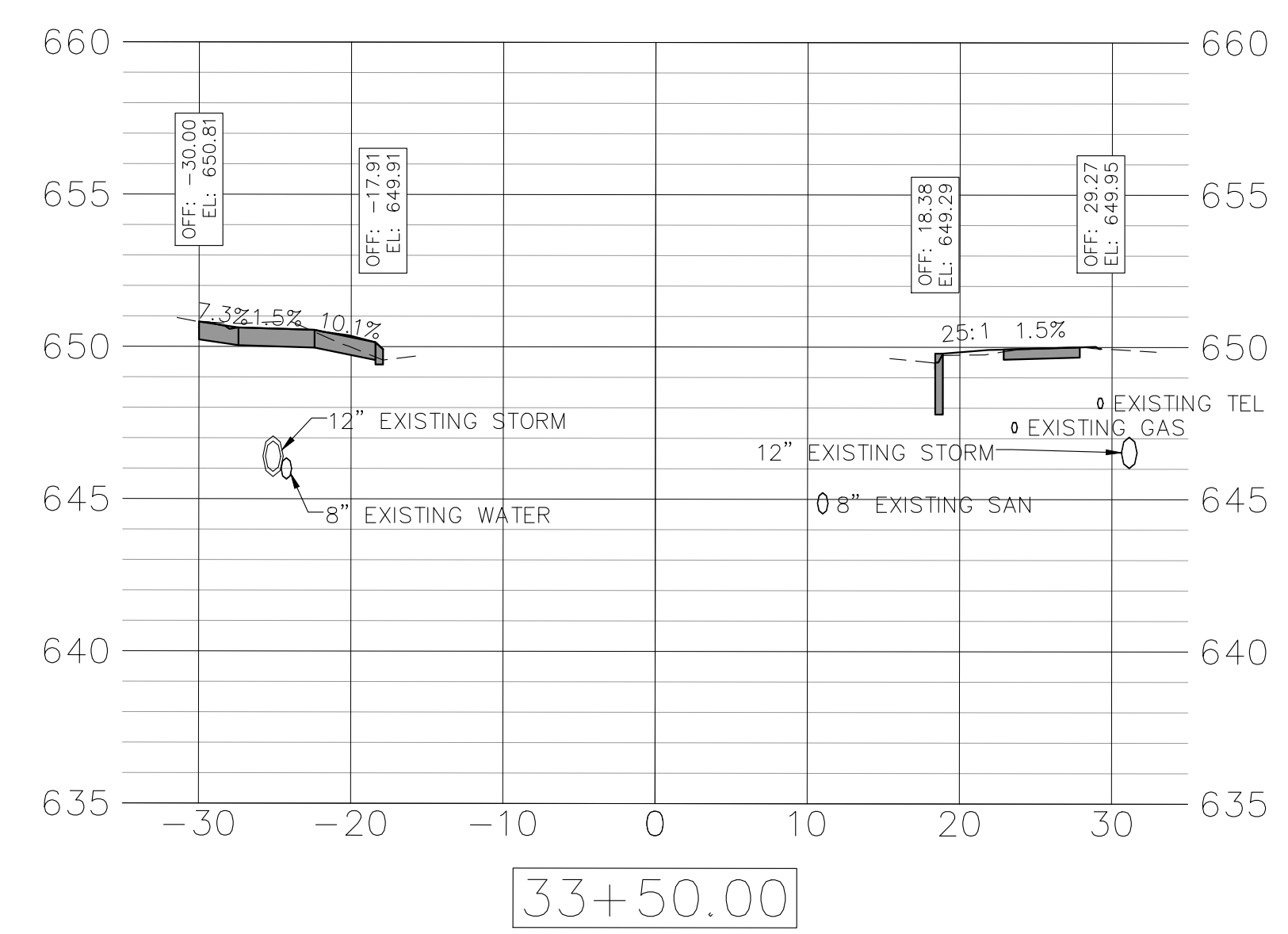
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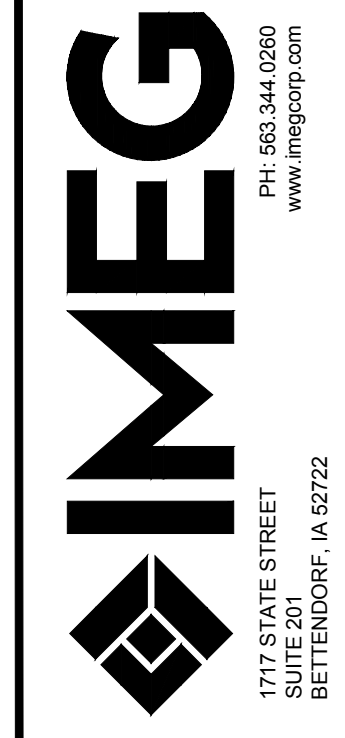
SOUTH CLAY AVENUE RESURFACING PROJECT
 KIRKWOOD, MISSOURI
ARGONNE - ADAMS CROSS SECTIONS

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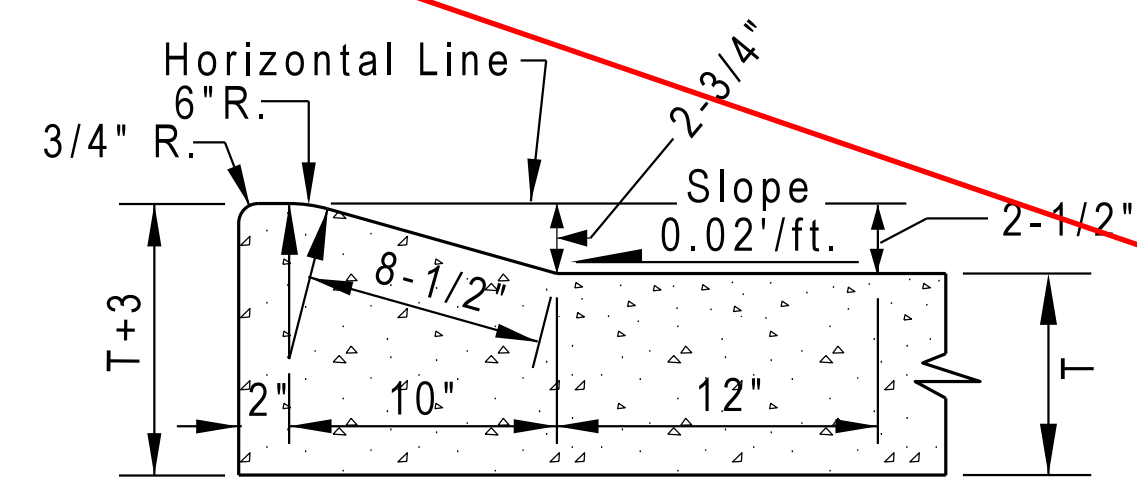


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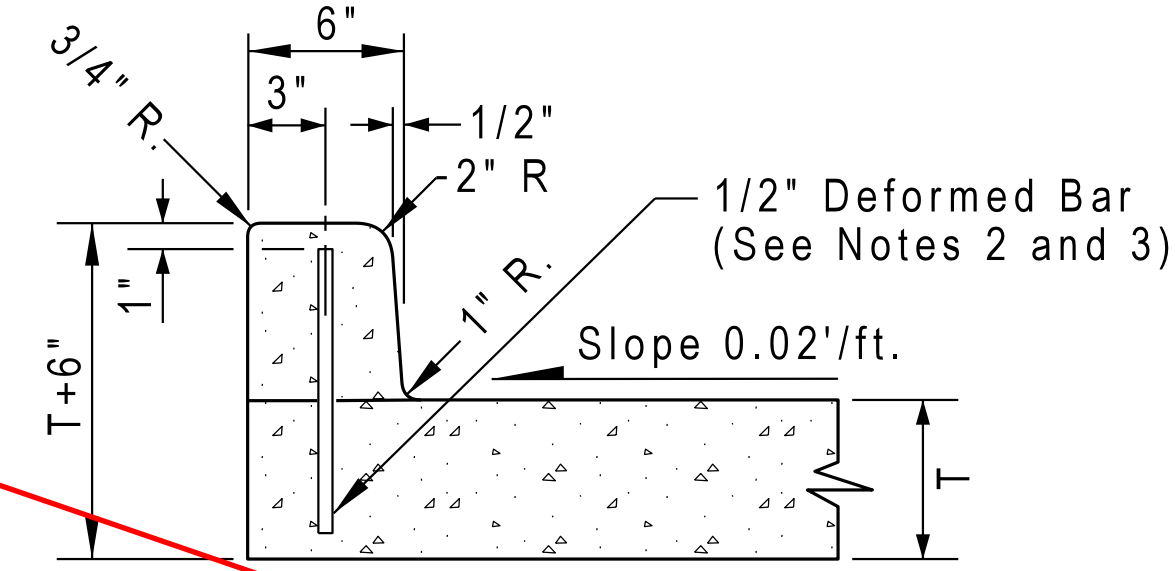


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ARGONNE - ADAMS CROSS SECTIONS

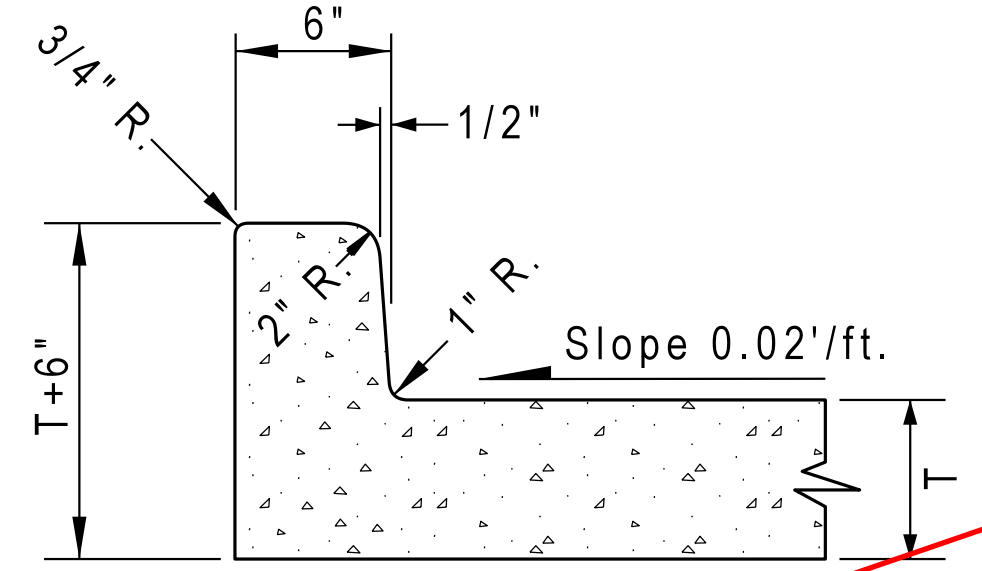
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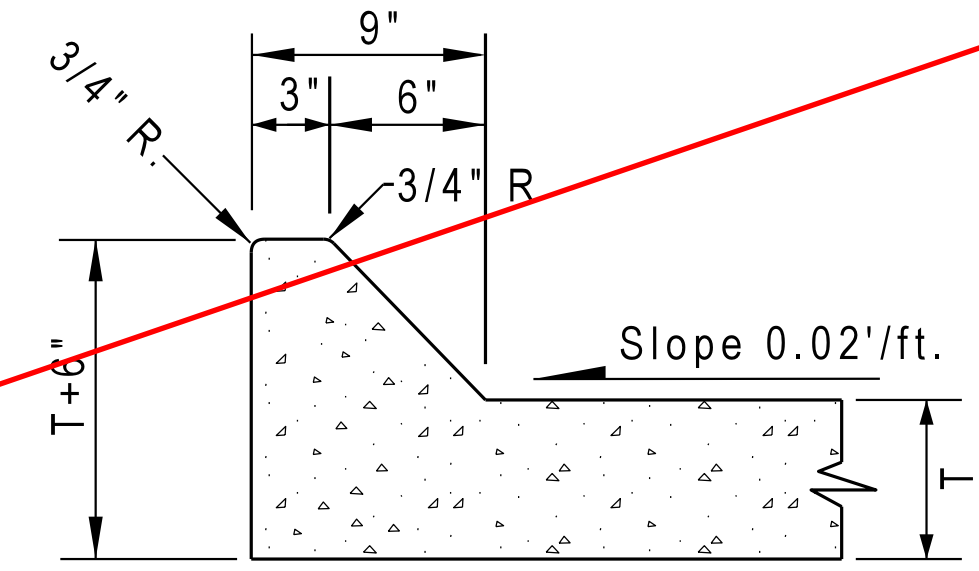
INTEGRAL ROLLED CURB



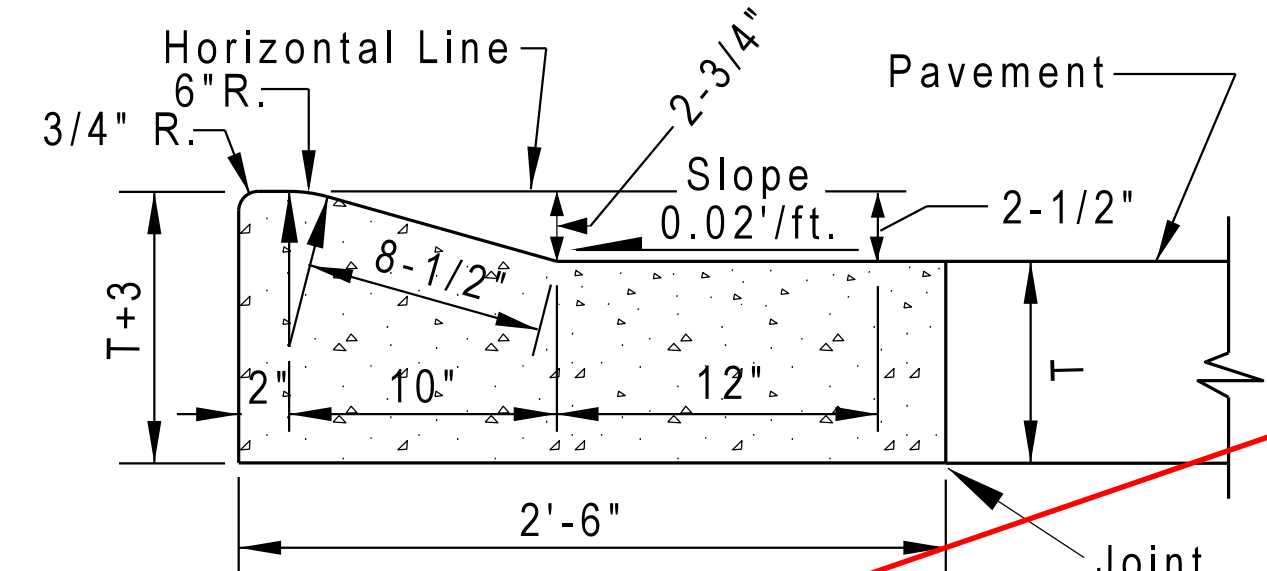
INTEGRAL 6 INCH VERTICAL CURB (DOWELED)



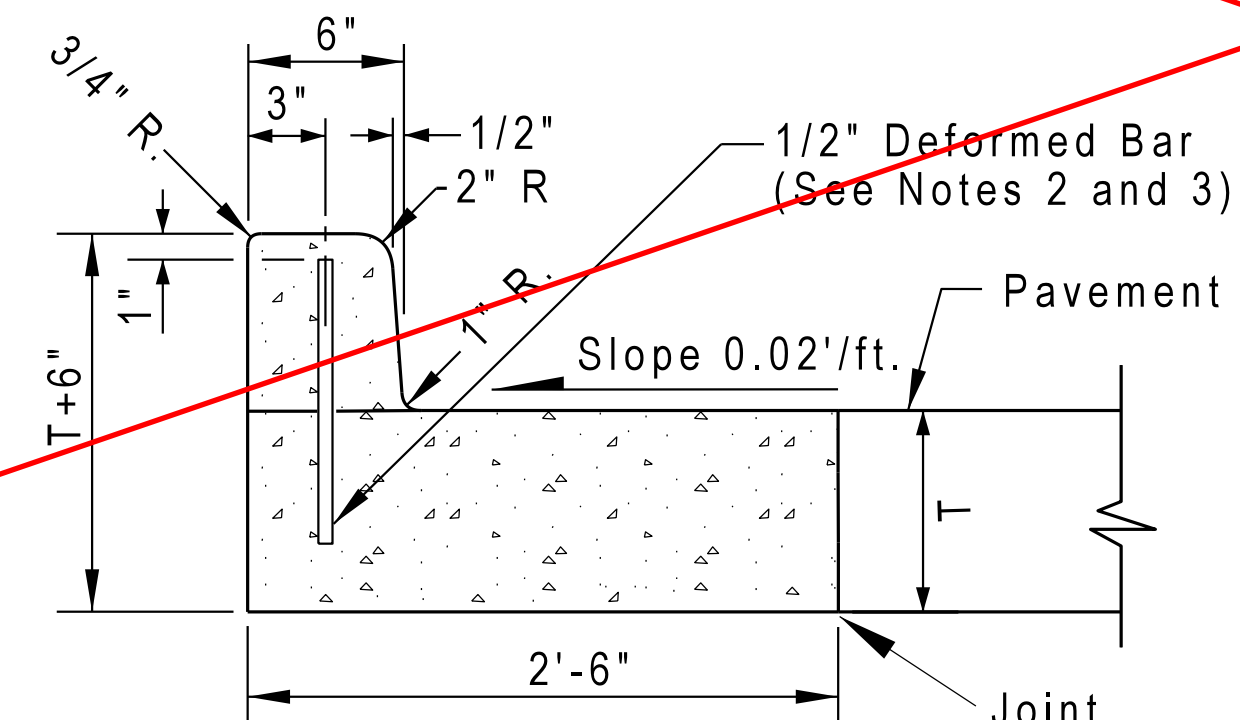
INTEGRAL 6 INCH VERTICAL CURB (MONOLITHIC)



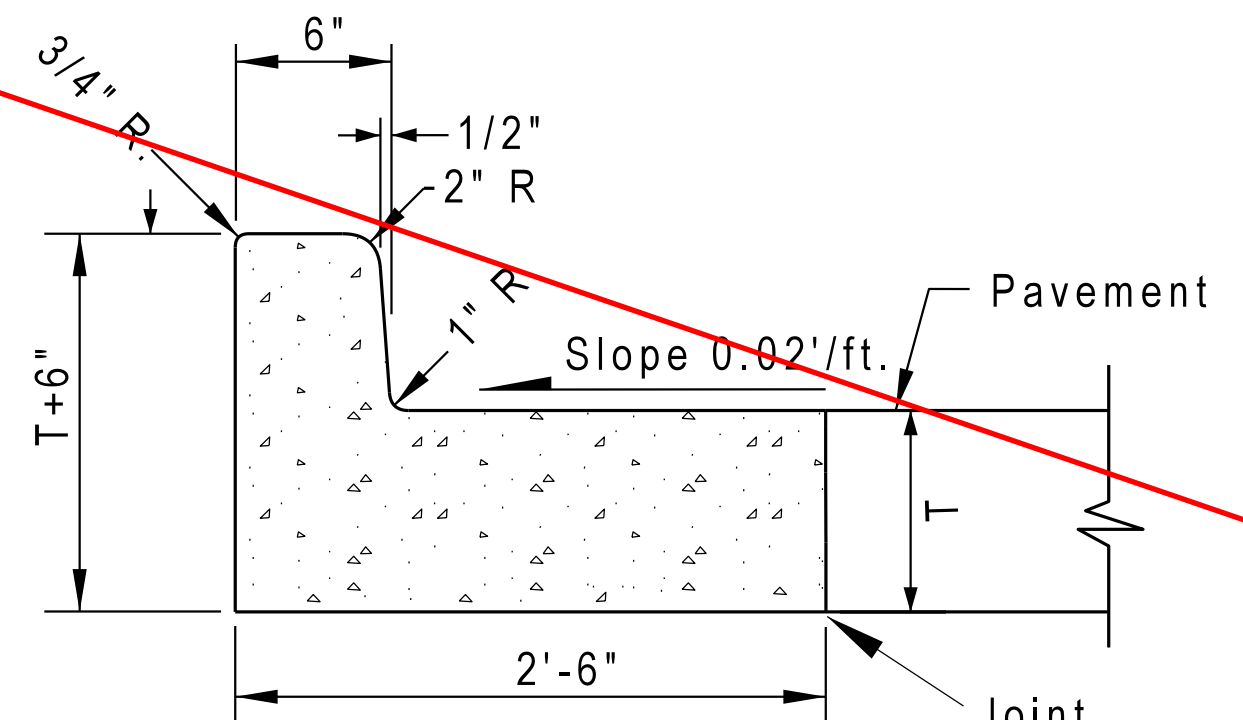
INTEGRAL 6 INCH MOUNTABLE CURB (See Note 10)



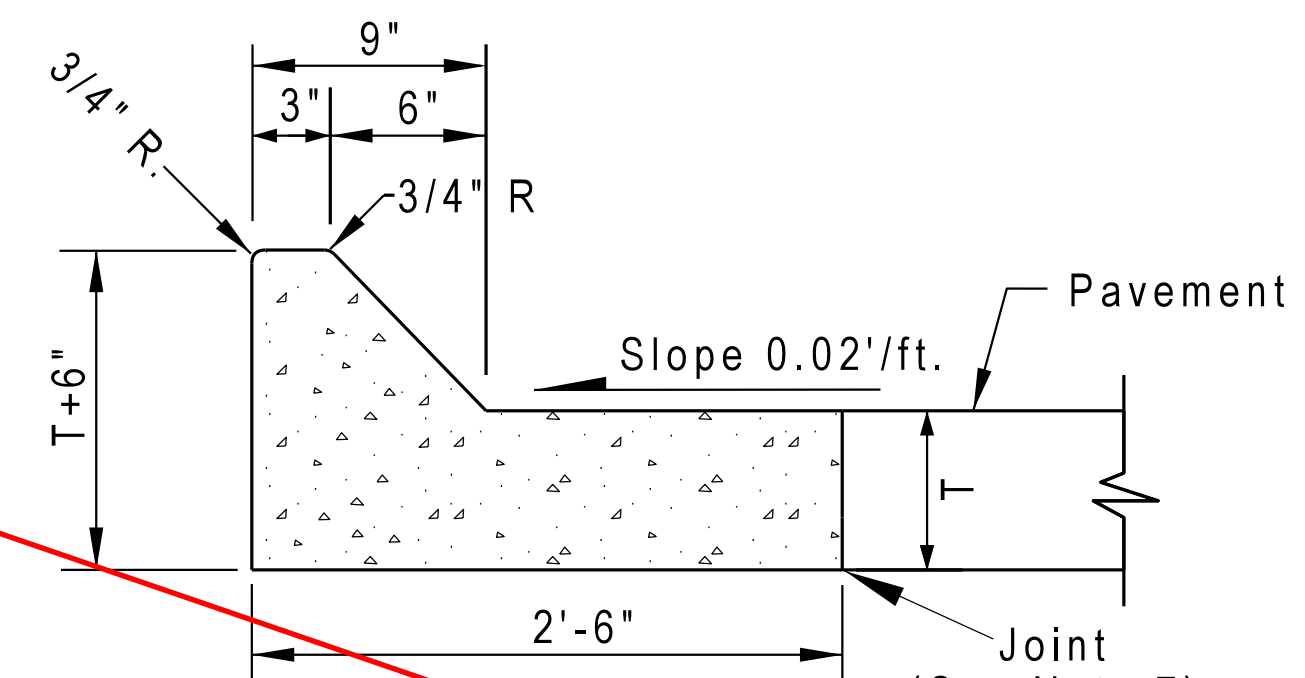
ROLLED CURB AND GUTTER (2'-6" WIDE)



6 INCH VERTICAL CURB AND GUTTER (DOWELED 2'-6" WIDE)



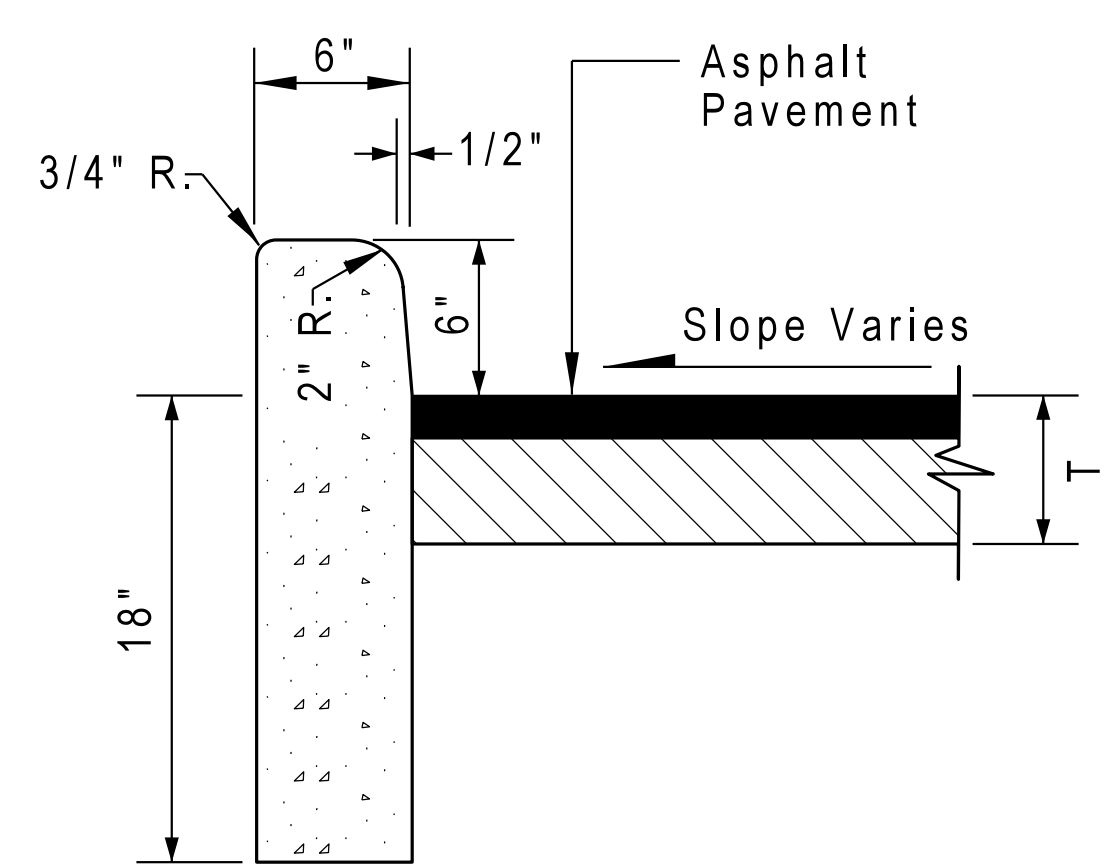
6 INCH VERTICAL CURB AND GUTTER (MONOLITHIC 2'-6" WIDE)



6 INCH MOUNTABLE CURB AND GUTTER (2'-6" WIDE) (See Note 10)

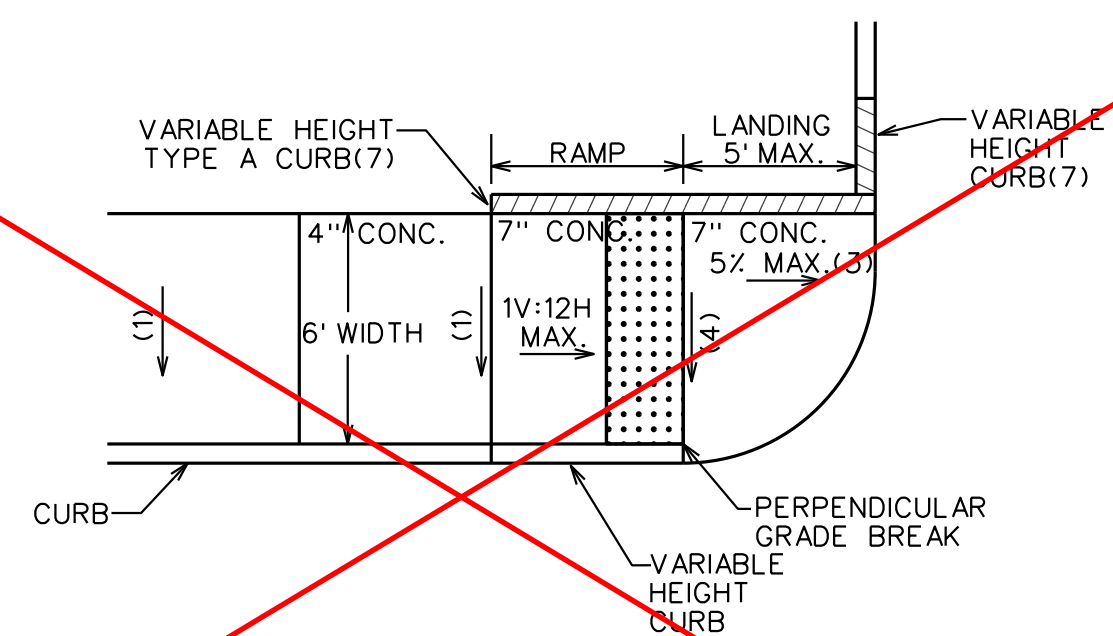
NOTES

- 1) Do not scale drawing, follow dimensions.
- 2) Construction joint and tie bars may be omitted when curb is poured integral with pavement.
- 3) Length of the tie bars shall be equal to the thickness of pavement plus the height of curb less 3 inches. Tie bars shall be placed at 24 inch centers.
- 4) All deformed bars for joints and curbs shall be in accordance with section 1057.4 of the "St. Louis County Standard Specifications" manual.
- 5) Transverse or longitudinal construction joints in slip formed pavements may be made with a groover or tool, if such device as been approved in advance by the Department.
- 6) Certain streets will be required to overlay the gutter with 2 inches of asphalt as directed by the Department. Curb height shall be adjusted to maintain the standard curb height. Asphalt overlay type to be determined by the Department.
- 7) Type "B" joint required for concrete pavement.
- 8) If the Contractor elects to construct concrete pavement by slip-form methods in accordance with Section 502.17, the width of the aggregate base shown on the plans shall be increased to provide a 3-foot width outside the edge of the pavement being placed.
- 9) Type "S" curb shall only be used where repair of existing Type "S" curb is required, and only with Departmental approval.
- 10) 6" mountable curb shall only be used where repair of existing 6" mountable curb is required. Use of 6" mountable curb for other applications requires Departmental approval.

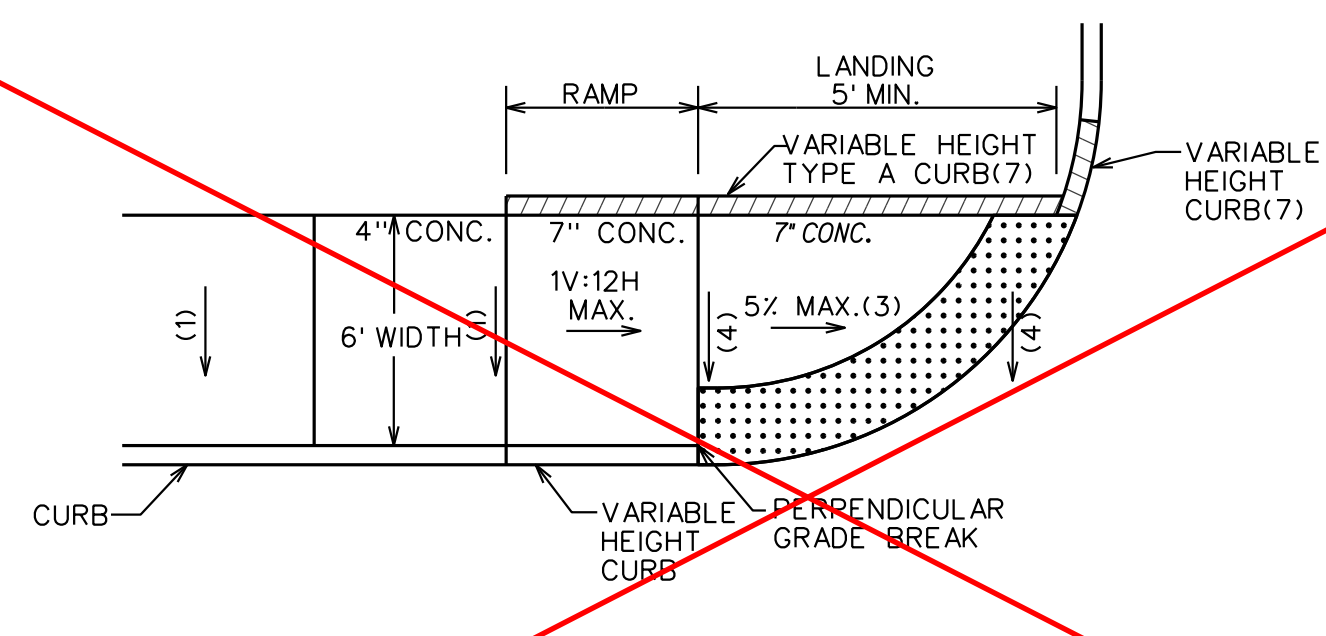


TYPE "S" CONCRETE CURB (See Note 9)

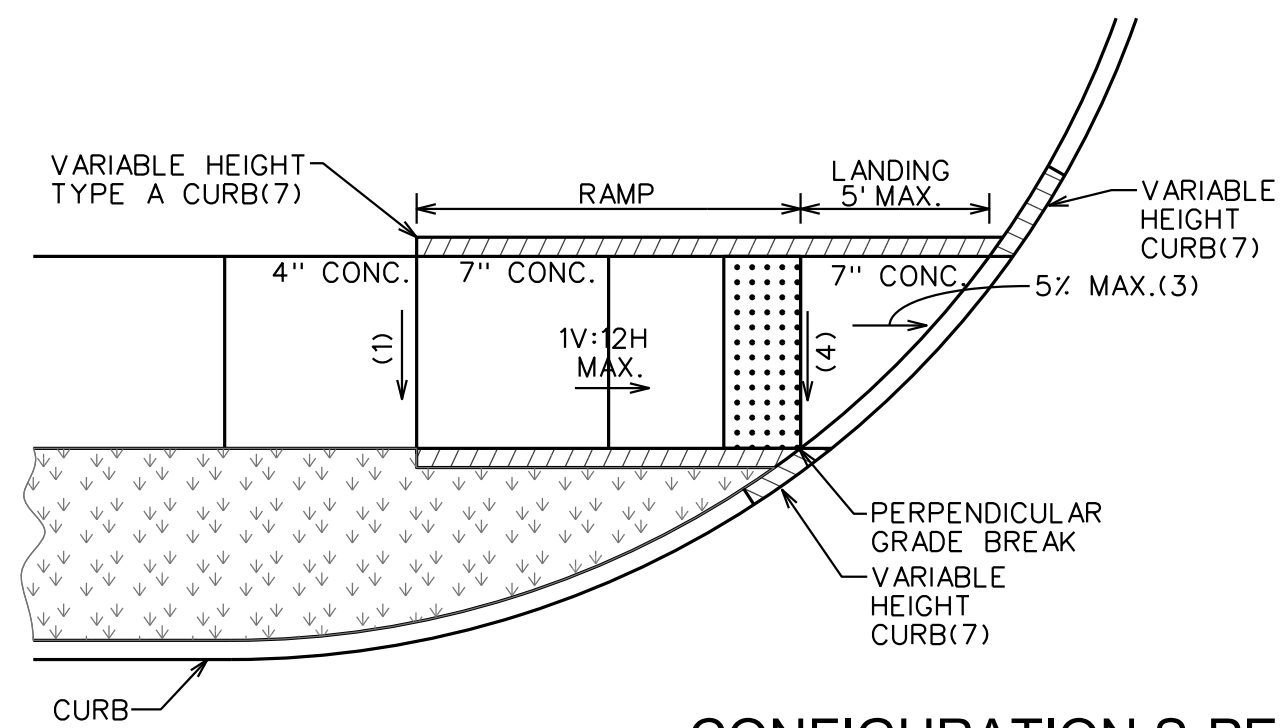
ISSUE DATE 03/09/2020	REVISIONS	THIS IS NOT A CERTIFIED DOCUMENT	<p style="font-size: 8px; margin-top: 5px;">1050 NORTH LINDBERGH BLVD. ST. LOUIS, MISSOURI 63132</p>	STANDARD DRAWING CONCRETE CURB DETAILS
EFFECTIVE 05/01/2020		SHEET 1 OF 1		DRAWING C502.14



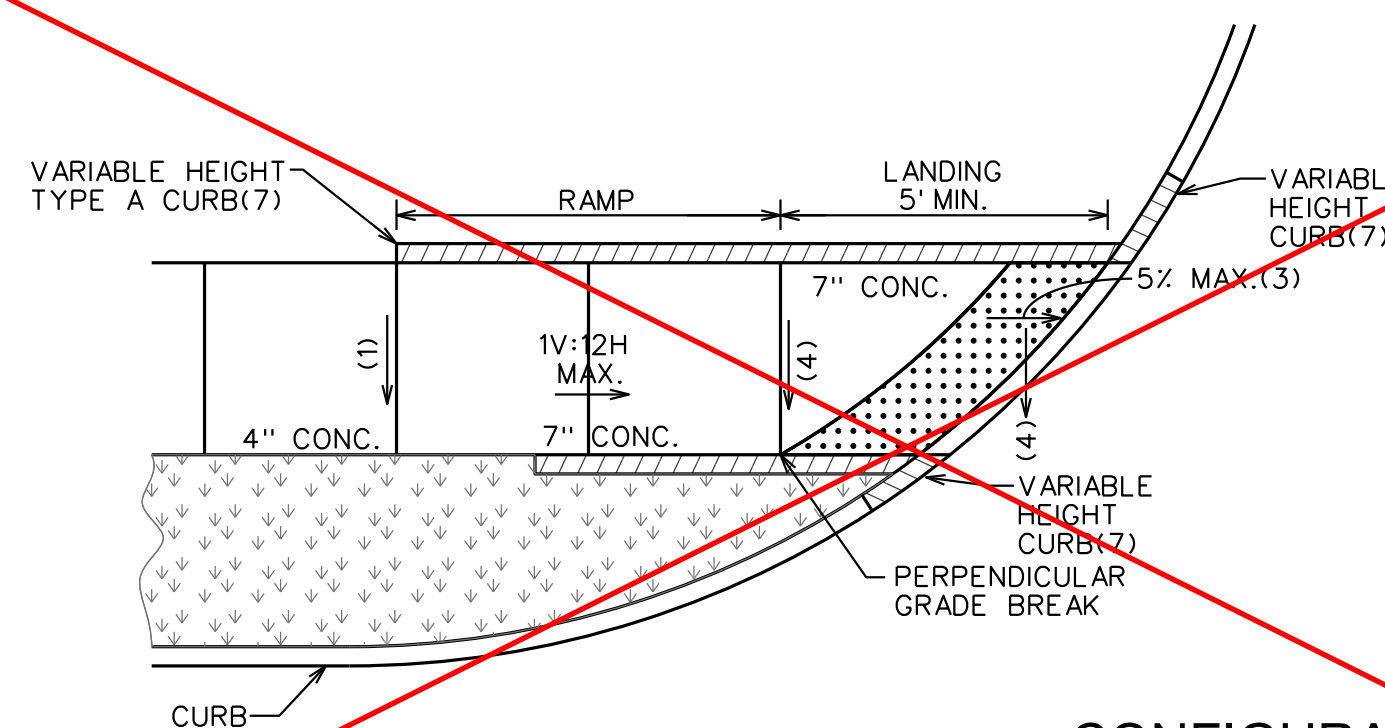
**CONFIGURATION S-PERP-1:
TYPE B CURB RAMP
SINGLE PERPENDICULAR,
SMALL RADIUS, BACK OF CURB**



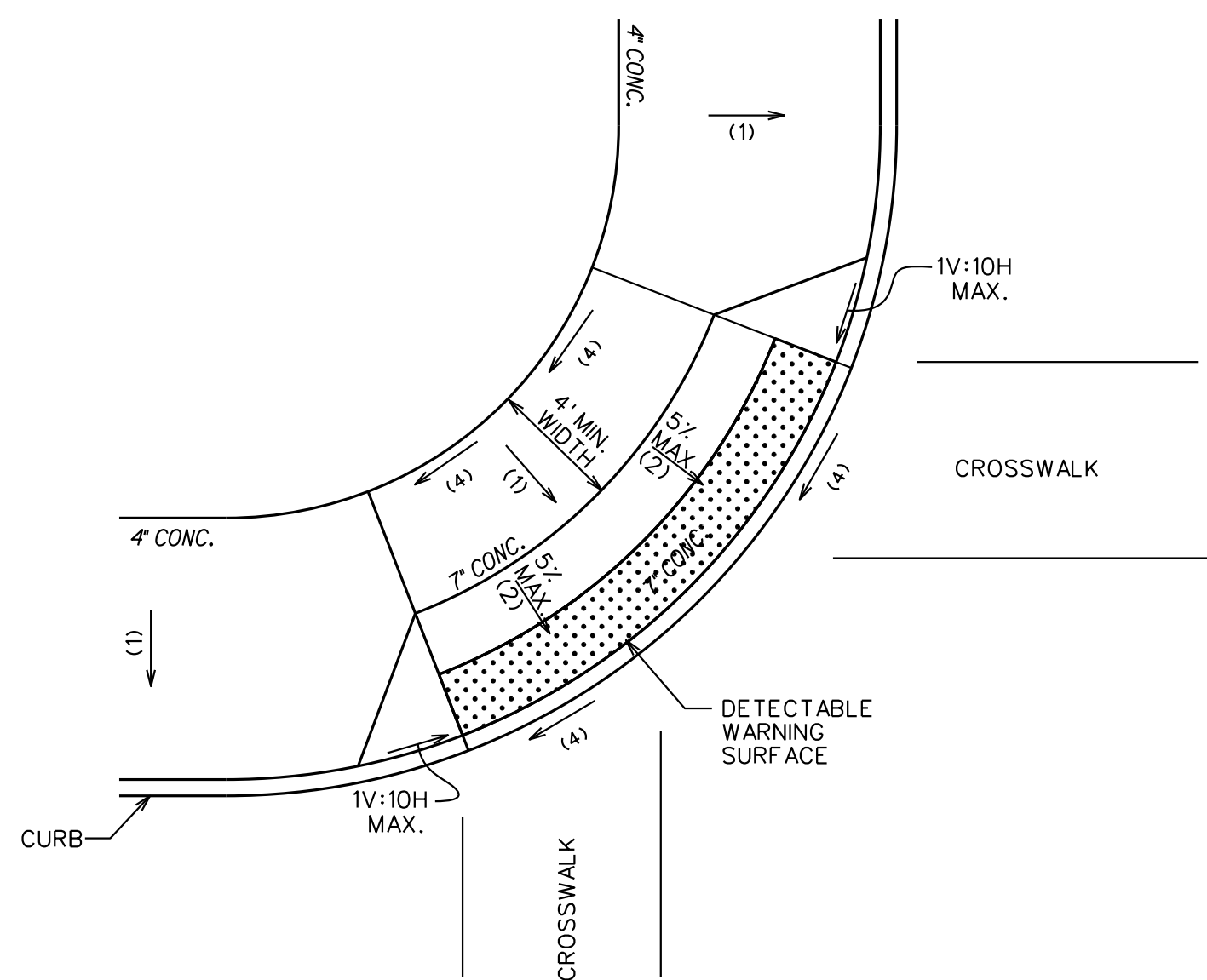
**CONFIGURATION S-PERP-2:
TYPE B CURB RAMP
SINGLE PERPENDICULAR,
LARGE RADIUS, BACK OF CURB**



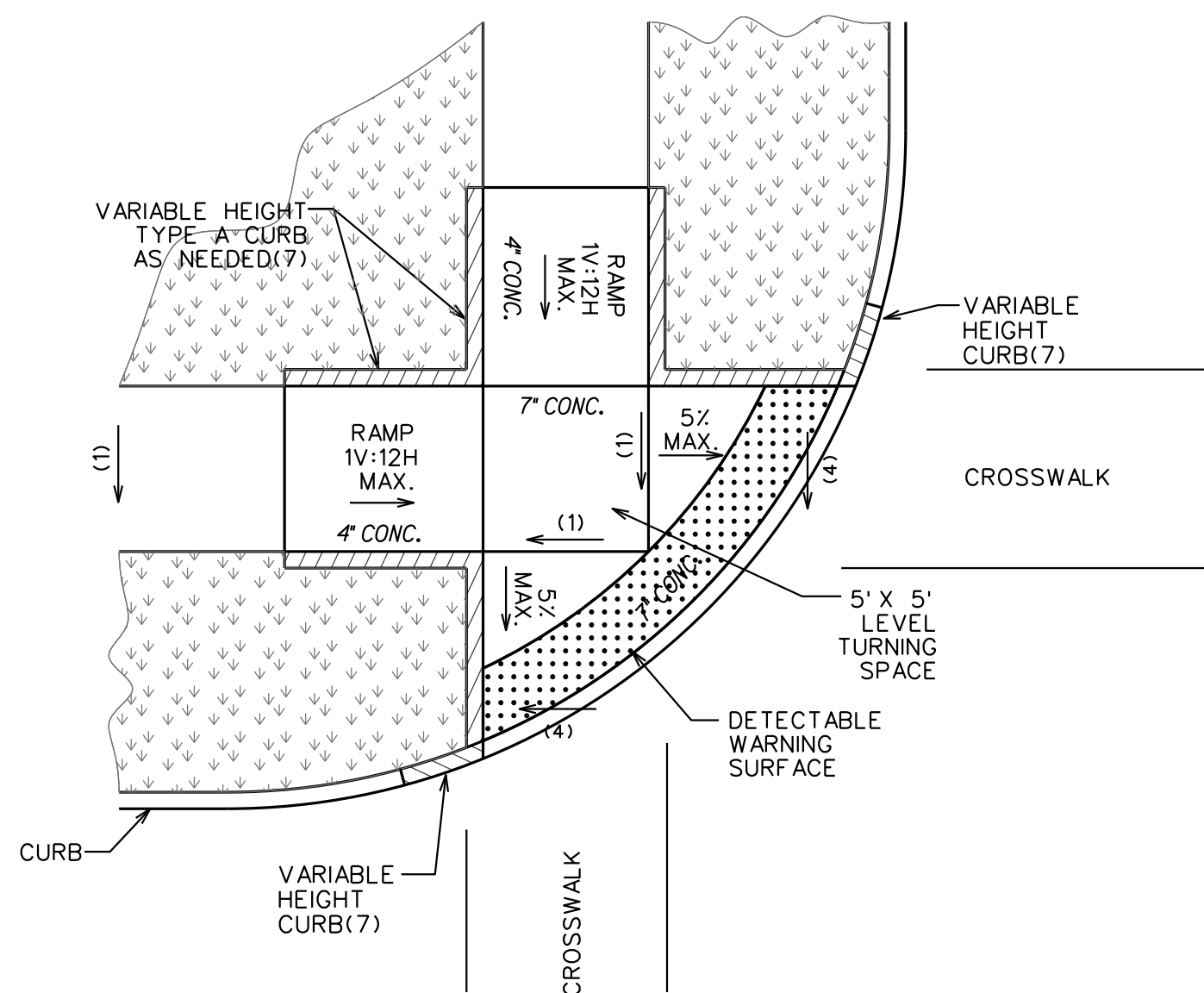
**CONFIGURATION S-PERP-3:
TYPE B CURB RAMP
SINGLE PERPENDICULAR,
SMALL RADIUS, GRASS BUFFER**



**CONFIGURATION S-PERP-4:
TYPE B CURB RAMP
SINGLE PERPENDICULAR,
LARGE RADIUS, GRASS BUFFER**



**CONFIGURATION BT-1:
TYPE E CURB RAMP
BLENDED TRANSITION,
BACK OF CURB**



**CONFIGURATION BT-COMB-1:
TYPE E CURB RAMP
BLENDED TRANSITION
GRASS BUFFER**

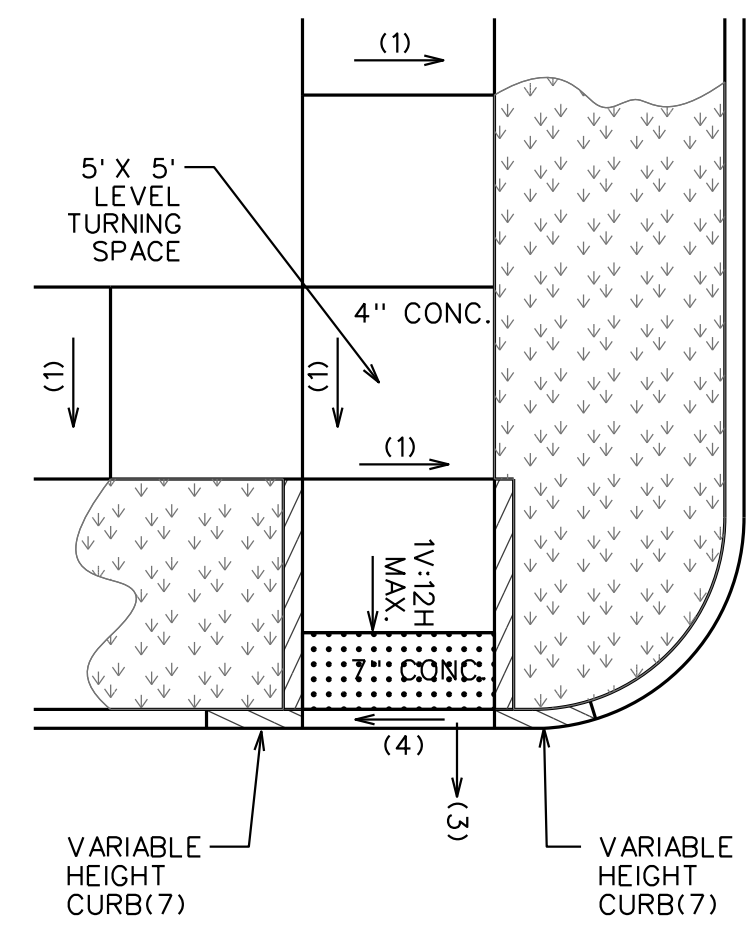
NOTES:

- (1) CROSS SLOPE SHALL BE SUFFICIENT TO PREVENT PONDING UP TO A MAXIMUM OF 2% UNLESS OTHERWISE NOTED. 1.5% (+/-) 0.5% CROSS SLOPE RECOMMENDED.
- (2) THE RUNNING SLOPE OF A BLENDED TRANSITION MAY BE WARPED TO MEET ROAD GRADE UP TO 5% MAXIMUM.
- (3) ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT OR GUTTER SLOPE IN THE DIRECTION OF PEDESTRIAN TRAVEL AND CURB RAMP SLOPE SHALL NOT EXCEED 13.0%. PROVIDE A 24" LEVEL LANDING IF ALGEBRAIC DIFFERENCE EXCEEDS 13.0%.
- (4) CROSS SLOPE SHALL BE A MIN OF 1.0% AND A MAX OF 2.0% AT INTERSECTIONS WITH STOP OR YIELD CONTROL. CROSS SLOPE SHALL BE A MAX OF 5.0% AT INTERSECTIONS WITHOUT STOP OR YIELD CONTROL.
- (5) CURB RAMP RUNNING SLOPE SHALL BE 8.3% MAX.
- (6) TRAVERSABLE FLARE RUNNING SLOPE SHALL BE 10% MAX.
- (7) IN ORDER TO PREVENT TRIPPING HAZARDS, RETURN CURBS AND NON-TRAVERSIBLE FLARES MAY BE USED ONLY WHEN NECESSARY. IF GRADING CAN BE PERFORMED TO SURROUNDING VEGETATIVE AREAS THAT IS SIMILAR TO ADJOINING SLOPES, VERTICAL CURBS ADJACENT TO THE PEDESTRIAN ACCESS ROUTE SHALL BE AVOIDED.

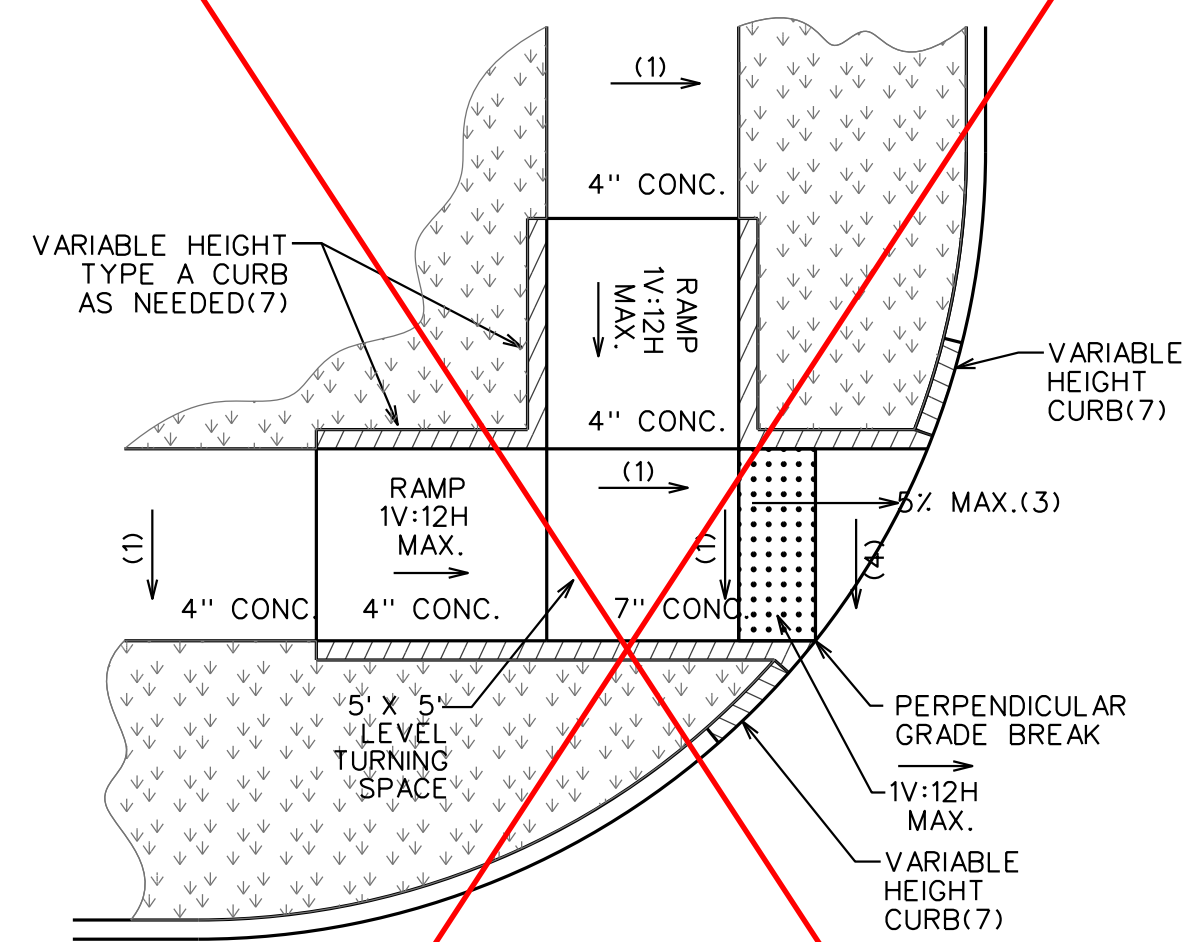
GENERAL NOTES:

- THE CONTRACTOR SHALL COMPLY WITH ALL LAWS PERTAINING TO THE AMERICANS WITH DISABILITIES ACT ON PUBLIC RIGHTS OF WAY. EXCEPTIONS MUST BE APPROVED BY THE ENGINEER. ALL AREAS OF NON-COMPLIANCE SHALL BE REMOVED AND CORRECTED AT THE CONTRACTOR'S EXPENSE.
- SIDEWALK AND RAMP WIDTH SHALL BE 5' MIN FOR ARTERIAL (ARS), COLLECTOR (CRS-2), AND NONRESIDENTIAL (CRS) ROADS; 4' MIN WIDTH FOR RESIDENTIAL (CRS) STREETS; 6' MIN WIDTH IN ALL CASES WHEN SIDEWALK IS CONSTRUCTED AT THE BACK OF CURB.
- FINISHED SIDEWALK SHALL BE STABLE, SLIP RESISTANT, AND SURFACE SHALL NOT POND WATER.
- THE RUNNING GRADE OF A SIDEWALK SHALL NOT EXCEED 5.0% EXCEPT WHEN MATCHING THE GRADE OF AN ADJACENT ROADWAY.
- A DIAGONAL RAMP IS A SINGLE CURB RAMP WHICH SERVES TWO CROSSINGS. DIAGONAL RAMP SHALL NOT BE PERMITTED FOR NEW CONSTRUCTION OR REDEVELOPMENT PROJECTS. DIAGONAL RAMP MAY ONLY BE USED FOR ALTERATION TYPE PROJECTS WHERE CONSTRUCTION OF SPLIT RAMP IS NOT PRACTICABLE AS DETERMINED BY THE ENGINEER.
- LANDINGS, TURNING SPACES, AND CLEAR SPACES LOCATED IN THE ROADWAY MUST BE COMPLETELY CONTAINED WITHIN THE CROSSWALK.
- TRANSITIONS FROM SIDEWALK TO GUTTER TO ROADWAY MUST BE FLUSH.
- DETECTABLE WARNING DEVICES ARE REQUIRED AT PUBLIC STREETS AND SIGNALIZED ENTRANCES. FOR DETECTABLE WARNING SURFACE REQUIREMENTS SEE STANDARD DRAWING C608.51
- RUNNING SLOPE OF CURVED SIDEWALKS AND RAMPS SHALL BE MEASURED ON THE INSIDE EDGE OF THE CURVE.
- 15 FOOT RULE: THE RUNNING SLOPE FOR A CURB RAMP IS NOT LIMITED TO 8.33% MAXIMUM IF THE CONSTRUCTED CURB RAMP EXCEEDS 15 FEET IN LENGTH. THIS EXCEPTION DOES NOT APPLY TO SIDEWALK RAMPS.
- GRADE BREAK BETWEEN RAMP AND ADJACENT SIDEWALK MUST BE PERPENDICULAR TO THE DIRECTION OF TRAVEL.
- TYPE A CURB IS INCIDENTAL TO CURB RAMP CONSTRUCTION. NO ADDITIONAL PAY WILL BE PROVIDED.
- A CLEAR SPACE SHALL BE PROVIDED BEYOND THE BOTTOM GRADE BREAK OF ALL CURB RAMPS. THE CLEAR SPACE SHALL BE A MINIMUM OF 4 FT BY 4 FT AND RESIDE WHOLLY OUTSIDE OF THE PARALLEL VEHICLE TRAVEL LANE.
- A 4' BY 4' MINIMUM TURNING SPACE SHALL BE PROVIDED AT THE BOTTOM OF ANY SINGLE RAMP WHICH SERVES MORE THAN ONE DIRECTION, REGARDLESS OF CONFIGURATION. TURNING SPACE SHALL RESIDE WHOLLY OUTSIDE OF BOTH PARALLEL VEHICLE TRAVEL LANES.
- A SEGMENTAL TRANSITION SHALL BE PROVIDED WHEN CONNECTING NEW PEDESTRIAN FACILITIES TO AN EXISTING ROADWAY.
- ALL SLOPES AND GRADES SHALL BE MEASURED WITH A CALIBRATED 2 FOOT LONG DIGITAL LEVEL.
- PERMEABLE PAVEMENT SHALL NOT BE PERMITTED FOR THE CONSTRUCTION OF CURB RAMPS, BLENDED TRANSITIONS, OR TURNING SPACES. ALL PROPOSED PERMEABLE PAVEMENT INSTALLATIONS SHALL REQUIRE DEPARTMENTAL APPROVAL.

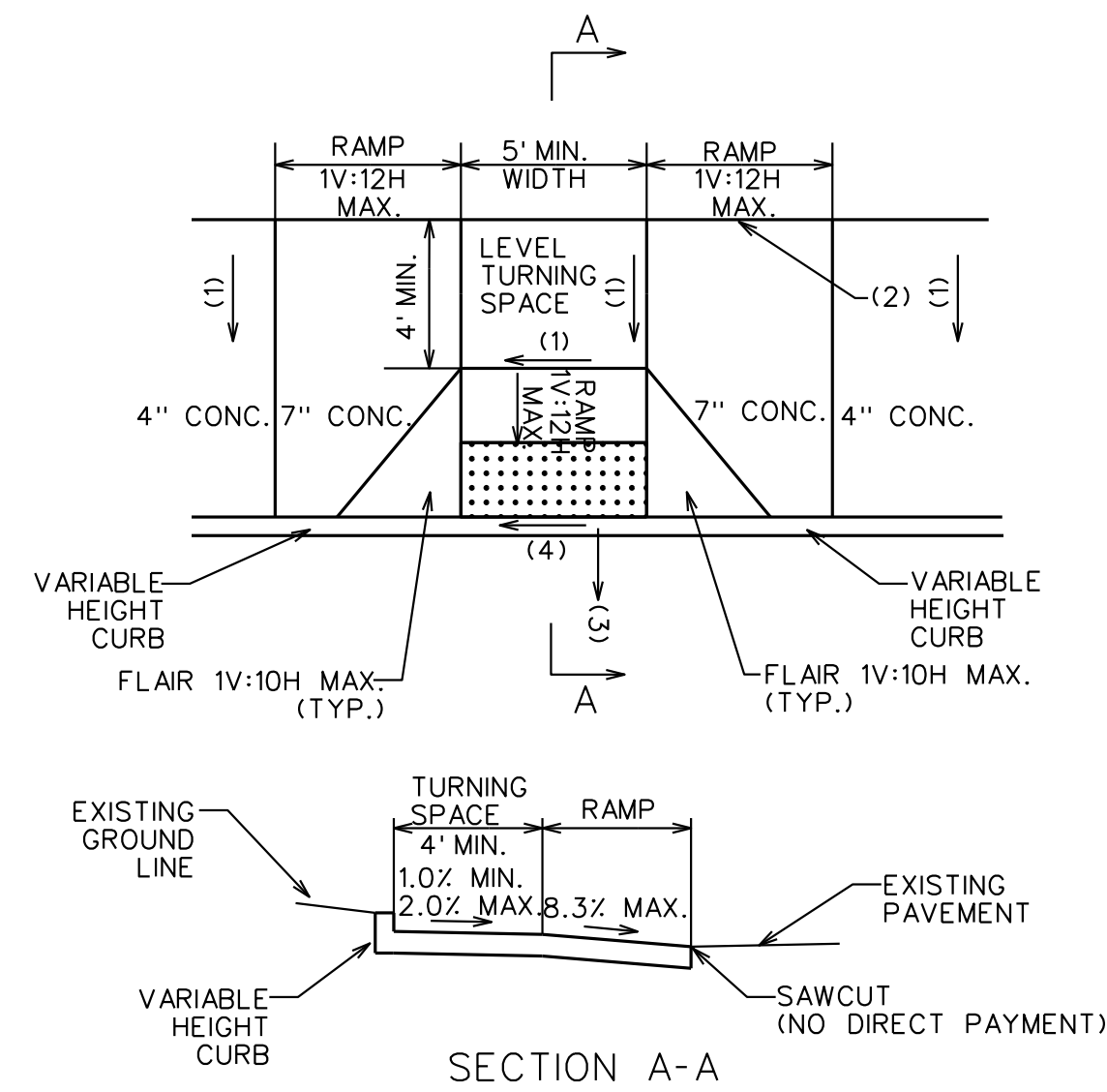
ISSUE DATE	THIS IS NOT A CERTIFIED DOCUMENT		CURB RAMP CONFIGURATIONS FOR USE WITHIN PUBLIC RIGHT-OF-WAY	
03/09/2020				
REVISIONS				
1050 NORTH LINDBERGH BLVD. ST. LOUIS, MISSOURI 63132	EFFECTIVE 05/01/2020	SHEET 1 OF 7	DRAWING C608.41	



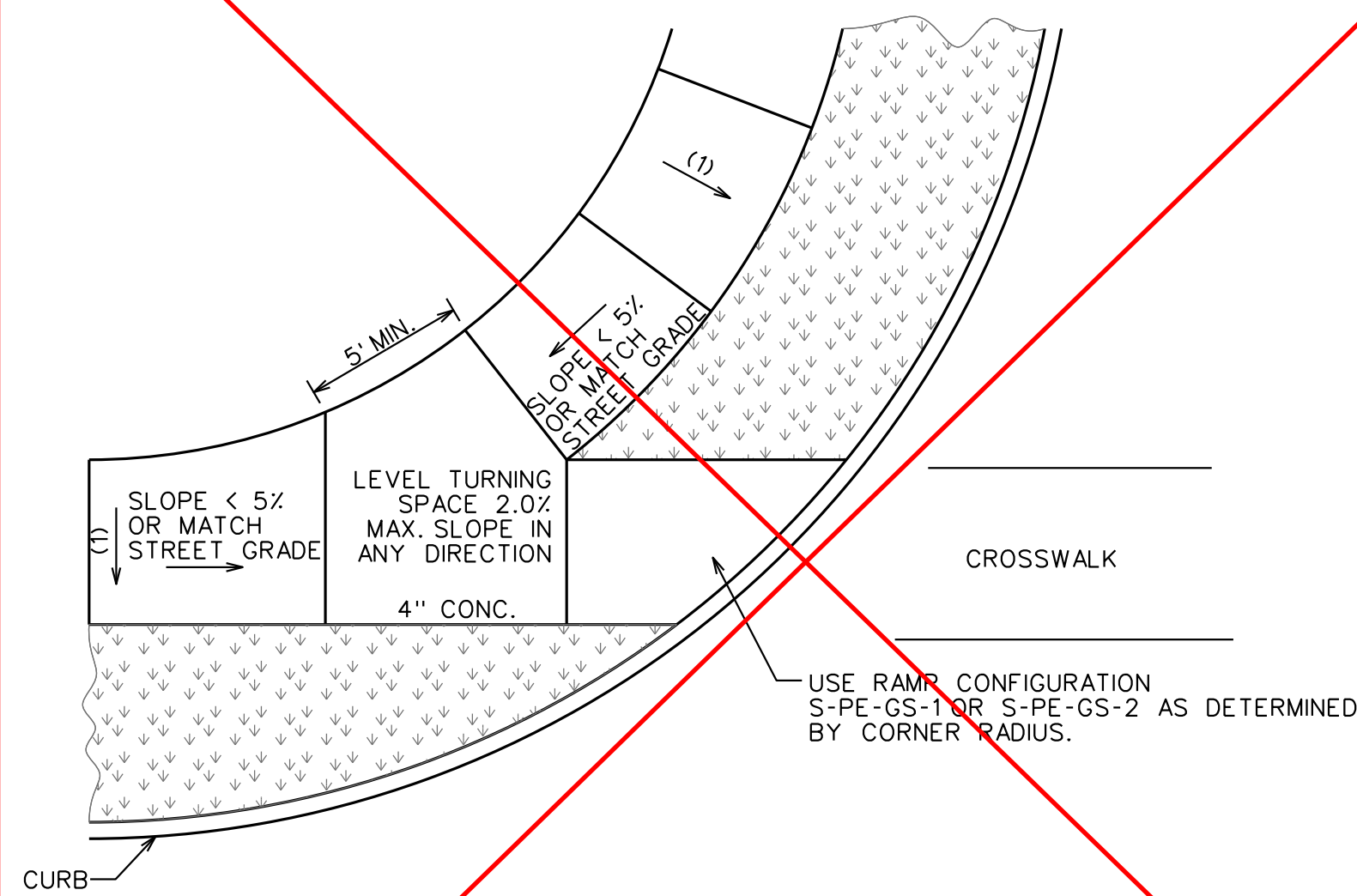
**CONFIGURATION S-PERP-5:
TYPE B CURB RAMP
SINGLE PERPENDICULAR,
GRASS BUFFER**



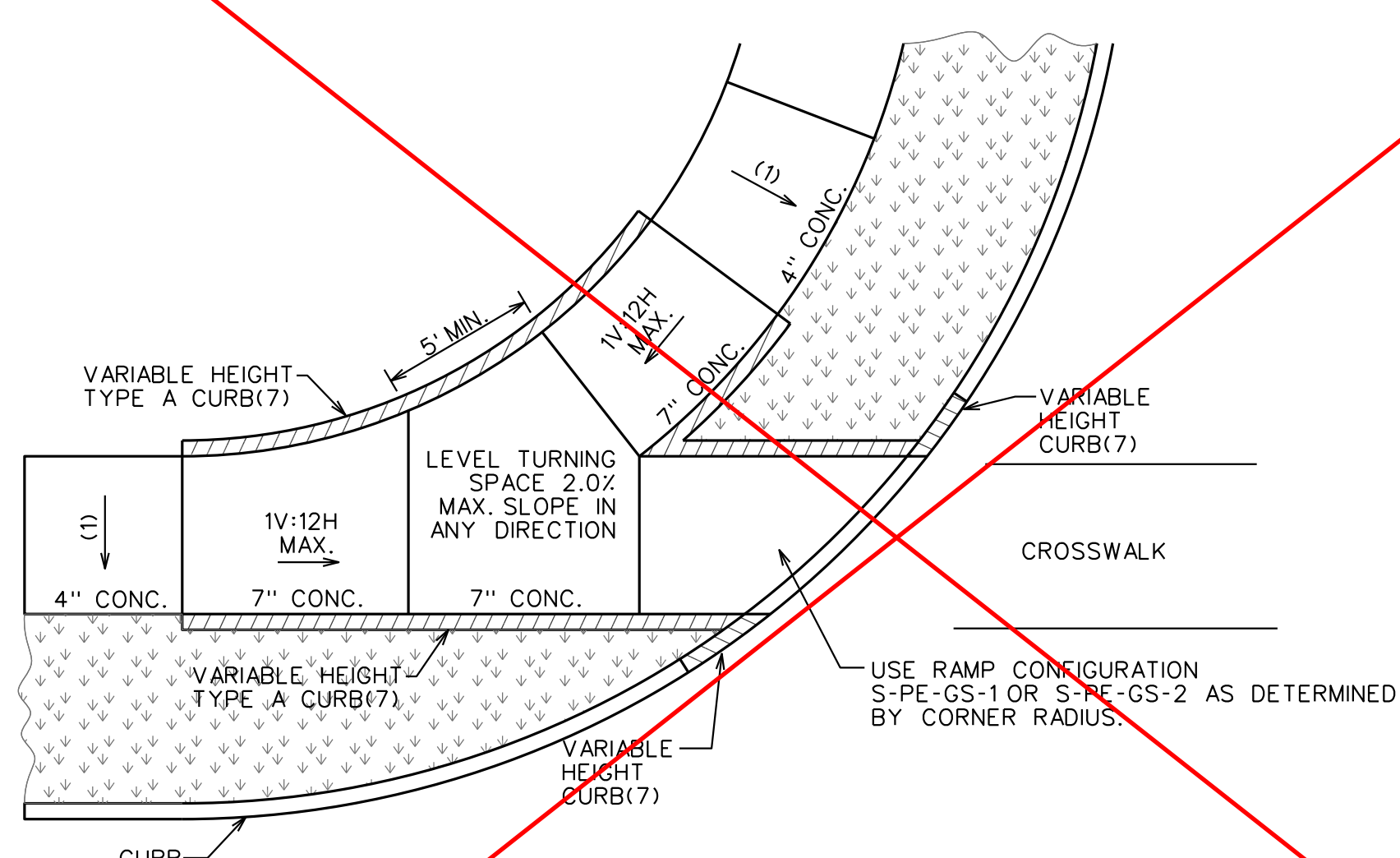
**CONFIGURATION S-COMB-1:
TYPE B,C CURB RAMP
SINGLE COMPOUND,
GRASS BUFFER**



**CONFIGURATION S-COMB-2:
TYPE A,C CURB RAMP
SINGLE COMPOUND,
BACK OF CURB**



**CONFIGURATION S-PERP-6:
TYPE B CURB RAMP
SINGLE PERPENDICULAR,
NON-PERPENDICULAR DIVERGENT
PATH, GRASS BUFFER**



**CONFIGURATION S-COMB-3:
TYPE B CURB RAMP
SINGLE COMPOUND,
NON-PERPENDICULAR DIVERGENT
PATH, GRASS BUFFER**

NOTES:

- (1) CROSS SLOPE SHALL BE SUFFICIENT TO PREVENT PONDING UP TO A MAXIMUM OF 2% UNLESS OTHERWISE NOTED. 1.5% (+/-) 0.5% CROSS SLOPE RECOMMENDED.
- (2) THE RUNNING SLOPE OF A BLENDED TRANSITION MAY BE WARPED TO MEET ROAD GRADE UP TO 5% MAXIMUM.
- (3) ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT OR GUTTER SLOPE IN THE DIRECTION OF PEDESTRIAN TRAVEL AND CURB RAMP SLOPE SHALL NOT EXCEED 13.0%. PROVIDE A 24" LEVEL LANDING IF ALGEBRAIC DIFFERENCE EXCEEDS 13.0%.
- (4) CROSS SLOPE SHALL BE A MIN OF 1.0% AND A MAX OF 2.0% AT INTERSECTIONS WITH STOP OR YIELD CONTROL. CROSS SLOPE SHALL BE A MAX OF 5.0% AT INTERSECTIONS WITHOUT STOP OR YIELD CONTROL.
- (5) CURB RAMP RUNNING SLOPE SHALL BE 8.3% MAX.
- (6) TRAVERSABLE FLARE RUNNING SLOPE SHALL BE 10% MAX.
- (7) IN ORDER TO PREVENT TRIPPING HAZARDS, RETURN CURBS AND NON-TRAVERSIBLE FLARES MAY BE USED ONLY WHEN NECESSARY. IF GRADING CAN BE PERFORMED TO SURROUNDING VEGETATIVE AREAS THAT IS SIMILAR TO ADJOINING SLOPES, VERTICAL CURBS ADJACENT TO THE PEDESTRIAN ACCESS ROUTE SHALL BE AVOIDED.

GENERAL NOTES:

THE CONTRACTOR SHALL COMPLY WITH ALL LAWS PERTAINING TO THE AMERICANS WITH DISABILITIES ACT ON PUBLIC RIGHTS OF WAY. EXCEPTIONS MUST BE APPROVED BY THE ENGINEER. ALL AREAS OF NON-COMPLIANCE SHALL BE REMOVED AND CORRECTED AT THE CONTRACTOR'S EXPENSE.

SIDEWALK AND RAMP WIDTH SHALL BE 5' MIN FOR ARTERIAL (ARS), COLLECTOR (CRS-2), AND NONRESIDENTIAL (CRS) ROADS; 4' MIN WIDTH FOR RESIDENTIAL (CRS) STREETS; 6' MIN WIDTH IN ALL CASES WHEN SIDEWALK IS CONSTRUCTED AT THE BACK OF CURB.

FINISHED SIDEWALK SHALL BE STABLE, SLIP RESISTANT, AND SURFACE SHALL NOT POND WATER.

THE RUNNING GRADE OF A SIDEWALK SHALL NOT EXCEED 5.0% EXCEPT WHEN MATCHING THE GRADE OF AN ADJACENT ROADWAY.

A DIAGONAL RAMP IS A SINGLE CURB RAMP WHICH SERVES TWO CROSSINGS. DIAGONAL RAMP SHALL NOT BE PERMITTED FOR NEW CONSTRUCTION OR REDEVELOPMENT PROJECTS. DIAGONAL RAMP MAY ONLY BE USED FOR ALTERATION TYPE PROJECTS WHERE CONSTRUCTION OF SPLIT RAMP IS NOT PRACTICABLE AS DETERMINED BY THE ENGINEER.

LANDINGS, TURNING SPACES, AND CLEAR SPACES LOCATED IN THE ROADWAY MUST BE COMPLETELY CONTAINED WITHIN THE CROSSWALK.

TRANSITIONS FROM SIDEWALK TO GUTTER TO ROADWAY MUST BE FLUSH.

DETECTABLE WARNING DEVICES ARE REQUIRED AT PUBLIC STREETS AND SIGNALIZED ENTRANCES. FOR DETECTABLE WARNING SURFACE REQUIREMENTS SEE STANDARD DRAWING C608.51

RUNNING SLOPE OF CURVED SIDEWALKS AND RAMPS SHALL BE MEASURED ON THE INSIDE EDGE OF THE CURVE.

15 FOOT RULE: THE RUNNING SLOPE FOR A CURB RAMP IS NOT LIMITED TO 8.3% MAXIMUM IF THE CONSTRUCTED CURB RAMP EXCEEDS 15 FEET IN LENGTH. THIS EXCEPTION DOES NOT APPLY TO SIDEWALK RAMPS.

GRADE BREAK BETWEEN RAMP AND ADJACENT SIDEWALK MUST BE PERPENDICULAR TO THE DIRECTION OF TRAVEL.

TYPE A CURB IS INCIDENTAL TO CURB RAMP CONSTRUCTION. NO ADDITIONAL PAY WILL BE PROVIDED.

A CLEAR SPACE SHALL BE PROVIDED BEYOND THE BOTTOM GRADE BREAK OF ALL CURB RAMPS. THE CLEAR SPACE SHALL BE A MINIMUM OF 4 FT BY 4 FT AND RESIDE WHOLLY OUTSIDE OF THE PARALLEL VEHICLE TRAVEL LANE.

A 4' BY 4' MINIMUM TURNING SPACE SHALL BE PROVIDED AT THE BOTTOM OF ANY SINGLE RAMP WHICH SERVES MORE THAN ONE DIRECTION, REGARDLESS OF CONFIGURATION. TURNING SPACE SHALL RESIDE WHOLLY OUTSIDE OF BOTH PARALLEL VEHICLE TRAVEL LANES.

A SEGMENTAL TRANSITION SHALL BE PROVIDED WHEN CONNECTING NEW PEDESTRIAN FACILITIES TO AN EXISTING ROADWAY.

ALL SLOPES AND GRADES SHALL BE MEASURED WITH A CALIBRATED 2 FOOT LONG DIGITAL LEVEL.

PERMEABLE PAVEMENT SHALL NOT BE PERMITTED FOR THE CONSTRUCTION OF CURB RAMPS, BLENDED TRANSITIONS, OR TURNING SPACES. ALL PROPOSED PERMEABLE PAVEMENT INSTALLATIONS SHALL REQUIRE DEPARTMENTAL APPROVAL.

ISSUE DATE	03/09/2020
REVISIONS	

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CURB RAMP CONFIGURATIONS FOR USE WITHIN PUBLIC RIGHT-OF-WAY

EFFECTIVE 05/01/2020

SHEET 2 OF 7

DRAWING C608.41

NOTES:

- (1) CROSS SLOPE SHALL BE SUFFICIENT TO PREVENT PONDING UP TO A MAXIMUM OF 2% UNLESS OTHERWISE NOTED. 1.5% (+/-) 0.5% CROSS SLOPE RECOMMENDED.
- (2) THE RUNNING SLOPE OF A BLENDED TRANSITION MAY BE WARPED TO MEET ROAD GRADE UP TO 5% MAXIMUM.
- (3) ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT OR GUTTER SLOPE IN THE DIRECTION OF PEDESTRIAN TRAVEL AND CURB RAMP SLOPE SHALL NOT EXCEED 13.0%. PROVIDE A 24" LEVEL LANDING IF ALGEBRAIC DIFFERENCE EXCEEDS 13.0%.
- (4) CROSS SLOPE SHALL BE A MIN OF 1.0% AND A MAX OF 2.0% AT INTERSECTIONS WITH STOP OR YIELD CONTROL. CROSS SLOPE SHALL BE A MAX OF 5.0% AT INTERSECTIONS WITHOUT STOP OR YIELD CONTROL.
- (5) CURB RAMP RUNNING SLOPE SHALL BE 8.3% MAX.
- (6) TRAVERSABLE FLARE RUNNING SLOPE SHALL BE 10% MAX
- (7) IN ORDER TO PREVENT TRIPPING HAZARDS, RETURN CURBS AND NON-TRAVERSIBLE FLARES MAY BE USED ONLY WHEN NECESSARY. IF GRADING CAN BE PERFORMED TO SURROUNDING VEGETATIVE AREAS THAT IS SIMILAR TO ADJOINING SLOPES, VERTICAL CURBS ADJACENT TO THE PEDESTRIAN ACCESS ROUTE SHALL BE AVOIDED
- (8) OMIT THE 3.5' X 5' LANDING WHEN NO PEDISTRIAN PUSH BUTTON IS REQUIRED

GENERAL NOTES:

THE CONTRACTOR SHALL COMPLY WITH ALL LAWS PERTAINING TO THE AMERICANS WITH DISABILITIES ACT ON PUBLIC RIGHTS OF WAY. EXCEPTIONS MUST BE APPROVED BY THE ENGINEER. ALL AREAS OF NON-COMPLIANCE SHALL BE REMOVED AND CORRECTED AT THE CONTRACTOR'S EXPENSE.

SIDEWALK AND RAMP WIDTH SHALL BE 5' MIN FOR ARTERIAL (ARS), COLLECTOR (CRS-2), AND NON-RESIDENTIAL (CRS) ROADS; 4' MIN WIDTH FOR RESIDENTIAL (CRS) STREETS; 6' MIN WIDTH IN ALL CASES WHEN SIDEWALK IS CONSTRUCTED AT THE BACK OF CURB.

FINISHED SIDEWALK SHALL BE STABLE, SLIP RESISTANT, AND SURFACE SHALL NOT POND WATER.

THE RUNNING GRADE OF A SIDEWALK SHALL NOT EXCEED 5.0% EXCEPT WHEN MATCHING THE GRADE OF AN ADJACENT ROADWAY.

A DIAGONAL RAMP IS A SINGLE CURB RAMP WHICH SERVES TWO CROSSINGS. DIAGONAL RAMPS SHALL NOT BE PERMITTED FOR NEW CONSTRUCTION OR REDEVELOPMENT PROJECTS. DIAGONAL RAMPS MAY ONLY BE USED FOR ALTERATION TYPE PROJECTS WHERE CONSTRUCTION OF SPLIT RAMPS IS NOT PRACTICABLE AS DETERMINED BY THE ENGINEER.

LANDINGS, TURNING SPACES, AND CLEAR SPACES LOCATED IN THE ROADWAY MUST BE COMPLETELY CONTAINED WITHIN THE CROSSWALK.

TRANSITIONS FROM SIDEWALK TO GUTTER TO ROADWAY MUST BE FLUSH.

DETECTABLE WARNING DEVICES ARE REQUIRED AT PUBLIC STREETS AND SIGNALIZED ENTRANCES. FOR DETECTABLE WARNING SURFACE REQUIREMENTS SEE STANDARD DRAWING C608.51

RUNNING SLOPE OF CURVED SIDEWALKS AND RAMPS SHALL BE MEASURED ON THE INSIDE EDGE OF THE CURVE.

15 FOOT RULE: THE RUNNING SLOPE FOR A CURB RAMP IS NOT LIMITED TO 8.33% MAXIMUM IF THE CONSTRUCTED CURB RAMP EXCEEDS 15 FEET IN LENGTH. THIS EXCEPTION DOES NOT APPLY TO SIDEWALK RAMPS.

GRADE BREAK BETWEEN RAMP AND ADJACENT SIDEWALK MUST BE PERPENDICULAR TO THE DIRECTION OF TRAVEL.

TYPE A CURB IS INCIDENTAL TO CURB RAMP CONSTRUCTION. NO ADDITIONAL PAY WILL BE PROVIDED.

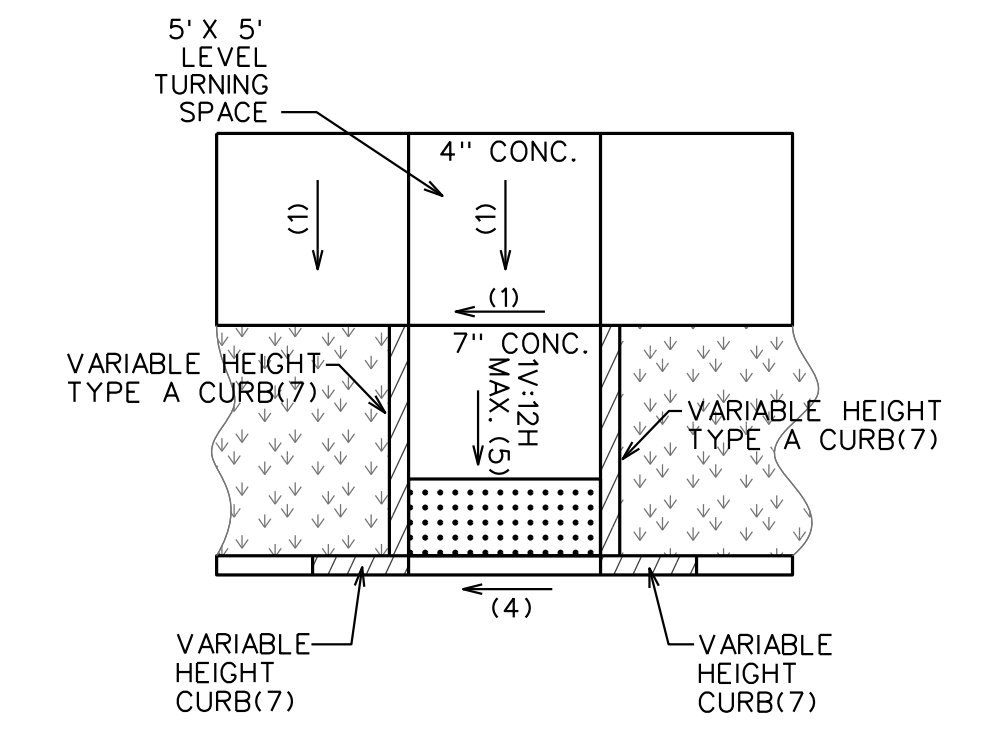
A CLEAR SPACE SHALL BE PROVIDED BEYOND THE BOTTOM GRADE BREAK OF ALL CURB RAMPS. THE CLEAR SPACE SHALL BE A MINIMUM OF 4 FT BY 4 FT AND RESIDE WHOLLY OUTSIDE OF THE PARALLEL VEHICLE TRAVEL LANE.

A 4' BY 4' MINIMUM TURNING SPACE SHALL BE PROVIDED AT THE BOTTOM OF ANY SINGLE RAMP WHICH SERVES MORE THAN ONE DIRECTION, REGARDLESS OF CONFIGURATION. TURNING SPACE SHALL RESIDE WHOLLY OUTSIDE OF BOTH PARALLEL VEHICLE TRAVEL LANES.

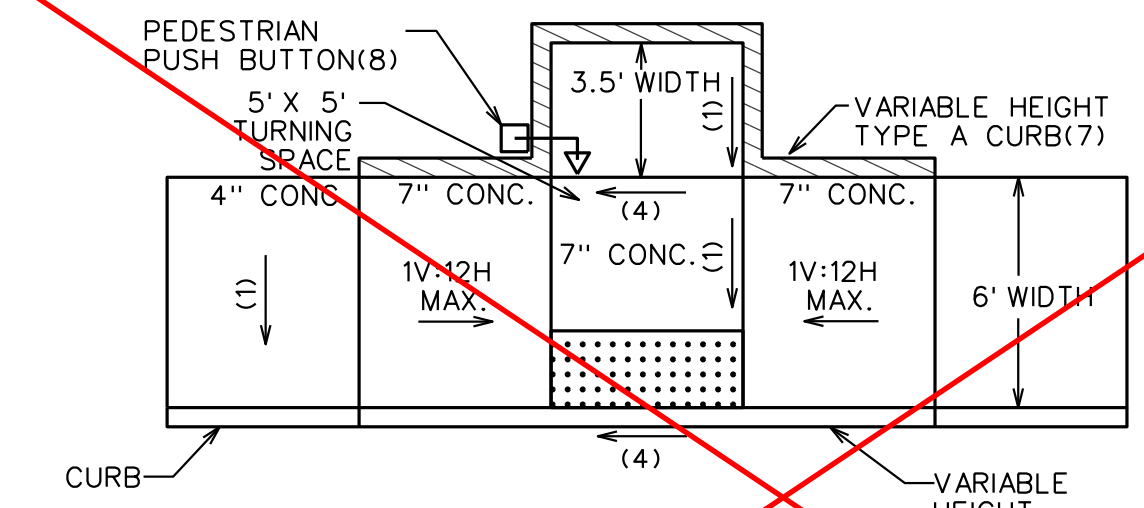
A SEGMENTAL TRANSITION SHALL BE PROVIDED WHEN CONNECTING NEW PEDESTRIAN FACILITIES TO AN EXISTING ROADWAY.

ALL SLOPES AND GRADES SHALL BE MEASURED WITH A CALIBRATED 2 FOOT LONG DIGITAL LEVEL.

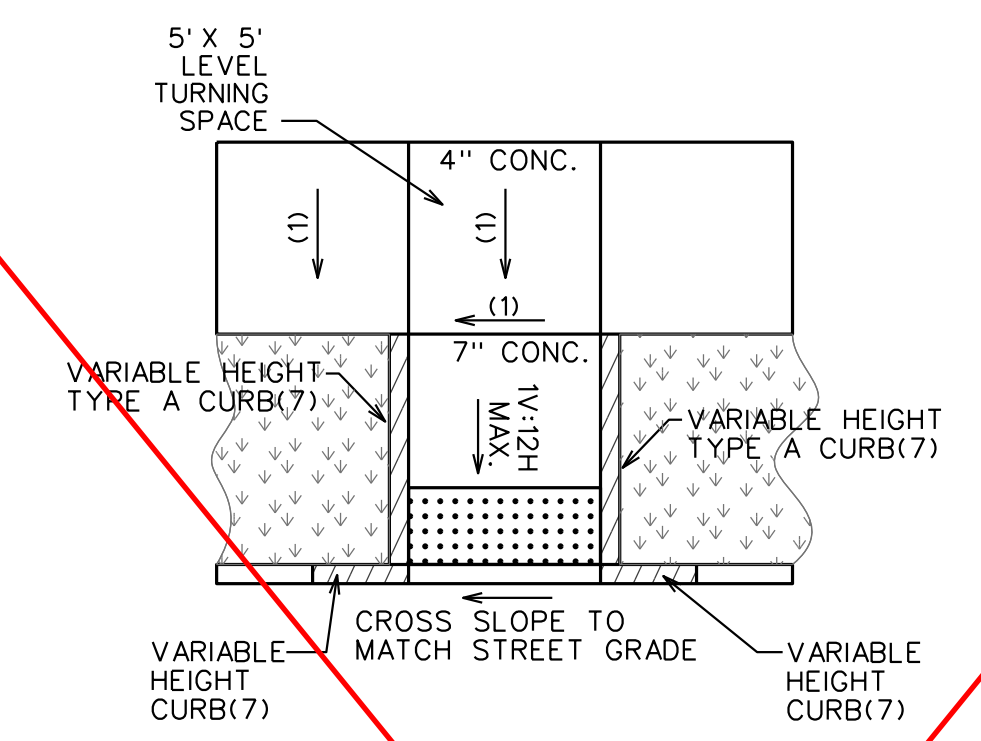
PERMEABLE PAVEMENT SHALL NOT BE PERMITTED FOR THE CONSTRUCTION OF CURB RAMPS, BLENDED TRANSITIONS, OR TURNING SPACES. ALL PROPOSED PERMEABLE PAVEMENT INSTALLATIONS SHALL REQUIRE DEPARTMENTAL APPROVAL.



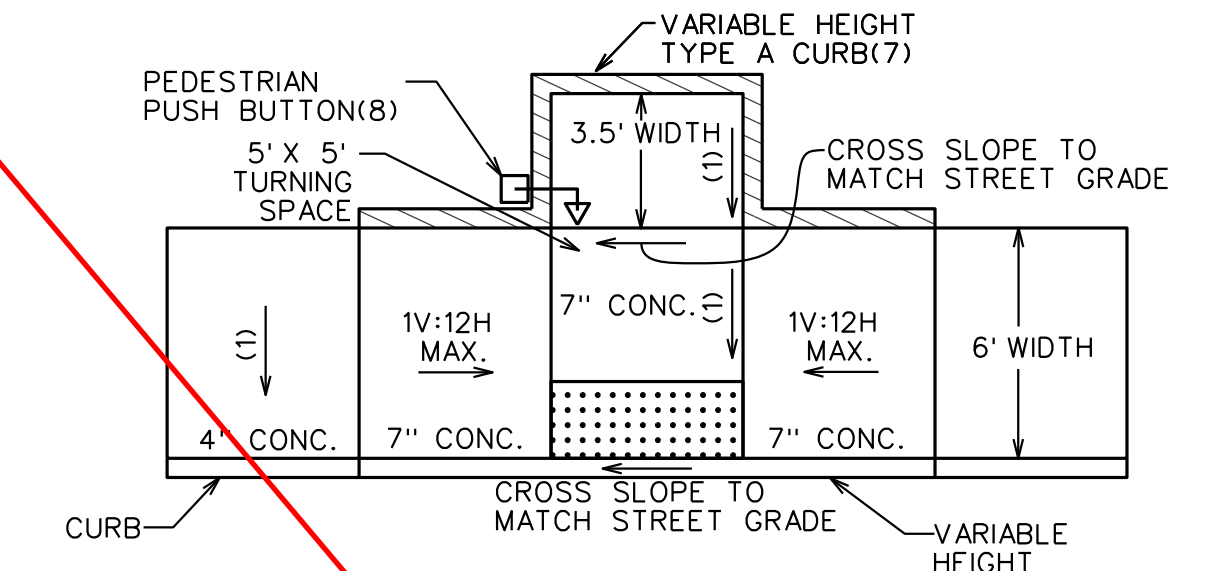
**CONFIGURATION S-PERP-8:
TYPE B CURB RAMP
SINGLE PERPENDICULAR**



**CONFIGURATION S-PAR-3:
TYPE C CURB RAMP
SINGLE PARALLEL**

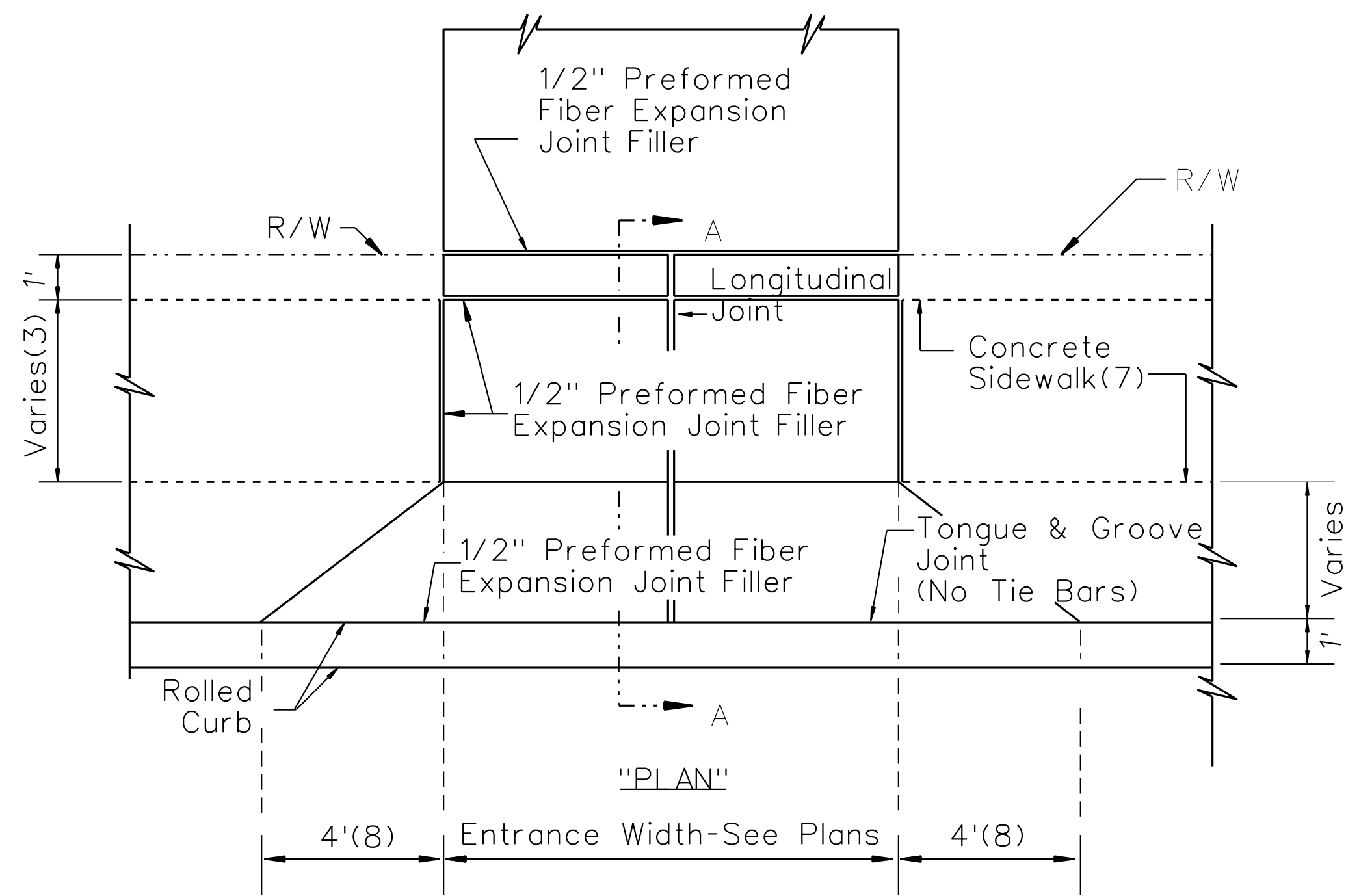


**CONFIGURATION MB-1:
TYPE B CURB RAMP
SINGLE PERPENDICULAR
AT MIDBLOCK CROSSING**

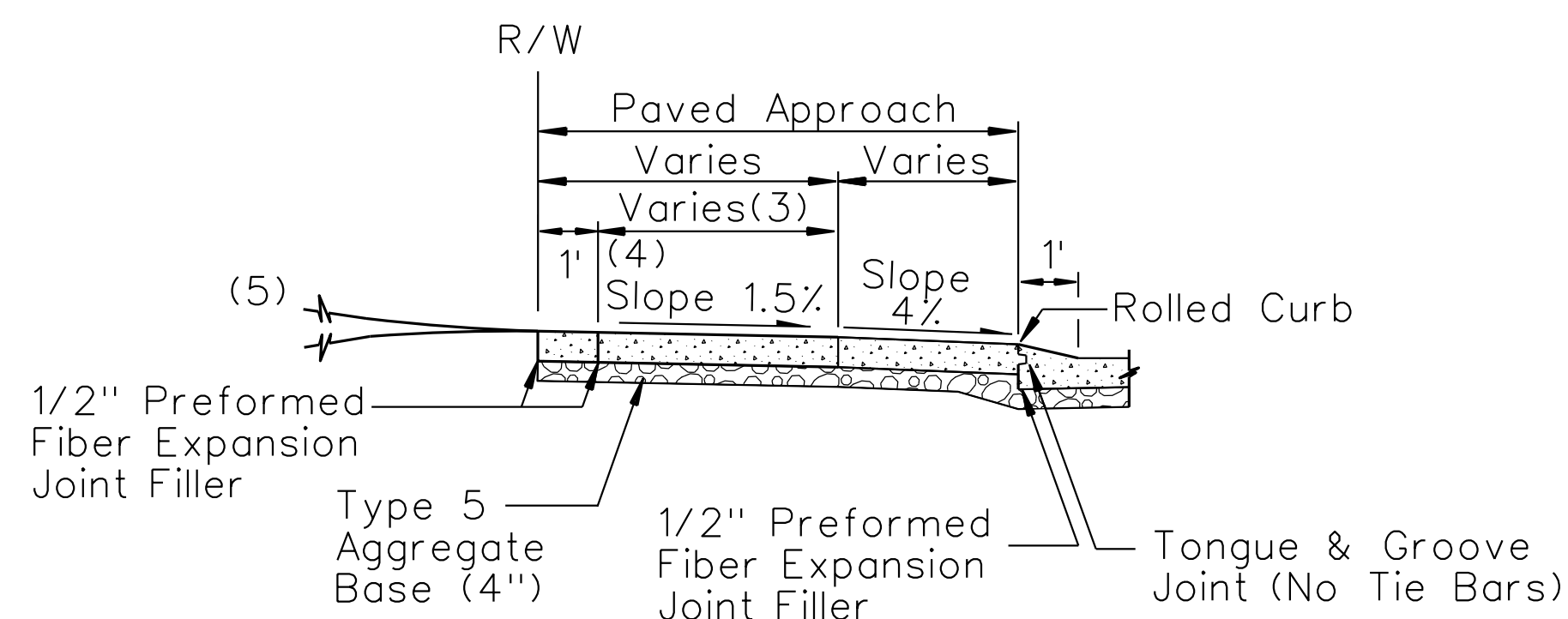


**CONFIGURATION MB-2:
TYPE C CURB RAMP
SINGLE PARALLEL
AT MIDBLOCK CROSSING**

ISSUE DATE 03/09/2020	REVISIONS	THIS IS NOT A CERTIFIED DOCUMENT	Saint Louis COUNTY TRANSPORTATION <small>1050 NORTH LINDBERGH BLVD. ST. LOUIS, MISSOURI 63132</small>	CURB RAMP CONFIGURATIONS FOR USE WITHIN PUBLIC RIGHT-OF-WAY			
				<table style="width: 100%; border: none;"> <tr> <td style="border: 1px solid black; padding: 5px;">EFFECTIVE 05/01/2020</td> <td style="border: 1px solid black; padding: 5px;">SHEET 6 OF 7</td> <td style="border: 1px solid black; padding: 5px;">DRAWING C608.41</td> </tr> </table>	EFFECTIVE 05/01/2020	SHEET 6 OF 7	DRAWING C608.41
EFFECTIVE 05/01/2020	SHEET 6 OF 7	DRAWING C608.41					



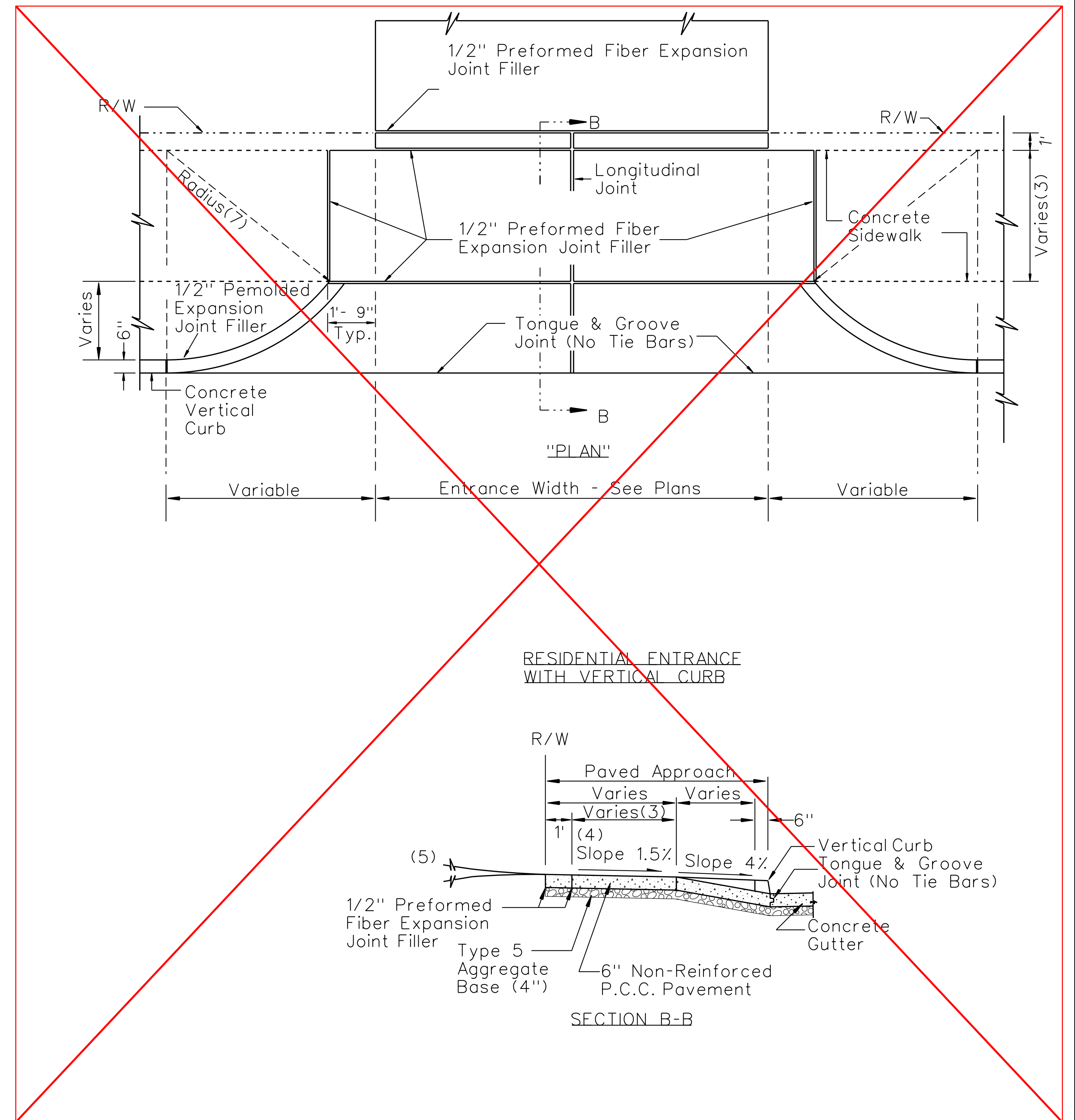
RESIDENTIAL ENTRANCE WITH ROLLED CURB



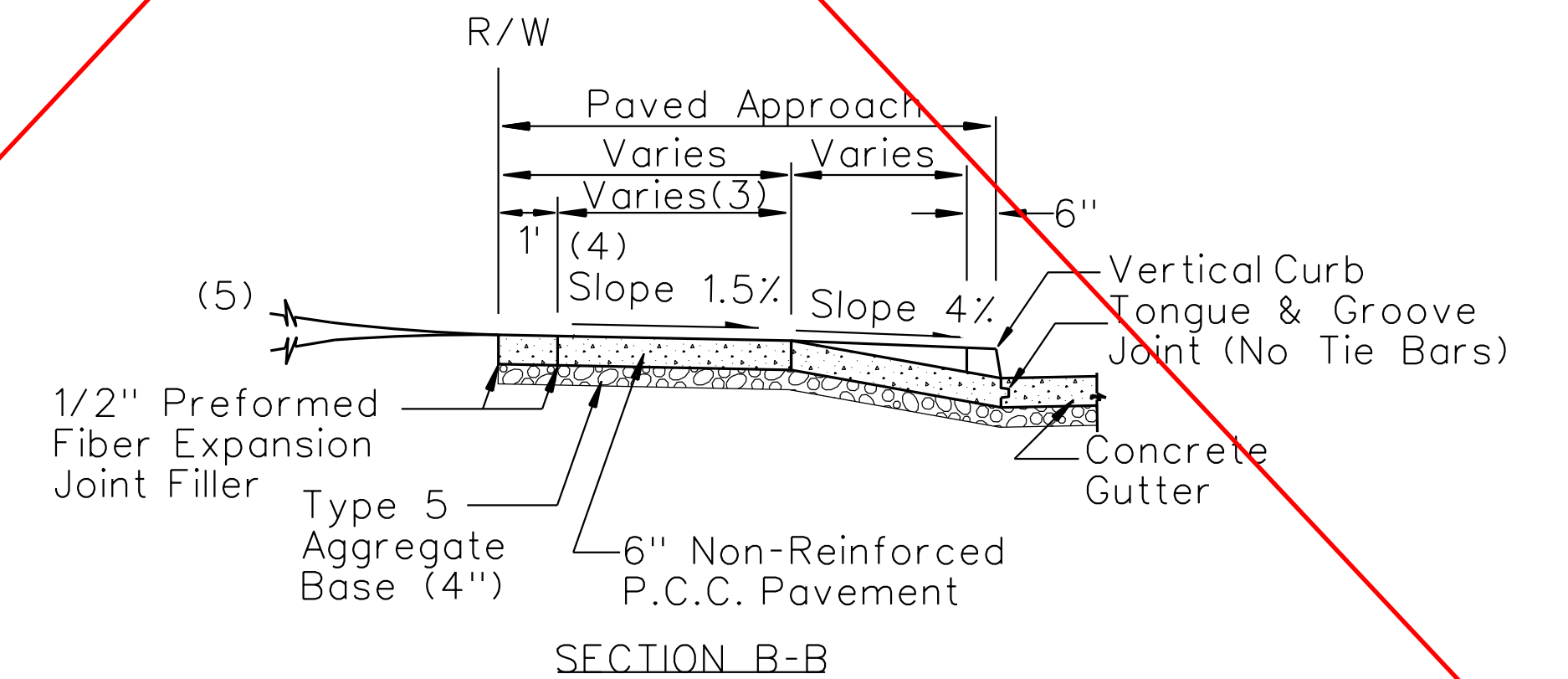
SECTION A - A

GENERAL NOTES

- 1) Do not scale drawing, follow dimensions.
- 2) The thickness of the first section of sidewalk on each side of the entrance shall be increased to match the driveway approach pavement thickness (6 inches).
- 3) Minimum sidewalk width shall be as defined by the sidewalk master plan. Sidewalk width shall be a minimum of 5 feet where not defined by the sidewalk master plan. Sidewalk width may be reduced to 4 feet within residential subdivision developments.
- 4) Cross slopes 1.5% (+/- 0.5%)
- 5) See Design Criteria For The Preparation of Improvement Plans, Saint Louis County Transportation.
- 6) Approach shall be platformed to accommodate future sidewalk when new sidewalk is not required.
- 7) See approved plan.
- 8) Driveway flare is optional within residential subdivisions. Flare is required along Arterial and Collector roadways.
- 9) Additional construction requirements apply for driveways opposite "T" intersections or on outside of a curve, refer to "Entrance Construction Details Guidelines for Joint Filler Installation", Std. Dwg. C608.10.
- 10) This detail is not applicable where sidewalk is installed at the back of curb.

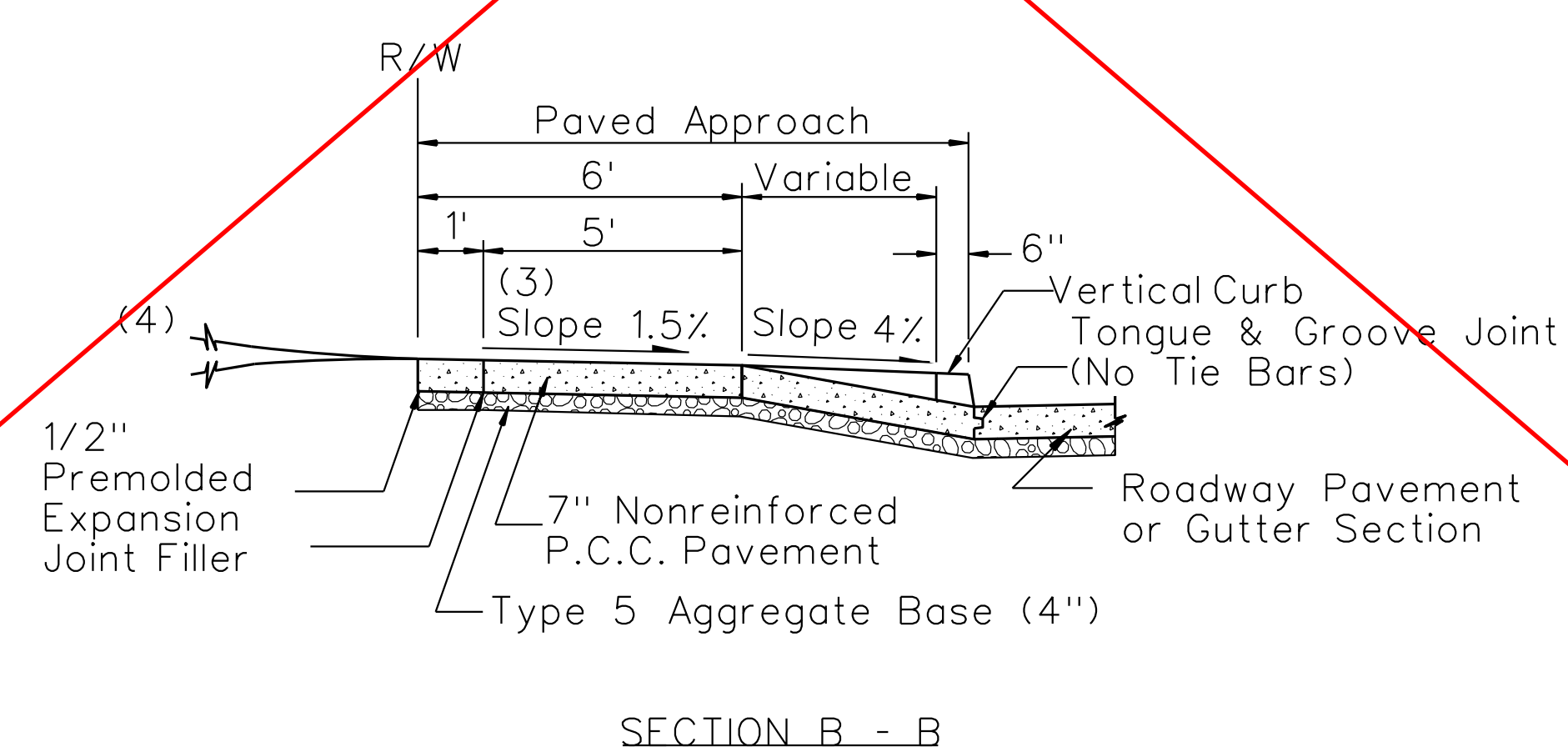
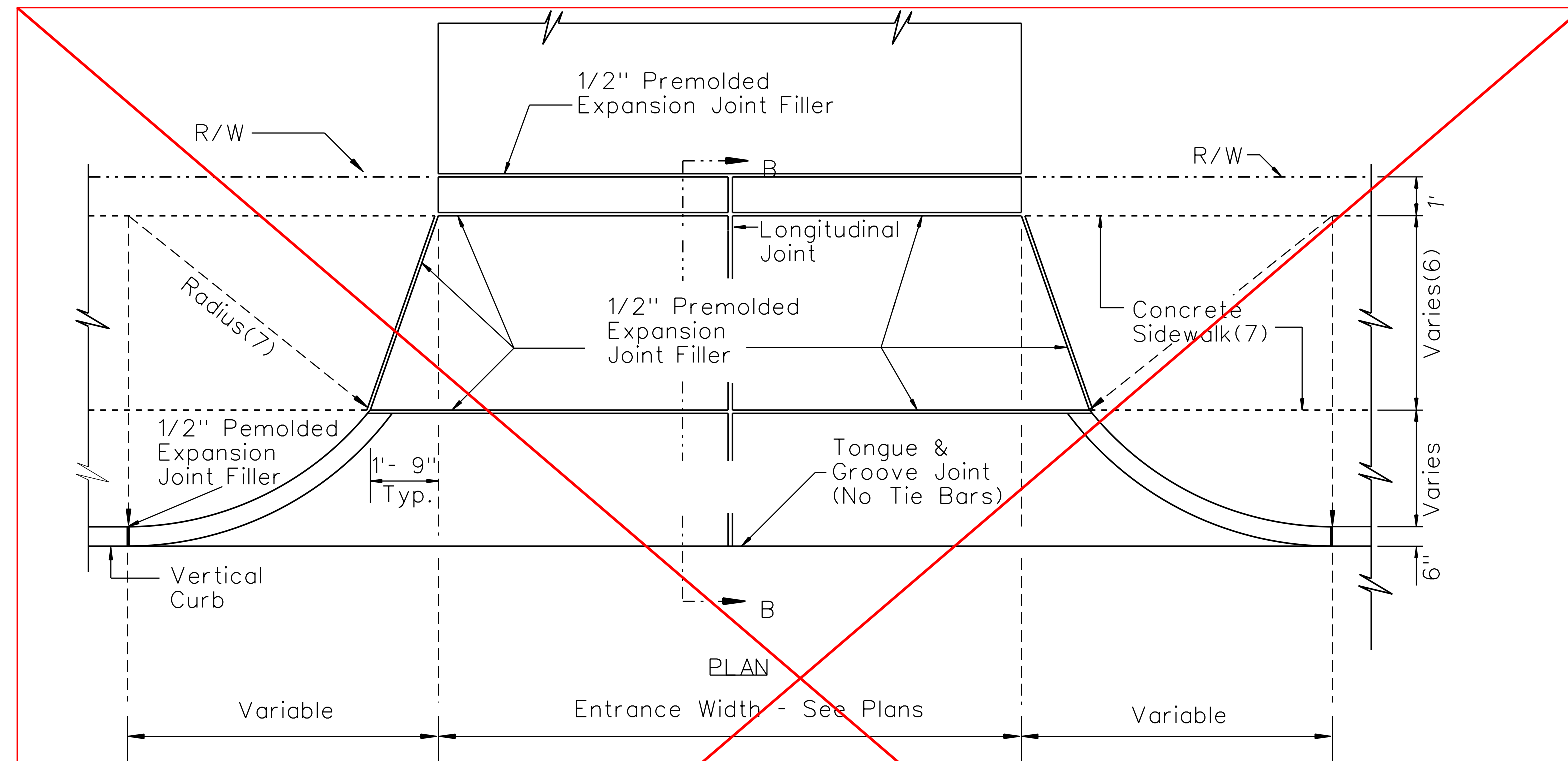
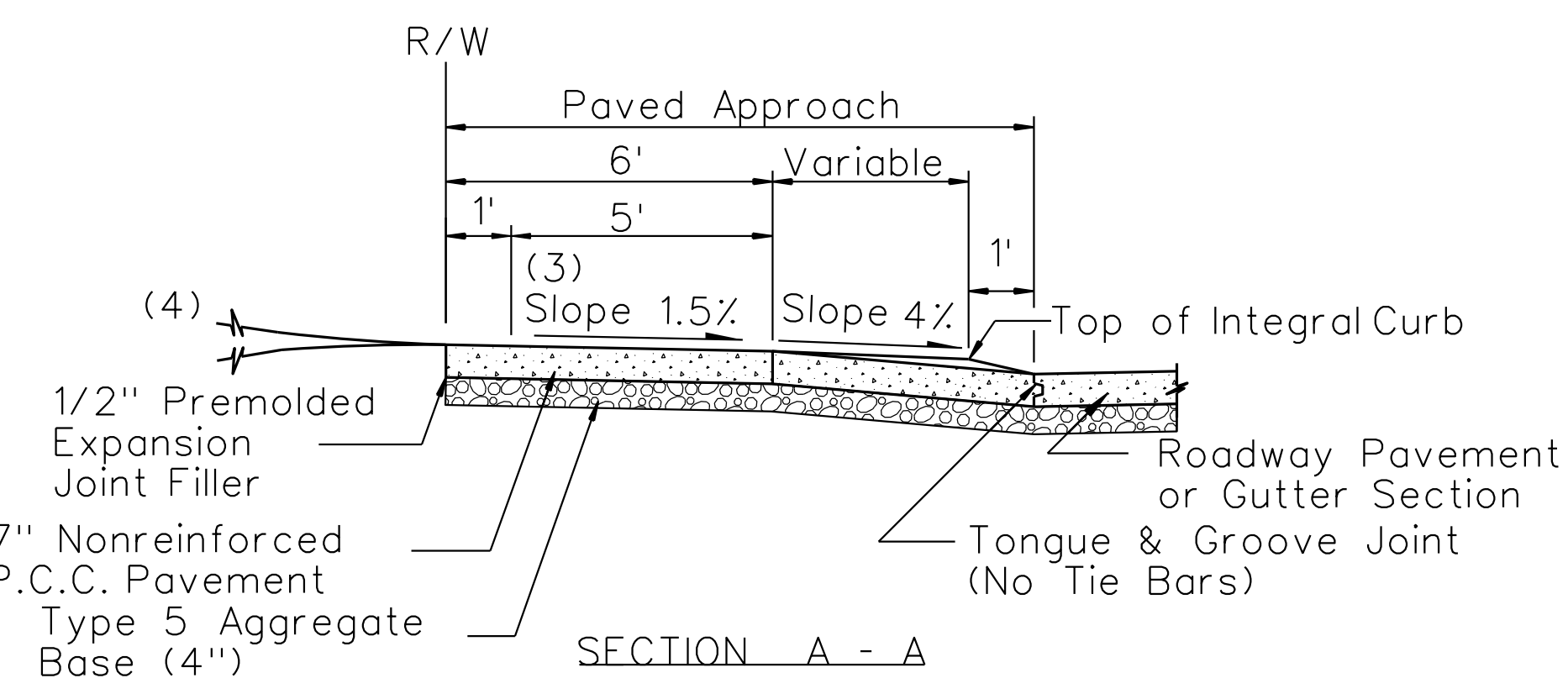
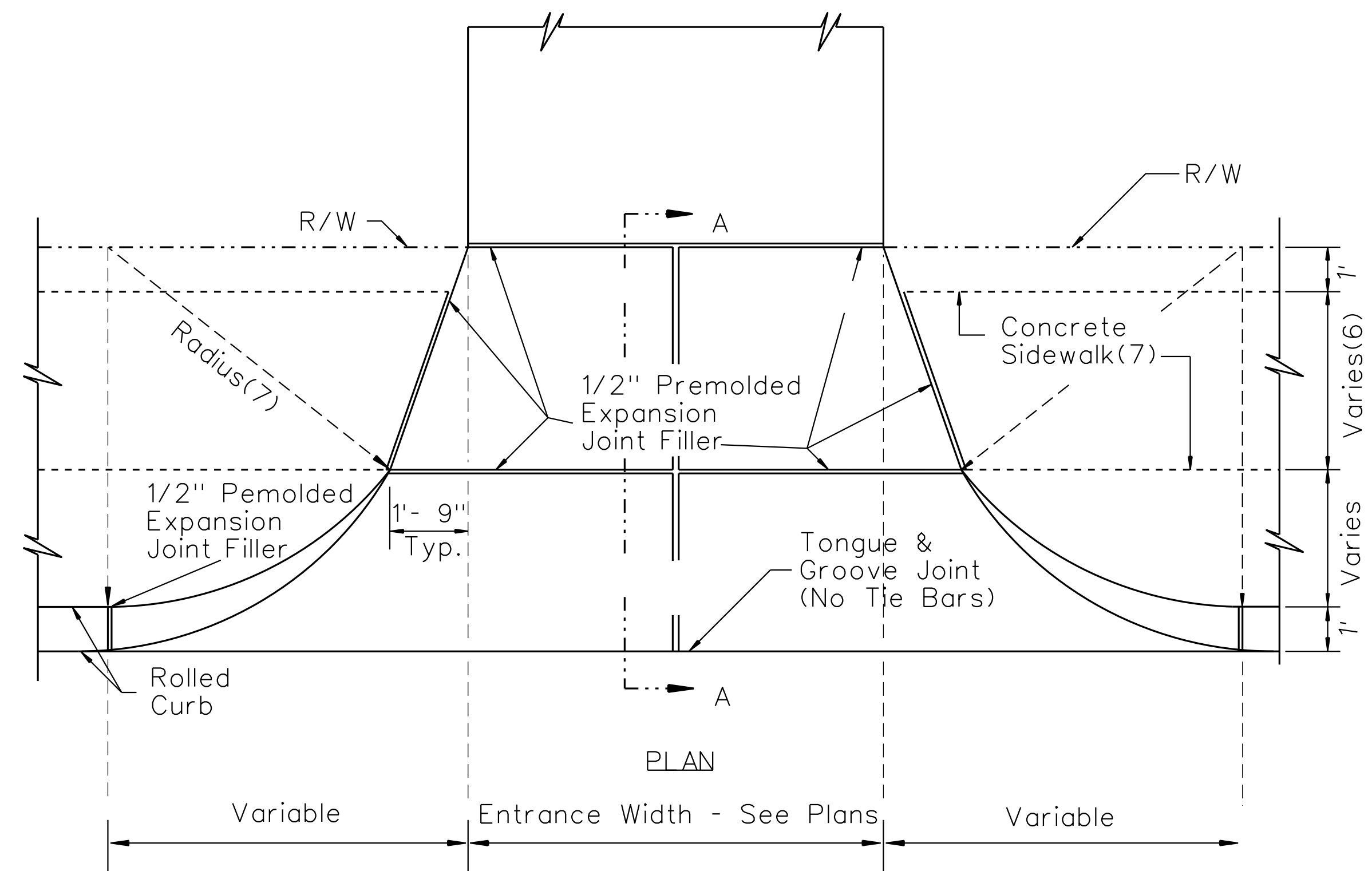


RESIDENTIAL ENTRANCE WITH VERTICAL CURB



SECTION B-B

ISSUE DATE 03/09/2020	THIS IS NOT A CERTIFIED DOCUMENT	Saint Louis COUNTY TRANSPORTATION <small>1050 NORTH LINDBERGH BLVD. ST. LOUIS, MISSOURI 63132</small>	ENTRANCE CONSTRUCTION DETAILS		
REVISIONS			RESIDENTIAL CONCRETE ENTRANCES		
			EFFECTIVE 05/01/2020	SHEET 1 OF 1	DRAWING C608.00



GENERAL NOTES

- 1) Do not scale drawing, follow dimensions.
- 2) The thickness of the first section of sidewalk on each side of the entrance shall be increased to match the driveway approach pavement thickness (7").
- 3) Cross slopes 1.5% (+/- 0.5%)
- 4) See Design Criteria For The Preparation of Improvement Plans, Saint Louis County Transportation.
- 5) Approach slab shall be platformed to accommodate future sidewalk where new sidewalk will not be constructed adjacent to the entrance.
- 6) Minimum sidewalk width shall be as defined by the sidewalk master plan. Sidewalk width shall be a minimum of 5 feet where not defined by the sidewalk master plan.
- 7) See approved plan.
- 8) Additional construction requirements apply for driveways opposite "T" intersections or on outside of a curve, refer to "Entrance Construction Details Guidelines for Joint Filler Installation", Std. Dwg. C608.10.
- 9) This detail is not applicable where sidewalk is installed at the back of curb.

ISSUE DATE 03/09/2020	THIS IS NOT A CERTIFIED DOCUMENT	Saint Louis COUNTY TRANSPORTATION <small>1050 NORTH LINDBERGH BLVD. ST. LOUIS, MISSOURI 63132</small>	ENTRANCE CONSTRUCTION DETAIL		
REVISIONS			COMMERICAL CONCRETE ENTRANCE		
			EFFECTIVE 05/01/2020	SHEET 1 OF 1	DRAWING C608.01