

# Longview Boulevard Sidewalk Project Engagement Report

## Introduction

Public input will be used along with technical analysis to determine the best alignment for a sidewalk on Longview Boulevard and East Essex. Rather than present the public with a draft alignment based on engineering analysis, which is typically the case, the City chose to gather public input before an alignment was developed, offering the public greater influence on the selection.

During the month of June 2022, public input was collected through an online Community Survey and an in-person Open House with an online companion. Both were promoted heavily through the City of Kirkwood's communication channels. In addition, a mailing went out to property owners on or near Longview to encourage feedback from those impacted most by the project. Engagement results for participants on or adjacent to Longview are noted in this report so they can be given special consideration.

Engagement results provide valuable insight into community preferences. Because they rely on self-selection by respondents rather than a random-sample, results don't necessarily reflect the opinions of the community as-a-whole and should be applied carefully.

This report includes an Executive Summary and public engagement results organized into five sections:

- I. About the Participants
- II. Sidewalk Preference: Sidewalk Type
- III. Sidewalk Preference: Sidewalk Location
- IV. Bike Lanes, Traffic Calming, and Other Improvements
- V. General Comments

## Executive Summary

- The type of sidewalk/general location preferred based on Community Survey responses (341 respondents) is the sidewalk in the grassy median (53%), followed by a sidewalk with tree lawn (31%) and sidewalk at back of curb (23%). Respondents living on Longview, or an adjacent street had a stronger preference for the median option and ranked sidewalk with tree lawn last.
- The specific locations preferred based on feedback from Open House participants (36 participants), most of whom were Longview residents are:
  - Area 1, on Longview from Taylor to Club: Tie - Sidewalk at curb on north side (50%), south side (50%)
  - Area 2, on Longview from Club to N Woodlawn: Sidewalk in median preferred (88%)
  - Area 3, on E Essex from Longview to N Woodlawn: Sidewalk at curb, northside preferred (62%)
- Public engagement participants frequently mentioned safety as their biggest concern. Longview from Taylor to Club was often cited specifically as an area with safety concerns that should be prioritized for a sidewalk.
- Regarding bike facilities, most Community Survey (71%) respondents preferred an on-street, shared bike lane.
- Community Survey respondents indicated support for traffic calming measures with a ranking of 6.5 on a 10-point scale. Support for traffic calming was stronger among Longview residents with a ranking of 7.9.
- Regarding other improvements to Longview, Community Survey respondents ranked walking improvements the highest, followed by traffic calming and speed reduction. Among Longview residents, walking improvements and traffic calming were tied. Cycling improvements ranked lowest among all respondents, including those on Longview.
- Based on written comments to the Community Survey and Open House, more respondents support the project than oppose it, including those who live on or adjacent to Longview.



## Public Engagement Results

### I. About Engagement Participants (Community Survey and Open House)

#### Overall Participation

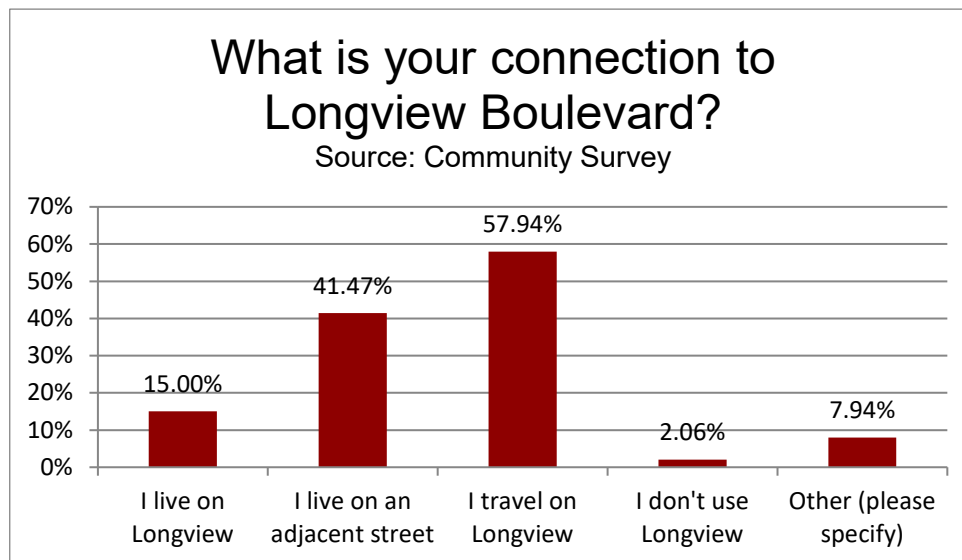
Community Survey. The Community Survey was completed by 341 respondents between June 1 and June 28. About half of survey respondents live on or near Longview, another half experience Longview as travelers through the area.

Open House. An Open-House was held on June 21 from 4-6 p.m. at the Kirkwood Performing Arts Center. A companion online meeting that was posted on the City's website from June 22 to June 28. A total of thirty-eight (38) people participated in the Open House. Most public meeting participants lived on Longview or an adjacent street.

#### Community Survey Participants

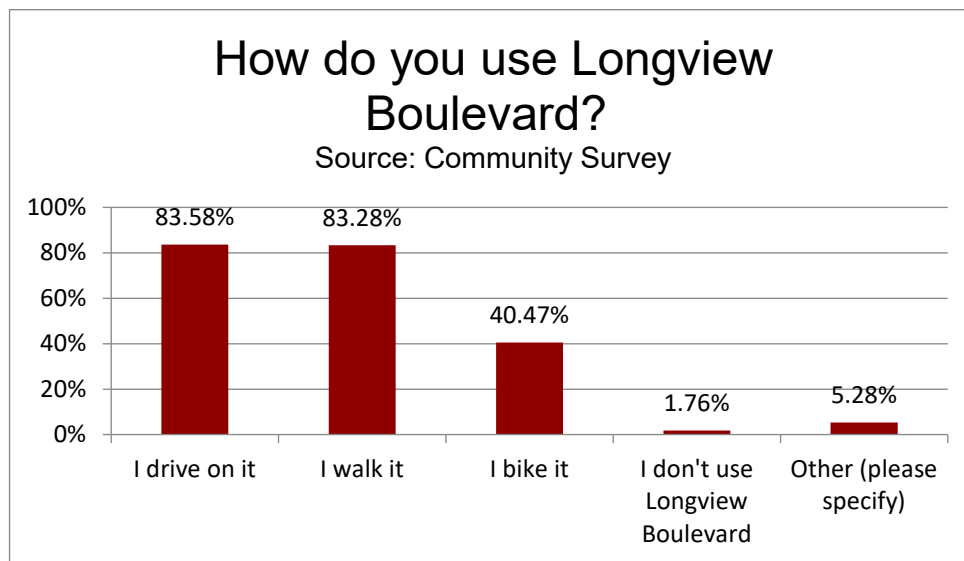
Community Survey respondents were almost evenly split between those who experience Longview as travelers (58%) and those who lived on Longview or an adjacent street (56%). 8% had other connections to Longview like walking and biking, and only 2% of respondents noted that they don't use Longview Boulevard.

Figure 1. Respondent relationship to Longview (Community Survey)



Respondents were asked how they use Longview Blvd. 84% reported driving on Longview, 83% walk on Longview, 40% bike on Longview, 5% reported other uses like running and playing, and less than 2% of respondents noted that they don't use Longview.

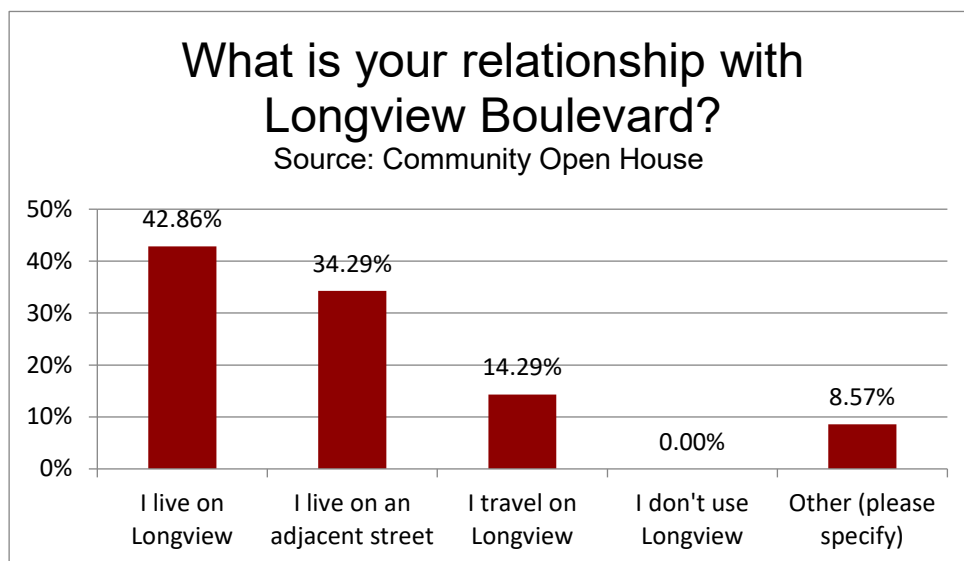
Figure 2. Usage of Longview (Community Survey)



**Open House Participants**

Public meeting participants were much more likely to live on Longview or an adjacent street compared to community survey respondents. Of the thirty-six meeting attendees, fifteen (43%) lived on Longview, twelve (34%) lived on an adjacent street, five (14%) travel Longview, and three (9%) had other connections like walking or biking on Longview.

Figure 3. Participant relationship to Longview (Community Open House)



## II. Sidewalk Preference: Sidewalk Type (Community Survey Only)

In the Community Survey, participants were offered three sidewalk types, pictured below, to consider.

Response Option 1: Sidewalks with Tree Lawn



Response Option 2: Sidewalk at back of curb



Response Option 3: Sidewalk in Median

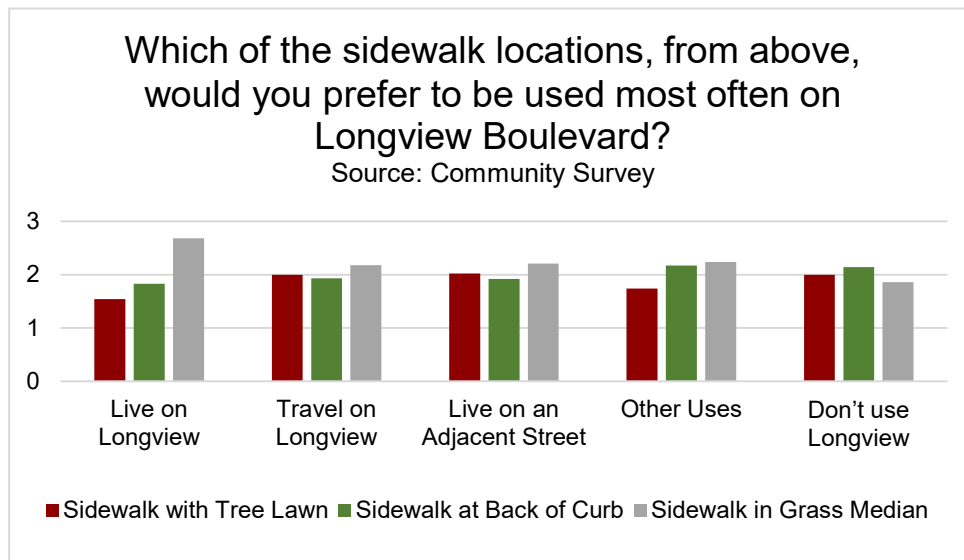


Respondents were asked to rank which they would prefer to be used most often on Longview. Rankings were as follows:

- Sidewalk in grassy median was ranked first (53%)
- Sidewalk with tree lawn was ranked second (31%)
- Sidewalk at back of curb was ranked third (23%)

Figure 4 below shows sidewalk type ratings by respondent relationship to Longview. All groups, other than the few who reported not using Longview, favored the sidewalk in grassy median option. Respondents living on Longview showed the strongest preference for this option. While Option 2, sidewalk with tree lawn ranked second overall, it was the least preferred option among Longview residents.

Figure 4. Sidewalk Preference: Sidewalk Type (Community Survey)



Survey respondents were asked about reasons for their preference.

- Sidewalk in Grassy Median (ranked #1)  
The most common reason provided, by a significant margin, was less impact to property owners. Other reasons included safety, aesthetics, and the option offers a good use of the median.
- Sidewalk with Tree Lawn (ranked #2)  
The most common reason provided by a significant margin, was safety. Added green space was the next most common reason.
- Sidewalk at Back of Curb (ranked #3)  
The most common reason, by a significant margin, was less impact to property owners.

Of note, the area from Club to Taylor on Longview was mentioned specifically by a number of respondents as an area with safety concerns in need of sidewalks.

Sample of respondent comments regarding safety concerns between Taylor and Club on Longview:

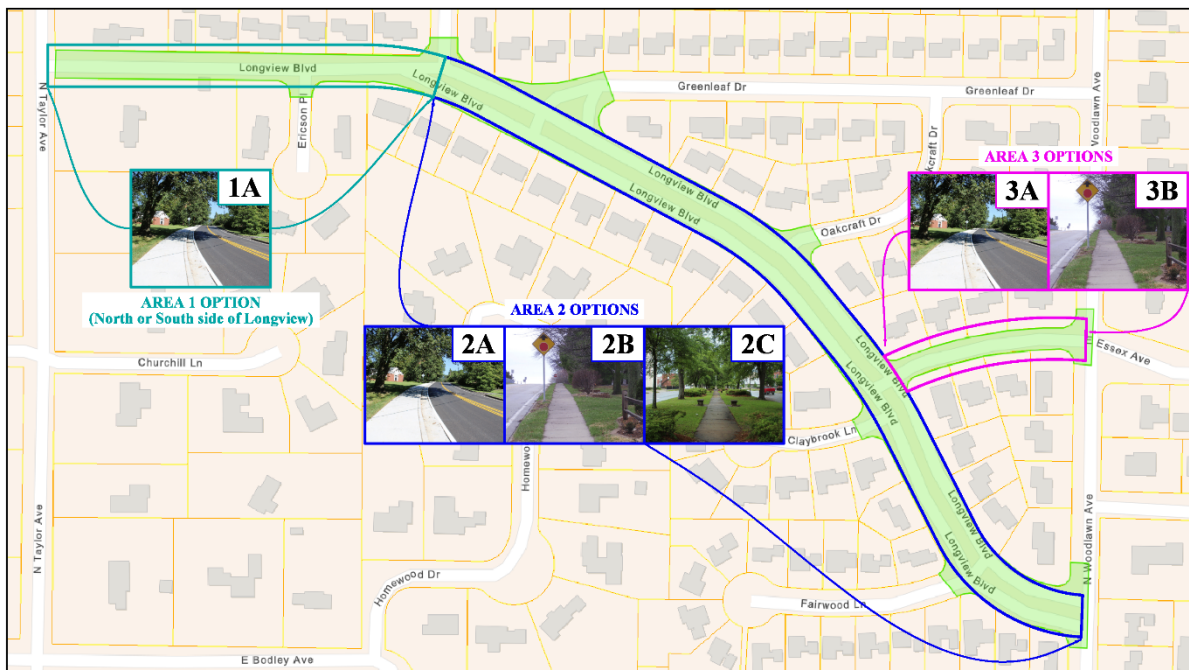
“I mainly walk the narrow section of Longview between Taylor and Club. It can be scary to have my young children with me (4, 2 and 4 months) since cars go so fast on Longview and there’s little to no room us pedestrians. We often have to pull over and stop to give way to cars or if we’re by the bushes and can’t “pull over” we pray the cars see us and move over a bit.”

“I would love to see a sidewalk that reaches Taylor. It is our only way to walk to downtown Kirkwood, Walker Park, etc. (via Taylor), but it is really unsafe. There’s no sidewalk, and the overgrown brush blocking vision of drivers turning right from Taylor to Longview. It’s an accident waiting to happen.”

### III. Sidewalk Preference: Specific Location

At the Longview Open House, participants were shown sidewalk examples and locations for three sections of the project area and asked to provide their preference on a comment form.

Figure 5. Open House Display Board, Sidewalk Location Options

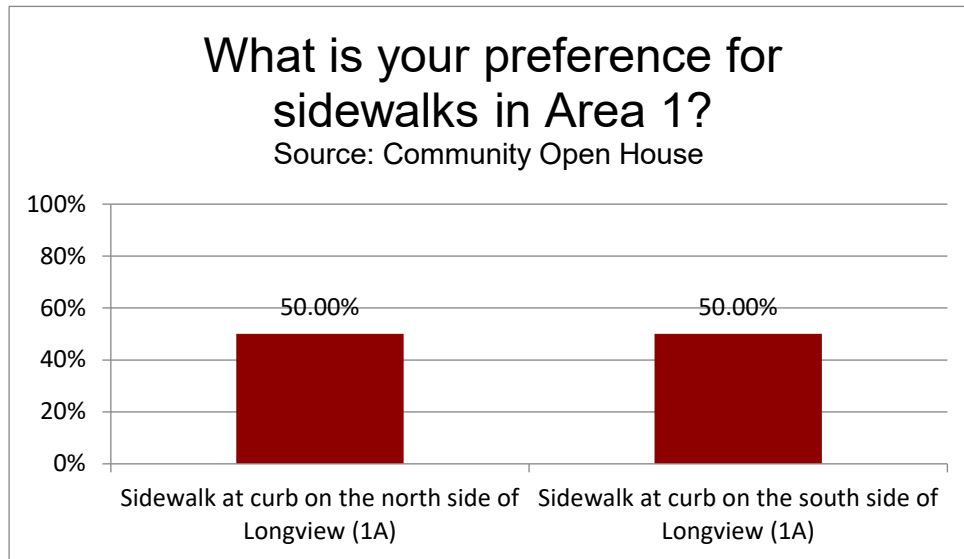


Area 1

*Tie: Sidewalk at curb on north side (50%), south side (50%)*

In Area 1, respondents could choose between two options: a sidewalk on the north side of Longview or a sidewalk on the south side of Longview. Results show a tie between the north and south sides, both chosen 50% of the time.

Figure 6. Sidewalk Preference: Specific Location, Area 1 (Community Open House)

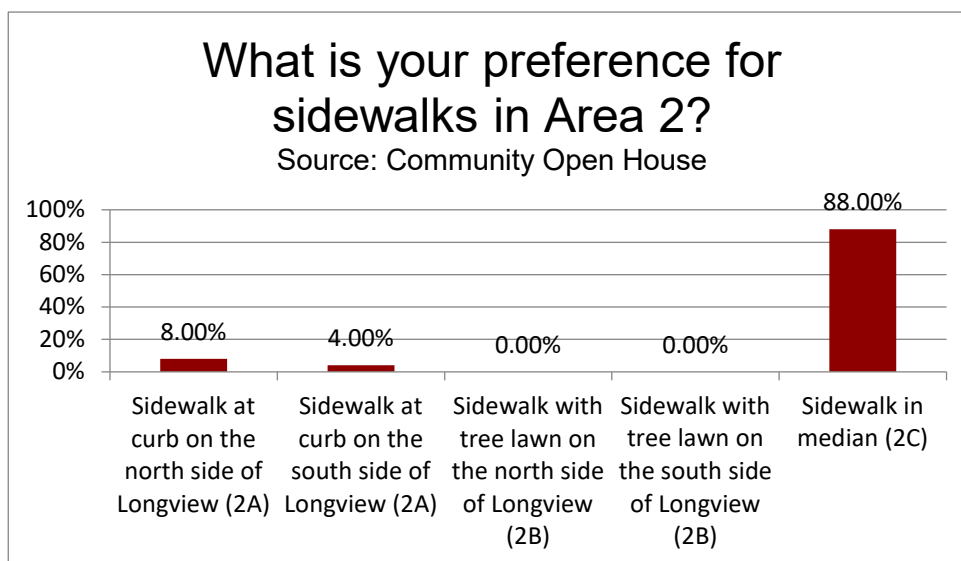


Area 2

*Sidewalk in median preferred (88%)*

In Area 2, respondents could choose between five options. Sidewalk in median was chosen most often, 88% of the time. Sidewalk at curb on north side was chosen 8% of the time and sidewalk on south side was chosen 4% of the time. Of note, no participants chose sidewalks with tree lawns on the north or south sides of Longview.

Figure 7. Sidewalk Preference: Specific Location, Area 2 (Community Open House)



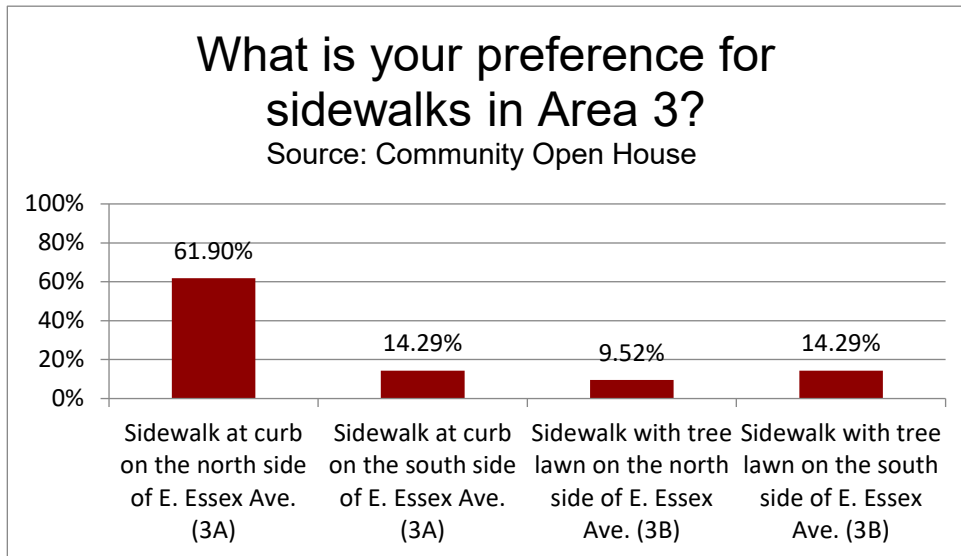


### Area 3

*Sidewalk at curb, northside preferred (62%)*

Four options were available for Area 3. 62% of respondents chose sidewalk at curb on the north side of E. Essex. Both sidewalk on curb on the south side of E. Essex and sidewalk with tree lawn on the south side of E. Essex were chosen 14% of the time. Sidewalk with tree lawn on the north side of E. Essex was chosen 10% of the time.

Figure 8. Sidewalk Preference: Specific Location, Area 3 (Community Open House)

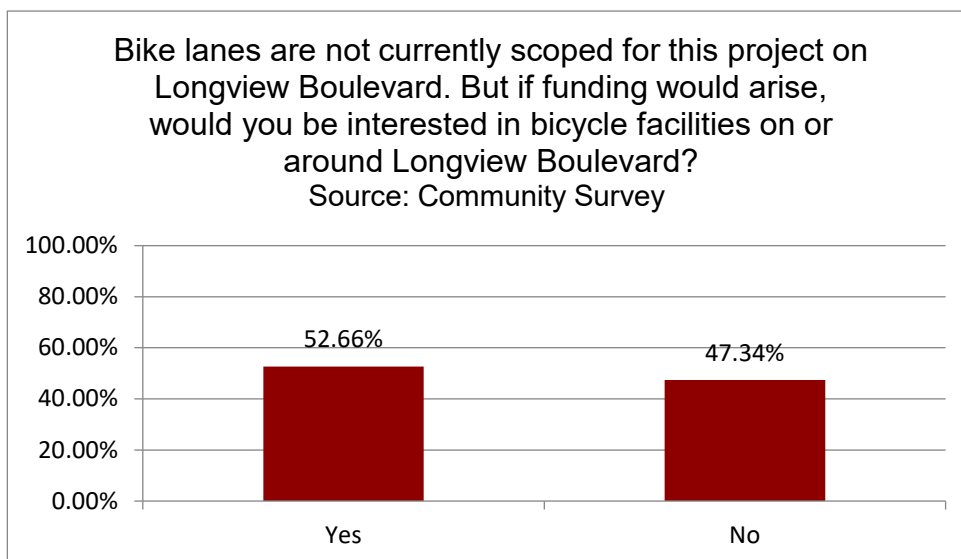


#### IV. Bike Lanes, Traffic Calming, and Other Improvements (Community Survey Only)

##### **Bike Lanes**

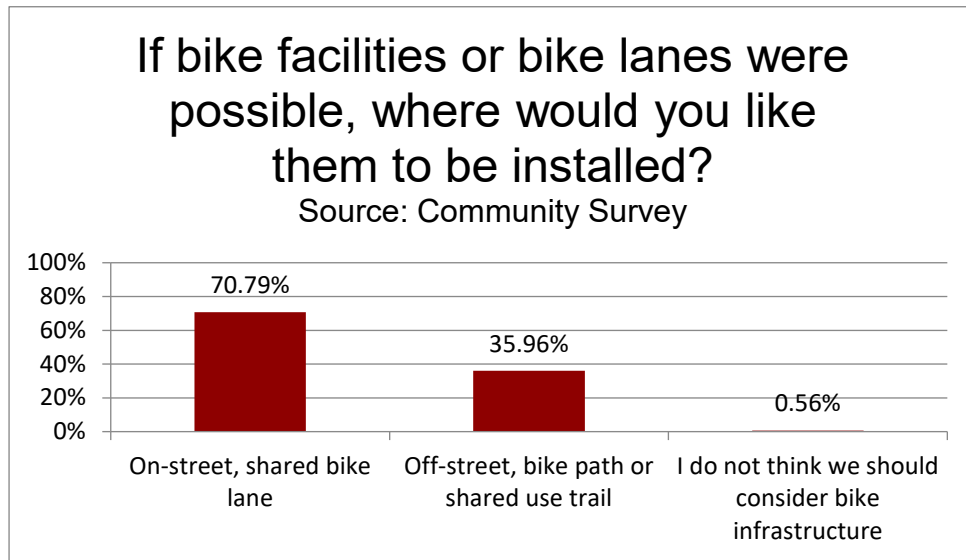
Bike lanes are not currently scoped for this project, however in an effort to gauge interest, respondents to the Community Survey were asked their opinion on having bike facilities on or around Longview. 53% of respondents said they would be interested in bike facilities and 47% noted they were not interested.

Figure 9. Interest in Bicycle Facilities (Community Survey)



Longview Boulevard Survey respondents who were interested in having bike facilities were asked where they should be installed and why. 71% of respondents said they would like to see on-street-shared bike lanes and 36% preferred off-street bike paths or shared use trails. Common responses included impact on property owners, similarity to other bike facilities in Kirkwood, and safety.

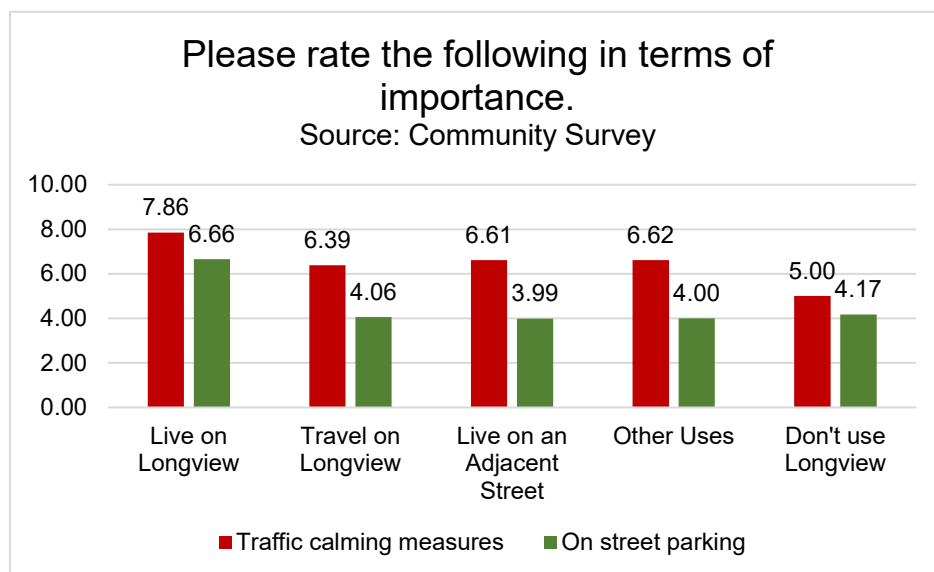
Figure 10. Preferred Location for Bike Facilities (Community Survey)



**Street Parking and Traffic Calming**

Street parking and traffic calming on Longview are both of moderate concern. Based on a scale of 1 to 10 where 1 was not at all concerned and 10 was very concerned, street parking had a rating of 4.5 while traffic calming measures rated slightly higher at 6.5. As shown in Figure 11 residents who live on Longview tend to be more concerned about traffic calming measures and on-street parking than other groups, with levels of concern of 7.9 and 6.7, respectively.

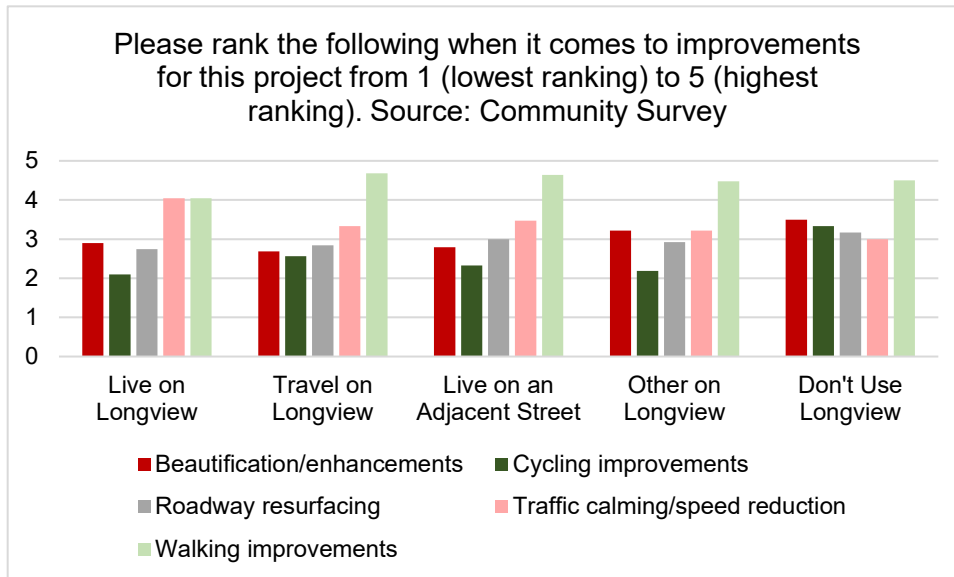
Figure 11. Interest in Street Parking and Traffic Calming (Community Survey)



## Project Improvements

Based on a scale of 1 to 5 where 5 was the highest ranking, survey respondents were asked to rank project improvements on Longview including cycling improvements, roadway resurfacing, beautification/enhancements (Including existing features), walking improvements, and traffic calming/speed reduction. Improvements to support walking was ranked highest most often, followed by traffic calming and speed reduction, and roadway resurfacing. Cycling improvements were considered least important by all user groups who had a relationship to Longview.

Figure 12. Preferred project improvements (Community Survey).



## V. General Comments

Both the Community Survey and the Open House Comment Form gave respondents a chance to provide open-ended comments on the project.

### Community Survey Comments

A total of one hundred and forty-four (144) comments were made by survey respondents. The most common comment, by far, was concern about safety (40); the specific location of Longview between Club and Taylor was often mentioned (34). Other commonly mentioned issues were the need for traffic calming (25) and more enforcement of traffic rules (14). Overall, there was a fairly balanced mix of opinions for and against the project with positive comments outweighing those negative/opposed.

### Open House Comment Form

A total of twenty-seven (27) comments were received from Open House participants, most of whom live on Longview or an adjacent street. Fifteen expressed support for the project (some qualified their support to the median option), four were against the project at a particular location, typically their property; two supported a sidewalk from Taylor to Club only; one was against any sidewalks on Longview at all. A couple comments asked the City to consider protecting trees if the median option was selected.